

# Business Paper

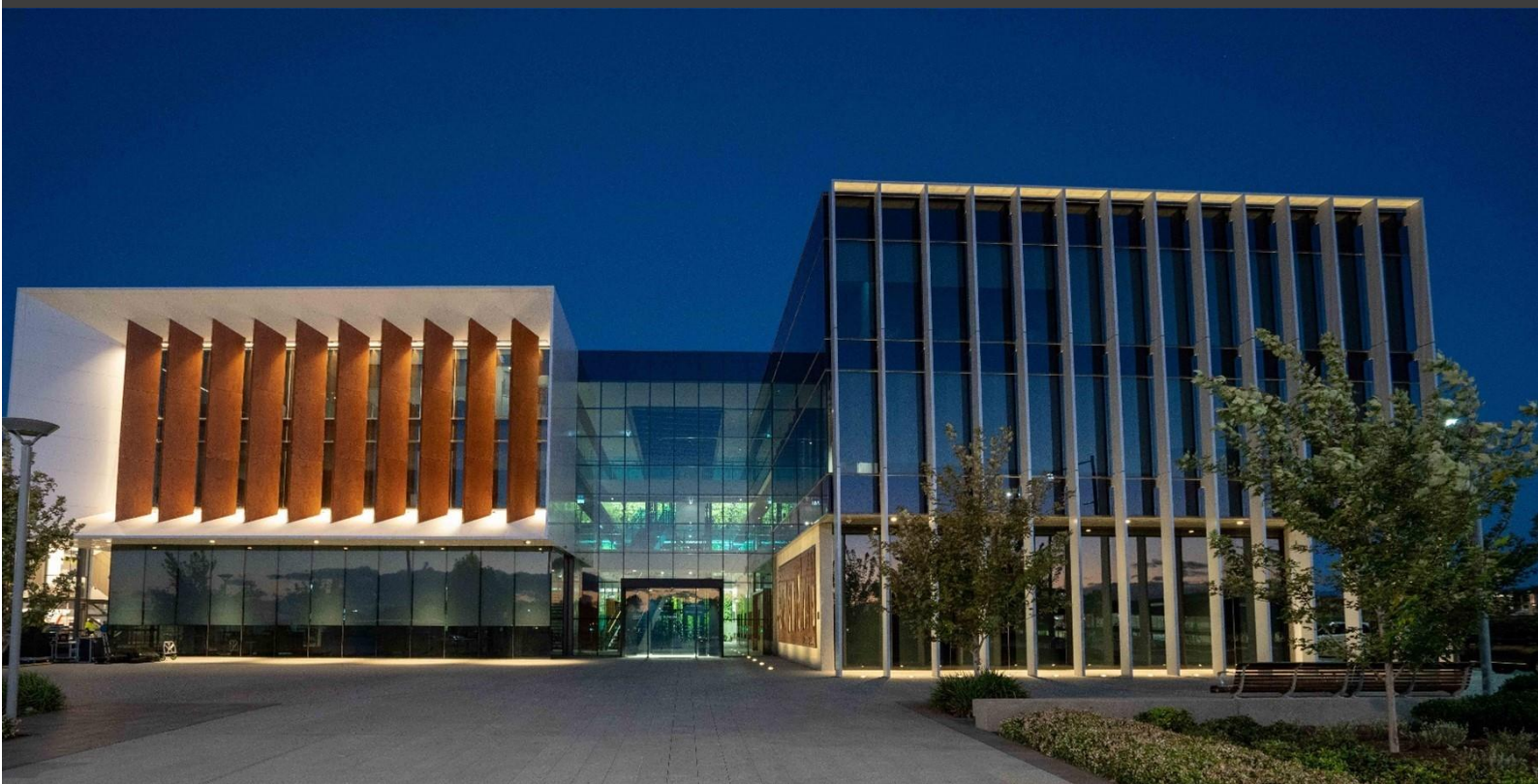
Camden Local Planning Panel

Camden Council

Administration Centre

70 Central Avenue, Oran Park

20 September 2022



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**SUBJECT: ACKNOWLEDGEMENT OF COUNTRY**

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I would like to acknowledge the traditional custodians of this land on which we meet and pay our respect to elders both past and present.

**SUBJECT: RECORDING OF LOCAL PLANNING PANEL MEETINGS**

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In accordance with Camden's Local Planning Panel Operational Procedures, this meeting is being audio recorded by Council staff for publication on Council's website.

No other recording by a video camera, still camera or any other electronic device capable of recording speech, moving images or still images is permitted without the prior approval of the panel.

**SUBJECT:       DECLARATION OF INTEREST**

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This section provides an opportunity for Panel Members to disclose any interest that they may have relating to a Report contained in this Agenda.



# CLPP01

CLPP01

**SUBJECT: DA/2021/1220/1 - CONSTRUCTION OF THE ORAN PARK TAVERN WITH ASSOCIATED SITE WORKS - 2-4 PORTER STREET, ORAN PARK**

**FROM:** Manager Statutory Planning

**EDMS #:** 22/267315

DA Number:	2021/1220/1
Development:	Construction of the Oran Park Tavern with 127 at-grade car parking spaces, business identification signs and associated site works including stormwater drainage and landscaping
Estimated Cost of Development:	\$19,319,065
Site Address(es):	2-4 Porter Street, Oran Park (previously known as 111 Steward Drive, Oran Park and 600C The Northern Road, Oran Park)
Applicant:	Ms Emily Lawson (Premise)
Owner(s):	Leppington Pastoral Co Pty Ltd
Number of Submissions:	Nil
Development Standard Contravention(s):	None
Classification:	Local Development
Recommendation:	Refuse.
Panel Referral Criteria:	New licenced premises
Report Prepared By:	Laura Morabito – Senior Town Planner

## PURPOSE OF REPORT

The purpose of this report is to seek the Camden Local Planning Panel’s (the Panel’s) determination of a development application (DA) for the construction of Oran Park Tavern with 127 at-grade car parking spaces, business identification signs and associated site works including stormwater and landscaping at 2-4 Porter Street, Oran Park (previously known as 111 Steward Drive, Oran Park and 600C The Northern Road, Oran Park).

The Panel is to exercise Council’s consent authority functions for this DA as, pursuant to the Minister for Planning’s Section 9.1 Direction, the application is for a new licenced premises.

## SUMMARY OF RECOMMENDATION

That the Panel determine DA/2021/1220/1 for the construction of Oran Park Tavern with associated site works pursuant to Section 4.16 of the *Environmental Planning and Assessment Act 1979* by refusing the DA for the reasons outlined at the end of this report.

## EXECUTIVE SUMMARY

Council is in receipt of a DA for the construction of the Oran Park Tavern with 127 at-grade car parking spaces, business identification signs and associated site works including stormwater and landscaping at 2-4 Porter Street, Oran Park (previously known as 111 Steward Drive, Oran Park and 600C The Northern Road, Oran Park).

The DA has been assessed against the *Environmental Planning and Assessment Act 1979*, the *Environmental Planning and Assessment Regulation 2021*, relevant environmental planning instruments, development control plans and policies.

The DA was publicly exhibited for a period of 28 days in accordance with the Camden Community Participation Plan 2021. The exhibition period was from 26 July 2021 to 23 August 2021 and no written submissions were received.

The proposal seeks a number of variations to the minimum setback and landscaping requirements as prescribed under Part B5 *Employment Area* of the Oran Park Development Control Plan (Oran Park DCP). Specifically, they include:

- Section 7.0 *Setbacks*, which requires a minimum boundary setback of 3m to the north, western and southern frontages,
- Section 11.0 *Landscaping*, which requires a minimum landscape buffer depth of 3m when boundary fencing is proposed and 2m when no boundary fencing is proposed, and
- Section 11.0 *Landscaping*, which requires planting within at-grade car parking areas to be contained within a 2.5m wide landscape bay, every 6-8 car parking spaces.

The subject proposal provides:

- A minimum setback of 2.19m from Southwell Road (west) to the outdoor dining terrace, contravening the minimum control by 810mm. The development achieves a compliant setback on the northern and southern boundaries.
- A modulated landscape buffer that ranges from 1.2m to 7.5m, in front of the car parking area fronting Dick Johnson Drive where boundary fencing is proposed. The results in a deficit of up to 1.8m.
- Landscape bays within the car parking area have a minimum width of 1m and are provided every 8-10 spaces. This will result in a 1.5m deficit, every two to four car parking spaces.

The proposed variations are considered to have merit in that there will be minimal adverse impacts to the overall streetscape amenity. The variations are assessed in detailed within this report and are generally supported by Council staff.

In addition to the above, the development proposes a variation to the minimum car parking rates as prescribed in the Camden DCP 2019. In accordance with the rates outlined in Section 2.18.2 of the Camden DCP, the development generates a demand for 614 off-street car parking spaces. As the proposal includes the provision of 127 at-grade car parking spaces, there is a shortfall of 487 spaces.

The application was accompanied by a Traffic and Parking Assessment, that contends that the car parking rates in the Camden DCP are outdated in that they do not reflect the current demand for a licenced premises.

Further, the report also submits that a car parking rate of 8.7 spaces per 100m<sup>2</sup> of public floor area would be most appropriate for the development. This rate was based on the average car parking rate of two comparable sites, being Marsden Park Brewhouse and Gregory Hills Hotel. Council staff, and an independent traffic expert engaged by Council, have reviewed the submitted car parking assessment and identified a number of concerns including:

- The Brewhouse, Bottle Shop, Kids Indoor Play Area and Gaming Room are excluded from the calculation of car parking in the applicant's assessment on the basis that they are ancillary to the wider development. Council's position is that these areas are classified as active / public spaces and therefore the applicable car parking rate should apply.
- The independent traffic expert engaged by Council included a review of an additional comparable site to determine a more appropriate car parking rate. After consideration of the three comparable sites, the independent traffic expert established that a car parking rate of 9.6 spaces per 100m<sup>2</sup> of public floor area would be more appropriate for the subject proposal.
- When the rate established by Council's independent traffic expert (9.6 spaces per 100m<sup>2</sup> of public floor area) is applied to all public areas within the development (inclusive of the Brewhouse, Kids Indoor Play Area and Gaming Room), there is a shortfall of 60 car parking spaces.
- If the proposed rate in the applicant's Traffic and Parking Assessment (8.7 spaces per 100m<sup>2</sup> of public floor area) is applied to all public areas within the development (inclusive of the Brewhouse, Kids Indoor Play Area and Gaming Room), there is a shortfall of 53 car parking spaces.
- As such, both car parking rates determine there is still a significant shortfall in car parking across the entire development site when including all public areas.

Given the significant shortfall in car parking it is likely that on-street car parking will be heavily relied on during peak periods. As such, the adverse impacts to the surrounding local road network and adjoining land uses could be significant and the proposed car parking variation is not supported by Council staff.

The DA was referred to Transport for NSW (TfNSW) pursuant to Clause 104 of the *State Environmental Planning Policy (Infrastructure) 2007* as the proposal was considered to be 'traffic-generating'. TfNSW commented on the proposal and noted a number of concerns regarding the provision of the slip lane and outstanding traffic modelling for the wider Oran Park Employment Area. Council staff have reviewed the comments provided by TfNSW and note that:

- All traffic modelling had been undertaken and submitted to TfNSW for review as part of the Planning Proposal that sought to rezone / expand the Oran Park Employment Area that was endorsed on 18 March 2022. The Planning Proposal included the subject site.

- The proposed slip lane from Dick Johnson Drive is generally in accordance with the relevant figures and planning controls contained in Part B5 of the Oran Park DCP. Figures and controls associated with the slip lane were reviewed and approved by TfNSW as part of the insertion of Part B5 of the Oran Park DCP in January 2020.

Despite the above, Council’s Development Engineering Team have raised concerns with the proposed dish crossing off the slip lane. Specifically, it was determined that the dish crossing could not be supported as Dick Johnson Drive is a 60km/hour road and therefore is considered to be a safety concern. The slip lane should have 3% crossfall with the kerb and gutter to match existing conditions. As the proposed dish crossing is likely to pose safety concerns it is also considered that the development is inconsistent with Section 2.1 of Councils Engineering Design Specifications. Given that both TfNSW and Councils Engineers have raised safety concerns, the proposed design and layout of the slip lane is not supported.

As there are a number of issues that remains outstanding, it is recommended that the DA be refused for the reasons outlined at the end of this report.

**KEY PLANNING CONTROL VARIATIONS**

Control / Requirement	Proposed	Variation
<b>Camden Development Control Plan 2019</b>		
<i>Car Parking Rates – Pub / Registered Club / Restricted Premises.</i> <ul style="list-style-type: none"> <li>- 1 car parking space per 2m<sup>2</sup> GFA of public bar area, and</li> <li>- 1 car parking space per 5m<sup>2</sup> GFA of lounge, beer garden, auditorium, billiard room, restaurant, and</li> <li>- 25 car parking spaces per 100m<sup>2</sup> of remaining public floor area.</li> </ul> <i>Retail Premises / Shop / Kiosk</i> <ul style="list-style-type: none"> <li>- Less than 200m<sup>2</sup> – 1 parking space per 30m<sup>2</sup> of GFA.</li> </ul> <i>Total requirement 614 car parking spaces.</i>	127 car parking spaces.	487 car parking spaces.
<b>Part B5 Controls for the Oran Park Employment Area – Oran Park Development Control Plan</b>		
<b>Section 7.0 Setbacks</b>		
<i>Minimum building setbacks are to be in accordance with Figure 6 (refer to Part B5 of the Oran Park DCP).</i> <p><i>*The site requires a minimum <b>3m</b> setback to the northern, western and southern frontages.</i></p>	2.19m (Southwell Road frontage only)	810mm (maximum)
<b>Section 11 Landscaping</b>		

<p><i>For corner lots, setback areas facing the primary street are to be provide with a landscape buffer with a minimum depth of 3m when boundary fencing is proposed to the street.</i></p>	<p>A modulated landscape buffer, ranging from 1.2m to 7.5m is proposed outside the car parking area, via Dick Johnson Drive.</p>	<p>1.8m (maximum).</p>
<p><i>Car parking areas within the Business Development Area (identified in Figure 2) must provide a 2.5m wide landscape bay between every 6-8 car parking spaces.</i></p>	<p>Landscape bays are proposed to be 1m wide (minimum) and are provided every 8-10 car parking spaces.</p>	<p>1.5m (maximum), every 2-4 car parking spaces.</p>

**THE SITE**

The site is legally described as Lot 2201 in DP1266426 and is commonly known as 2-4 Porter Street, Oran Park (previously known as 111 Steward Drive, Oran Park and 600C The Northern Road, Oran Park).

The site is irregular in shape, with a total site area of 9,321m<sup>2</sup> (refer to **Figure 3** below). The site is bounded by Dick Johnson Drive to the south, Southwell Road to the west and Porter Street to the north. The site is relatively flat with a slight slope to the southeast. The site is currently vacant and is clear of any vegetation (see **Figure 1** below).

The site is zoned B5 Business Development (see **Figure 4** below), pursuant to Appendix 1 *Oran Park and Turner Road Precinct Plan* of the *State Environmental Planning Policy (Sydney Region Growth Centres) 2006* (Growth Centres SEPP). The proposed development is classified as a **'pub'** and is permitted with development consent in the B5 Business Development zone pursuant to the Growth Centres SEPP.

Most lots situated along Dick Johnson Drive (heading east) are also zoned B5 Business Development and are projected to contain a range of business and commercial uses. Lots to the north and western ends of the site are zoned IN1 General Industrial and are predominately vacant (see **Figure 4** below). Directly opposite the site is a drainage reserve, which separates the proposal from the nearest residential developments located further south (see **Figure 2** and **Figure 4** below).

The site is located on the southern end of the Oran Park Employment Area and forms part of the Oran Park Precinct in the South West Growth Centre (see **Figure 5** below).



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Figure 1 – The site in its current context as viewed from Dick Johnson Drive.



Figure 2 – The drainage reserve opposite the site and the nearest residential allotments (further south).

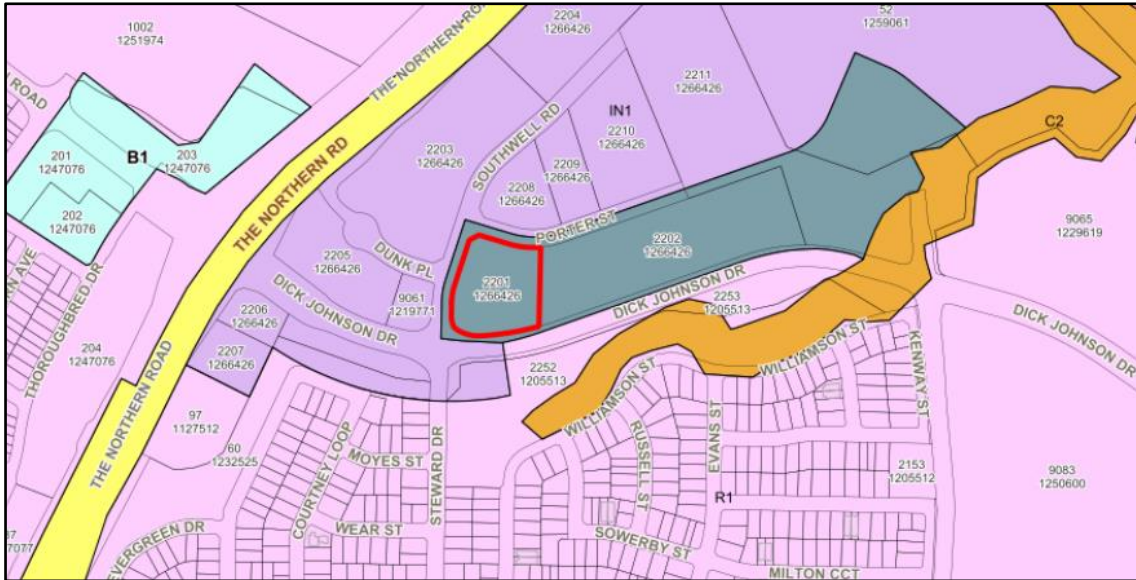
**AERIAL PHOTO**



Figure 3 – The site (outlined in red) and surrounding allotments.

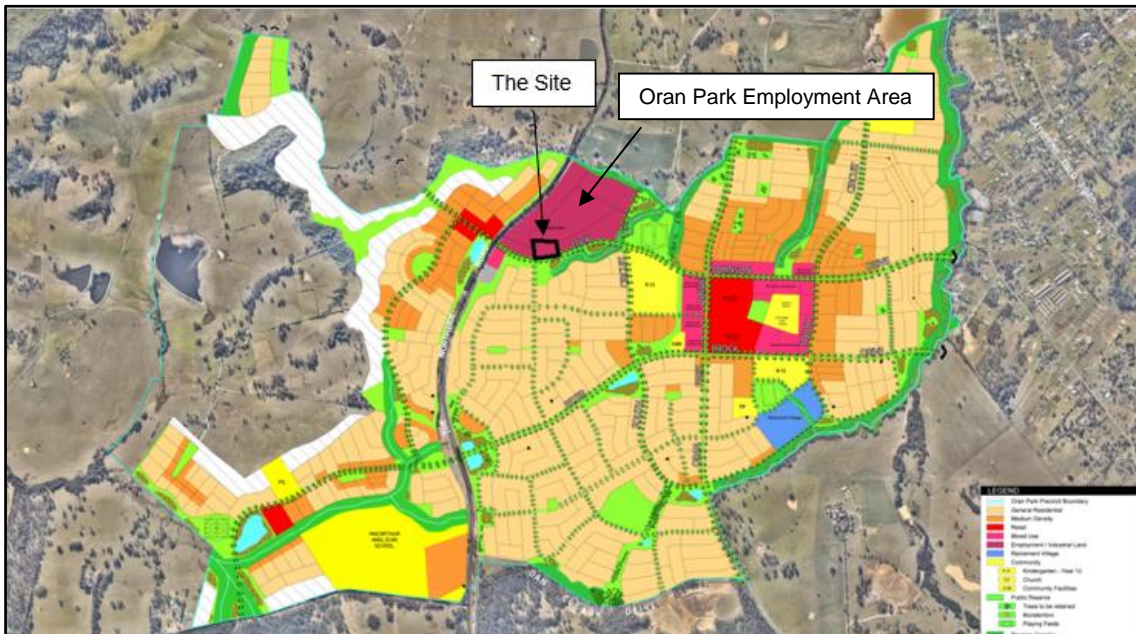
**ZONING PLAN**

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**Figure 4** – The site (outlined in red) and surrounding allotments. Lots coloured in blue are zoned B5 Business Development. Lots coloured in pink are zoned R1 General Residential. Lots coloured in purple (to the north and west of the site) are zoned IN1 General Industrial.

**AREA MASTER PLAN**



**Figure 5** – The site (outlined in black) and the wider Oran Park Precinct. The Oran Park Employment Area is coloured in maroon.

**HISTORY**

The relevant development history of the site is summarised in the following table:

Date	Development
27 January 2020	On 9 April 2019, Council resolved to endorse an amendment to the Oran Park DCP which resulted in the insertion of Part B5 <i>Oran Park Employment Area</i> . Part B5 provides site-specific controls for the Oran Park Employment Area identified under the Growth Centres SEPP (see <b>Figure 5</b> above).
28 June 2020	<b>DA/2019/498/1</b> – Development consent was issued for the remediation of land, Torrens title subdivision to create 10 employment land lots, one drainage reserve lot, one residue lot, civil works, bulk earthworks, construction and dedication of roads and associated landscaping. The proposed development is sought over one of the approved employment land lots.
18 March 2022	<b>PP/2019/6/1</b> – The Planning Proposal approved the rezoning / expansion of the Oran Park Employment Area. As part of the Planning Proposal, a detailed Traffic Study was prepared, which included area wide traffic modelling assessment from all approved developments in the Oran Park Precinct. In response to this, a review of Part B5 of the Oran Park DCP was undertaken and endorsed on 10 May 2022.

**THE PROPOSAL**

DA/2021/1220/1 seeks approval for the construction of the Oran Park Tavern comprising of a single storey pub and bottle shop with associated drive thru, at-grade car park, landscaping, signage and associated site works.

Specifically, the development involves:

- A single storey pub containing the following:
  - brewhouse,
  - bottle shop with associated drive thru,
  - bistro,
  - gaming room,
  - sports bar and tab,
  - garden terrace,
  - kids outdoor area,
  - smokehouse,
  - kitchen,
  - back of house, and
  - amenities.
- At-grade car parking for 127 car parking spaces and 44 bicycle spaces.
- 12 business identification signs,
- Landscaping, and
- Associated site works including civil works, infrastructure and services.

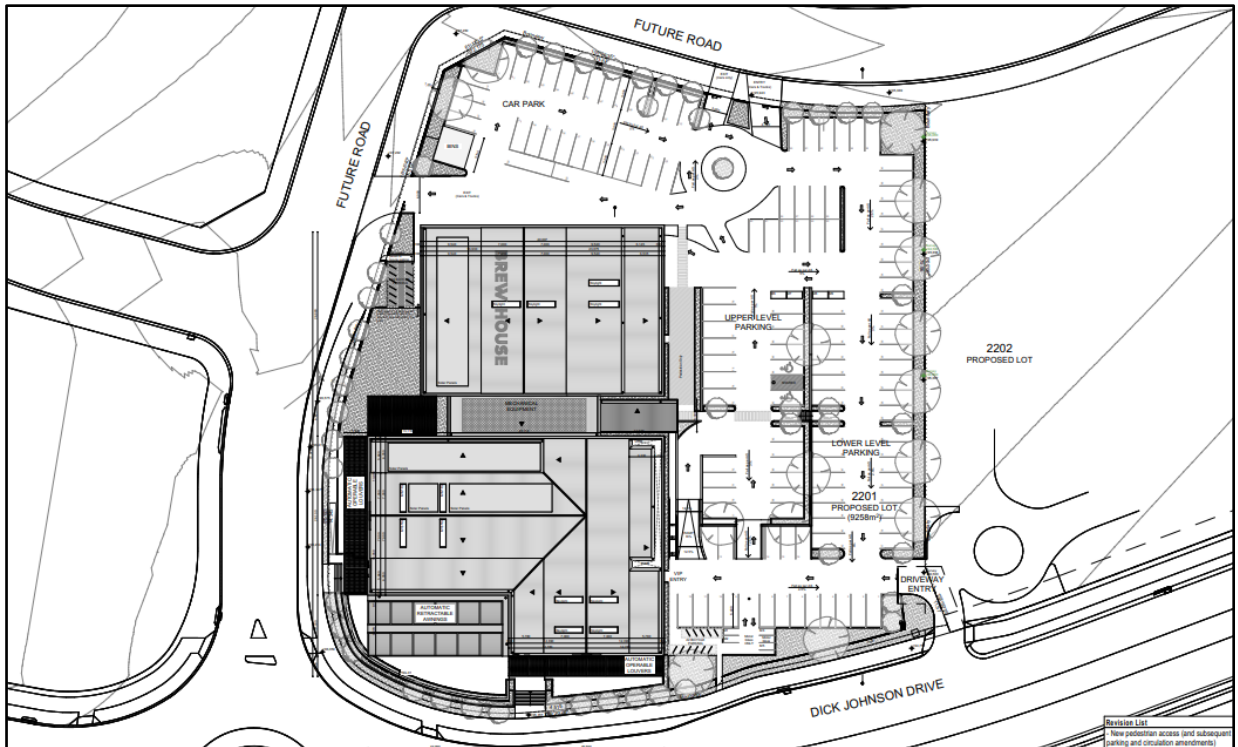


The subject development also seeks the following operational hours:

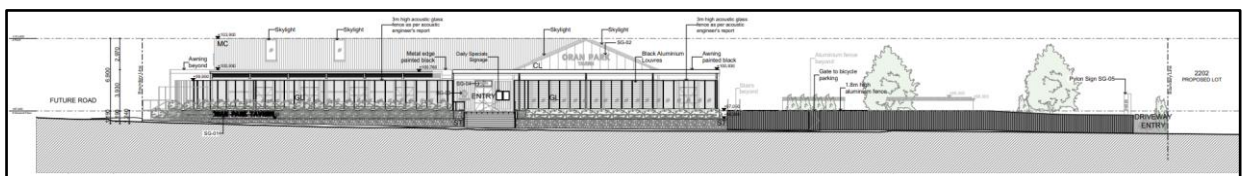
- Monday to Saturday: 7:00am – 2:00am (with a midnight lockout), and
- Sunday: 7:00am to 12am.

The estimated cost of the development is \$19,319,065.

A site plan of the proposed development is reproduced below in **Figure 6**. The front elevation as viewed from Dick Johnson Drive is reproduced below in **Figure 7**.



**Figure 6** – The proposed site plan of the subject proposal.



**Figure 7** – The front elevation of the subject proposal as viewed from Dick Johnson Drive.

**ASSESSMENT**

**Environmental Planning and Assessment Act 1979 - Section 4.15(1)**

In determining a DA, the consent authority is to take into consideration such of the following matters as are of relevance to the development the subject of the DA:

**(a)(i) the provisions of any environmental planning instrument**

The environmental planning instruments that apply to the development are:

- *State Environmental Planning Policy No. 55 Remediation of Land.*

- *State Environmental Planning Policy (Infrastructure) 2007.*
- *State Environmental Planning Policy No. 64 Advertising and Signage.*
- *State Environmental Planning Policy (Biodiversity and Conservation) 2021.*
- *State Environmental Planning Policy (Sydney Region Growth Centres) 2006.*

*State Environmental Planning Policy No.55 Remediation of Land (SEPP 55).*

SEPP 55 provides a Statewide planning approach to the remediation of contaminated land.

Clause 7 of SEPP 55 requires the consent authority to consider if the site is contaminated. If the site is contaminated, the consent authority must be satisfied that it is suitable in its contaminated state for the proposed use of the site. If the site requires remediation, the consent authority must be satisfied that it will be remediated before the land is used for the proposed development. Furthermore, the consent authority must consider a preliminary contamination investigation in certain circumstances.

The parent subdivision DA approved the remediation of contaminated land however, the affected area is located a sufficient distance from the subject site. Consequently, a *Summary of Contamination Investigation* was prepared by Douglas Partners, which concluded that the site is suitable for development from a contamination perspective. The application was reviewed by Councils Environmental Health Officer who raised no objection to the development, subject to the imposition of appropriate conditions. Consequently, the relevant matters contained under SEPP 55 are satisfied.

*State Environmental Planning Policy (Infrastructure) 2007 (ISEPP).*

The ISEPP aims to facilitate the effective delivery of infrastructure across the State.

Pursuant to Clause 104 of the ISEPP, DAs must be referred to TfNSW if the development requires a minimum of 200 car parking spaces, as this would classify the proposal as ‘traffic-generating’. Pursuant to the car parking rates contained in Section 2.18.2 of the Camden DCP, the development requires more than 200 car parking spaces. In response to this and given that Council staff did not support the initial variation request to the minimum car parking rates, the DA was referred to TfNSW for comment.

TfNSW raised a number of concerns with the proposal, specifically in relation to outstanding traffic modelling for the wider Oran Park Employment Area and the provision of the slip lane via Dick Johnson Drive. The Applicant prepared a written response to the concerns raised which was sent to TfNSW, however at the time of writing this report no further response / correspondence has been received.

Consequently, Council staff have reviewed by the comments made by TfNSW and the Applicant and have deemed all outstanding matters to be generally satisfied with the exception of the slip lane design. A response to each of the concerns raised by TfNSW in their response dated 12 April 2022 is provided below.

1. ***TfNSW Comment*** – *Dick Johnson Drive is a key east-west sub arterial road that includes a signalised intersection connection with The Northern Road. As such, TfNSW does not support access off Dick Johnson Drive as it may cause vehicle conflicts and potentially impact the road network.*

**Council Response** – On 9 April 2020, Council resolved to endorse an amendment to the Oran Park DCP. Specifically, the amendment included the introduction of Part B5 *Oran Park Employment Area*, which came into force on 27 January 2020. As part of the insertion of Part B5, a movement plan (and supportive planning controls) were implemented under Section 5.0 *Movement Network*. Specifically, it was noted that that vehicular access off Dick Johnson Drive into the subject site should be established (refer to **Figure 8** below). At the time of the exhibition of Part B5, state agencies (including TfNSW) were notified and given the opportunity to provide comment. TfNSW had commented on the draft DCP and stated, ‘Council is advised to implement the controls under Section 5 in Part B5 Development Control Plan (DCP) controls for Oran Park Employment Area to ensure future Development Applications would not result in unacceptable impacts on the surrounding transport network and if required identify appropriate mitigation measures to offset any impacts of future development to the existing transport network’. As part of the DA on 6 July 2022, Council staff requested clarification on the above comment (provided for the subject DA) given that the provision of the slip lane was considered and approved as part of the planning proposal (as noted above). A response from TfNSW was not received. It is therefore unclear if the concerns raised by TfNSW relate to vehicular access off Dick Johnson Drive or the design of the actual slip lane.

The provision of the slip lane was reviewed by Councils Traffic Engineer, who raised no concerns from a traffic perspective. However, Councils Engineers did raise some concerns with the proposed dish crossing over the slip lane. To minimise adverse safety concerns, it was requested that a 3% crossfall with the kerb and gutter be provided as opposed to the dish crossing for the slip lane. This was requested to match existing conditions of Dick Johnson Drive. The design of the slip lane was never updated to provide a 3% crossfall with the kerb and gutter, which consequently, is likely to have safety impacts for motorists. Given that the development is inconsistent with Section 2.1 of Councils Engineering Design Specifications with regards to patron safety, the slip lane is not supported.



**Figure 8** – Movement Plan contained in Part B5 of the Oran Park DCP.

2. **TfNSW Comment** – TfNSW raises concerns with the left turn entry via the slip lane due to the proximity to the round-about on Dick Johnson Drive. It is recommended that both entry/exit points shall be proposed on the local road networks other than Dick Johnson Drive.

**Council Response** – As noted above, the provision of the slip lane off Dick Johnson Drive was considered and approved under the endorsement of Part B5 of the Oran Park DCP. A review of Part B5 was undertaken as part of the expansion of the Oran Park Employment Area where TfNSW provided commentary during the exhibition period. At the time, TfNSW did not raise any concerns regarding the controls and figures contained under Section 5.0, Part B5 of the Oran Park DCP. Clarification on the above comment was requested by Council staff however a response from TfNSW has not been received.

3. **TfNSW Comment** – In the response prepared by John Cody Consulting, it is mentioned that traffic counts of traffic activity were unable to be obtained due to COVID-19 pandemic impacts on traffic activity within the road network. Considering traffic activity is now returned to pre-covid levels, the applicant is requested to validate the data on the key intersections, particularly, The Northern Road / Dick Johnson Drive signalised intersection. This could be achieved using SCATs output data or other methods agreed to by TfNSW and Council.

**Council Response** – A Planning Proposal was approved on 18 March 2022 for the rezoning and expansion of the Oran Park Employment Area. Specifically, the site was rezoned from IN1 General Industrial to B5 Business Development. As part of this Planning Proposal, a Traffic Study was prepared by GHD (Revision D, dated 1 December 2021), which included traffic modelling of the traffic generation from all approved developments in the Oran Park Precinct. This assessment included the subject site. The report also included modelling of the intersection at Dick Johnson Drive and The Northern Road for future year conditions as well as data from TfNSW Strategic Travel Model (STM v3.6.2), which provides population and employment forecasts in five-year increments between 2016 and 2056. As such, it is considered that the matters raised have been satisfied.

4. **TfNSW Comment** – TfNSW requests SIDRA modelling for day of opening and 10 years post development for the subject development. The modelling is to include cumulative impacts and approved developments within the Oran Park Precinct Employment Area. SIDRA modelling outputs are to be provided for further assessment.
5. **TfNSW Comment** – The response makes reference to the CUBE modelling of the Oran Park Precinct: Employment Area conducted by AECOM in March 2019. Considering the number of approved developments within the Oran Park Precinct Development Area since March 2019, there is lack of information with regards to the applied growth rate. TfNSW requires further clarification on this matter.

**Council Response** – As noted above, it appears TfNSW did not consider the Traffic Study prepared for Planning Proposal that was gazetted on 18 March 2022. In addition to this, the subject site is located on Lot 2201, which adopted a traffic generation rate based on an assumed Gross Floor Area (GFA) of 4,368m<sup>2</sup> (see **Table 1** below). The subject proposal seeks a total GFA of 2,598.9m<sup>2</sup> (inclusive of the bin storage area).

Consequently, it is considered that there is negligible change to future year traffic conditions, as the subject proposal is less than the largest GFA considered in the traffic assessment for the precinct. Given that this conclusion has been drawn, the need for further SIDRA analysis of existing/future intersection is not warranted. **Table 1** below provides an overview of land use allocation that was considered in the Oran Park Employment Area Traffic Assessment.

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**Table 1 – Employment area land use summary**

Land Use Classification	Total Area (m <sup>2</sup> )	Gross Floor Area (m <sup>2</sup> )
Business Development (bulky goods)	43,134	20,273
<b>Tavern</b>	<b>9,293</b>	<b>4,368</b>
General Industrial	136,467	51,857
Fast Food and Petrol Station	10,100	4,747
Fast Food	3,000	1,410
Storage	4,506	2,118

The Traffic and Parking Assessment submitted with the application compared the weekday PM peak period traffic generation between the proposed pub and the modelled business development purposes. Ultimately, the traffic generation between the smaller GFA pub and the potentially larger business development used are found to be similar (around 100 vtpd).

Although most issues raised by TfNSW have been addressed, there are still some concerns regarding the overall safety impacts of the proposed slip lane. Consequently, the provision of the slip lane in its current form is not supported.

State Environmental Planning Policy No.64 Advertising and Signage (SEPP 64)

Twelve business identification signs are sought across the development site. The signs will consist of:

- Wall mounted signs on all external facades of the proposed built form, consisting of 3D illuminated channels letters in copper powder coated aluminum, offset with a white acrylic front face and copper powder coated aluminum border.
- One pylon sign proposed off the slip lane via Dick Johnson Drive, consisting of 3D illuminated channels letters in copper powder coated aluminum offset with a white acrylic front face and copper powder coated aluminum border. The sign has a maximum height of 4m and width of 1.5m.
- One roof mounted sign that is visible on the western elevation, consisting of 3D letters in black powder coated aluminum conceal fixing to roof.

SEPP 64 aims to ensure that signage is compatible with the desired amenity and visual character of an area, provide effective communication in suitable locations and is of high-quality design and finish.

Clause 8 of SEPP 64 requires the consent authority to be satisfied that signage is consistent with the objectives of SEPP 64 and the assessment criteria specified in Schedule 1. A detailed assessment against Schedule 1 of SEPP 64 is provided as an **attachment** to this report. Subject to this assessment, Council staff are satisfied that proposed signs are consistent with the objectives contained in SEPP 64 in that it is compatible with the desired amenity and visual character of the area, will provide effective communication by displaying the development’s name, will be of a high-quality design and finish and will not detract from the overall heritage significance of the site. The proposed signs can be supported by Council staff, subject to the imposition of appropriate conditions of consent.

State Environmental Planning Policy (Biodiversity and Conservation) 2021 (Biodiversity and Conservation SEPP)

The Biodiversity and Conservation SEPP aims to protect the environment of the Hawkesbury-Nepean River system by ensuring impacts of future land uses are considered in a regional context. Council staff have considered the general planning considerations outlined by Biodiversity and Conservation SEPP and are satisfied there will be no detrimental impact on the Hawkesbury-Nepean River system as a result of the development.

State Environmental Planning Policy (Sydney Region Growth Centres) 2006 (Growth Centres SEPP)

*Site Zoning*

The site is zoned B5 Business Development, pursuant to Appendix 1 *Oran Park and Turner Road Precinct Plan*, and Clause 2.2 of the Growth Centres SEPP.

*Land Use/Development Definitions*

The development is characterised as a 'pub' by the Growth Centres SEPP.

*Permissibility*

The development is permitted with consent in the B5 Business Development zone pursuant to the land use table of the Growth Centres SEPP.

*Planning Controls*

An assessment table in which the development is considered against all relevant clauses contained in the Growth Centres SEPP is provided as an **attachment** to this report.

**(a)(ii) *the provisions of any proposed instrument that is or has been the subject of public consultation under this Act and that has been notified to the consent authority (unless the Secretary has notified the consent authority that the making of the proposed instrument has been deferred indefinitely or has not been approved)***

Draft Remediation State Environmental Planning Policy (Draft Remediation SEPP)

The development is consistent with the relevant matters contained in the Draft Remediation SEPP.

Draft Environment State Environmental Planning Policy

The development is consistent with the Draft Environment SEPP in that there will be no detrimental impacts upon the Hawkesbury-Nepean River system as a result of it.

**(a)(iii) *the provisions of any development control plan***

The development controls plans that apply to the proposal are:

- Camden Development Control Plan 2019, and
- Oran Park Development Control Plan (as amended).

An assessment table in which the development is considered against the Oran Park DCP and Camden DCP is provided as an **attachment** to this report. The assessment has determined that a number of variations are sought to accommodate the proposed development. The extent of the non-compliances are further discussed below.

Variations supported by Council staff

*Camden Development Control Plan 2019*

The application proposes a variation to Section 2.18.2 of the Camden DCP regarding the number of off-street car parking. Pursuant to the rates in the Camden DCP, the development generates a demand for **614 car parking spaces**. The car parking calculations as per the Camden DCP is shown below in **Table 2**.

The Camden DCP does stipulate that a variation may be considered to car parking rates in certain circumstances where supported by a Traffic and Parking Assessment submitted with the DA. The DA was accompanied by a Traffic and Parking Assessment prepared by John Coady Consulting Pty Ltd which contends that the car parking rates contained in the Camden DCP (see **Table 2** above) do not reflect the current demand for a licenced premises. Further, the DA was later accompanied by a 'Letter of Advice' prepared by McLarens Traffic Engineering which justifies that a car parking rate of 8.7 spaces per 100m<sup>2</sup> of public floor area would be most appropriate for the development. This rate was based on the average car parking rate of two comparable sites, being the Marsden Park Brewhouse and the Gregory Hills Hotel. The results are shown in **Table 3** below.

**Table 2 – Required car parking rates as per the Camden DCP.**

Camden Development Control Plan 2019				
Rate	Type	Rate	Area	Car Parking Requirement
<b>Pub / Registered Club / Restricted Premises</b> - 1 car parking space per 2m <sup>2</sup> GFA of public bar area and - 1 car parking space per 5m <sup>2</sup> GFA of lounge, beer garden, auditorium, billiard room, restaurant and - 25 car parking spaces per 100m <sup>2</sup> of remaining public floor area.  Developments must also accommodate: - 1 bicycle space per 25 car parking spaces in excess of the first 25 car parking spaces; and - 1 motorcycle space per 50 car parking spaces in excess of the first 50 car parking	<b>Indoor</b>			
	Bistro	1 space per 5m <sup>2</sup> .	554.00m <sup>2</sup>	110.80
	Private Dining	1 space per 5m <sup>2</sup> .	67.874m <sup>2</sup>	13.57
	Sports Bar and TAB	1 space per 2m <sup>2</sup> .	281.00m <sup>2</sup>	140.50
	Gaming Room	25 spaces per 100m <sup>2</sup> .	265.10m <sup>2</sup>	See remaining public floor area below.
	Kids Indoor Area	1 space per 5m <sup>2</sup> .	79.5m <sup>2</sup>	15.90
	Brew House	1 space per 2m <sup>2</sup> .	259.57m <sup>2</sup>	129.79
	Smoke House	Condition can be imposed to ensure there is no public access.		Not applicable.
	<b>Outdoor</b>			
	Outdoor Dining Terrace	1 space per 5m <sup>2</sup> .	94.40m <sup>2</sup>	18.88
	Kids Outdoor Area	25 spaces per 100m <sup>2</sup> .	79.50m <sup>2</sup>	See remaining public floor area below.
Garden Terrace	1 space per 5m <sup>2</sup> .	348.00m <sup>2</sup>	69.60	



spaces.	Sports Terrace	1 space per 5m <sup>2</sup> .	135.00m <sup>2</sup>	27.00
	<b>Total Car Parking Requirement</b>			<b>526.04 (526)</b>
	<b>Total Car parking requirement for remaining floor area</b>			<b>344.60m<sup>2</sup></b>
<b>Retail Premises / Shop / Kiosk</b> <i>Less than 200m<sup>2</sup> – 1 car parking space per 30m<sup>2</sup> of GFA.</i>	Bottle Shop	1 space per 30m <sup>2</sup> .	56.00m <sup>2</sup>	<b>1.88 (2)</b>
<b>Total</b>				<b>614.07 (614) car parking spaces.</b>

**Table 3 – Car parking rates for comparable sites and the proposed development.**

Site	Zoning	Surrounding Zones	Public Transport	Parking Provision	Public Floor Area	Approved / Proposed Parking Rate (per 100m <sup>2</sup> of public floor area)
Marsden Park Brewhouse	B7	IN2, B5	Bus stops within 200 walk	124 <sup>(2)</sup>	1651m <sup>2</sup>	7.5
Gregory Hills Hotel	B5	IN1	Frontage bus stops	165	1689m <sup>2</sup>	9.8
<b>Average</b>						<b>8.7</b>
<b>Proposed Oran Park Tavern</b>	B5	IN1, R1	None currently.	127	1440m <sup>2</sup> (as indicated in the Applicants revised Traffic Impact Assessment) <sup>(1)</sup>	8.8 <sup>(1)</sup>

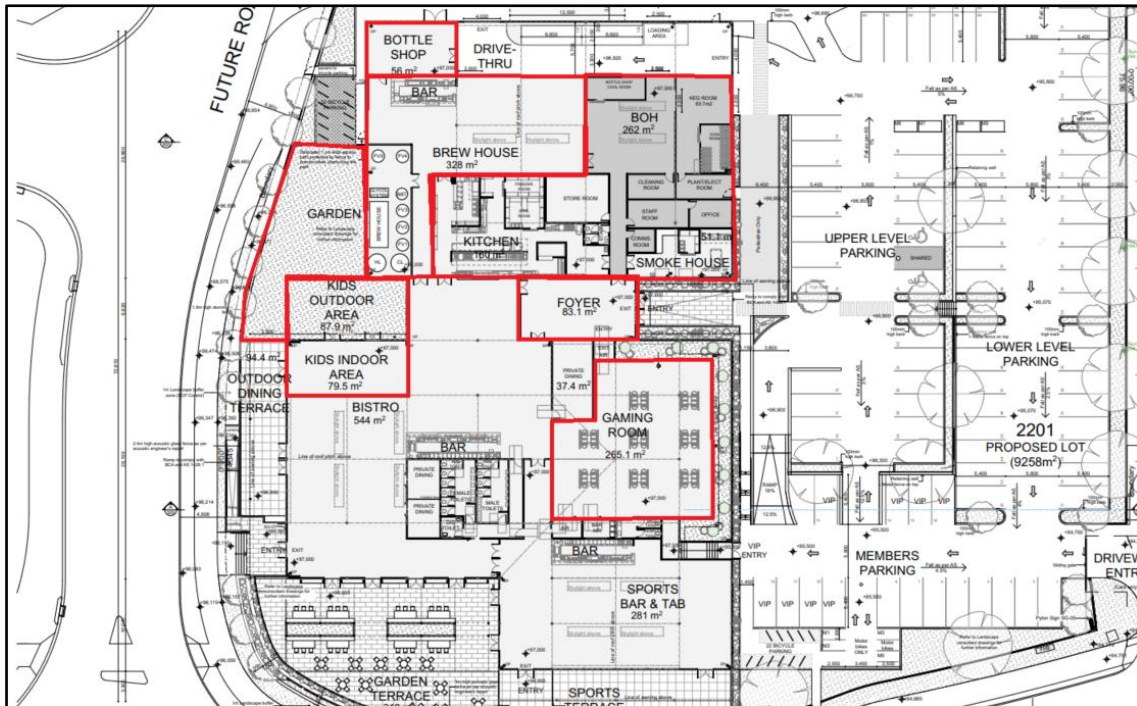
Notes: (1) The above calculations for the proposed Oran Park Tavern are based on the Letter of Advice, prepared by McLaren’s Traffic Engineering, dated 4 August 2022 only. Council staff are questioning the extent of public floor area proposed across the development site, which does result in a non-compliance with the proposed rate. Full details are provided in this assessment report.

(2) The entire development site containing Marsden Park Brewhouse provides a minimum of 266 car parking spaces. As per the Blacktown Development Control Plan 2015, Marsden Park Brewhouse requires a minimum of 124 spaces.

In the ‘Letter of Advice’, it was submitted that the car parking rates should be based on the total public floor area rather than the total GFA. As such, the proposed car parking rate of 8.7 spaces per 100m<sup>2</sup> of public floor area should apply to the following areas within the development:

- Bistro,
- Sports Bar and TAB,
- Private dining,
- Garden Terrace,
- Sports Terrace, and
- Outdoor Dining.

Further, the remaining areas (including the Brewhouse, Gaming Room, Garden, Back of House, Smokehouse, Bottle Shop and Kids Play Area) were noted to be ancillary and/or non-public areas within the premises (see **Figure 9** below). The letter justifies that these areas do not directly generate patron car parking demand and therefore the proposed car parking rate should not apply.



**Figure 9** – Floor plan of the proposed development. Areas outlined in red are classified as non-public areas and/or ancillary to the premises in the ‘Letter of Advice’, prepared by McLaren’s Traffic Consulting.

Council’s Traffic Engineer and an external/independent traffic expert engaged by Council have assessed the parking submissions prepared by John Coady Consulting Pty Ltd and McLaren’s Traffic Engineering and undertaken an analysis of the car parking demands of the development. Several concerns have been raised in relation to the following matters:

- The Brewhouse, Bottle Shop and Gaming Room are not considered ancillary to the wider development. These areas are classified as public spaces and therefore, the applicable car parking rate should apply. Further, the kids indoor play area can easily form part of the total floor area of the bistro, without requiring any modifications to development. As such, the car parking rate should also apply to the indoor kids play area. Depending on which areas are classified as public spaces, the proposed parking for the development falls short by up to 60 spaces (see **Table 5** below).
- Council’s external traffic consultant provided a review of an additional comparable site (being Peachtree Hotel) to determine a more accurate/appropriate car parking rate. With consideration of this comparable site in addition to the Gregory Hills Hotel and the Marsden Brewhouse, they established that a car parking rate of 9.7 spaces per 100m<sup>2</sup> of public floor area would be more appropriate.

Further, Council's expert found that the applicant's assessment fails to consider that the development site containing the Marsden Park Brewhouse provides a total of 266 car parking spaces to also accommodate an existing Function Centre. Consequently, Marsden Park Brewhouse benefits from overflow car parking.

Further detail regarding the above matters is provided below:

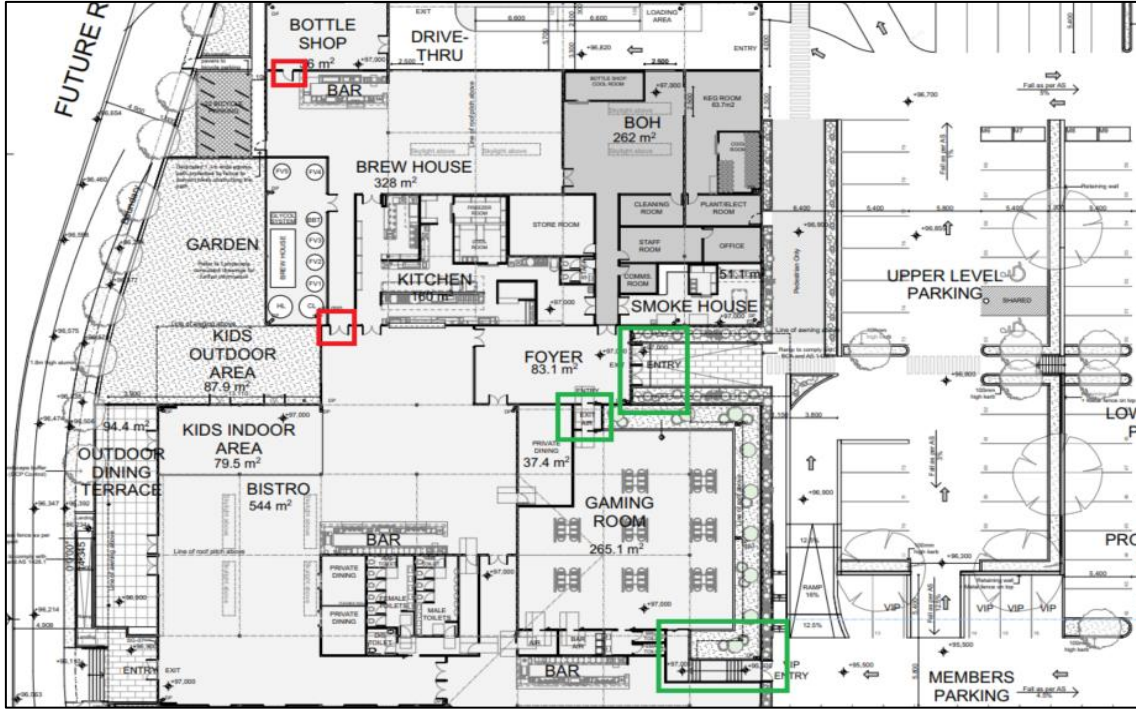
#### *Classification of public floor area*

As noted, it is not agreed that the Bottle Shop, Gaming Room and Brewhouse are ancillary to the development and/or non-public floor areas. This is because:

- The bottle shop provides separate vehicular and pedestrian entrances and therefore does not rely on the remaining development to operate.
- A separate VIP entrance (from the main patron entrance on the eastern elevation) is provided that directly connects to the Gaming Room. Further, the main pedestrian entrance into the premises also directly connects to the Gaming Room via the foyer (see areas outlined in green in **Figure 10** below). Therefore, to access the gaming room, patrons are not required to enter through the main entrance of the building.
- The Brewhouse includes a bar and two separate pedestrian entrances that directly connect to the bistro and bottle shop (see areas outlined in red in **Figure 10** below). A separate staff entrance is also proposed outside the kitchen that also connects directly to the Brewhouse (via a similar route). Therefore, the need for two separate pedestrian entrances into this area is being questioned. It is also noted that the Brewhouse can easily be made available for public use at any time without further development consent, and this area was previously identified as public floor area.

At the time when the DA was publicly notified, the kids indoor play area formed part of the bistro floor space. It appears this change was made to lower the number of car parking spaces (in accordance with the proposed rate in the Traffic Assessment) required to accommodate the development (see **Figures 11** and **12** below). Given that the kids indoor play area can easily form part of the bistro without requiring any modifications, it is considered that this space should also be considered as public floor area.

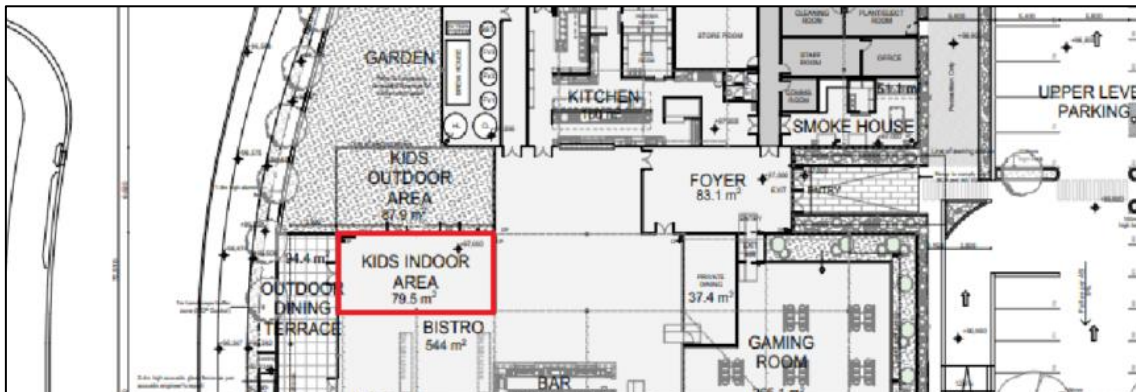
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**Figure 10** – Floor plan of the proposed development. Areas outlined in green relate to entrances within the gaming room. Areas outlined in red relate to entrances within Brewhouse.



**Figure 11** – Site plan of the proposed development submitted to Council on 5 April 2022. The area outlined in red is the proposed location of the kids indoor play area (see **Figure 12** below). At the time, the bistro proposed a floor area of 665m<sup>2</sup>.



**Figure 12** – Site plan of the proposed development submitted to Council on 8 August 2022. The area outlined in red notes the proposed location of the kids indoor play area. The bistro now proposes a floor area of 544m<sup>2</sup>.

*Comparative Study of Similar Sites*

It was determined that the number of comparable sites considered in the original Traffic and Parking Assessment would not adequately reflect the car parking demand for the proposed development. Consequently, the external consultant undertook a site inspection and parking survey at Peachtree Hotel which is also considered to be a comparable site with the proposed development. Specifically, Peachtree Hotel has an approximate public floor area of 1,394m<sup>2</sup> and provides a total of 142 car parking spaces, plus an overflow for approximately 20 vehicles. This equates to a rate of 11.6 spaces per 100m<sup>2</sup> of public floor area. In consideration of these findings, the average derived from all three comparable sites determined a car parking rate of a9.6 spaces per 100m<sup>2</sup> of public floor area would be more appropriate for the development. The results are shown in **Table 4** below.

**Table 4 – Car parking rates for three comparable sites.**

Site	Zoning	Surrounding Zones	Public Transport	Parking Provision	Public Floor Area	Approved / Proposed Parking Rate (per 100m <sup>2</sup> of public floor area)
Marsden Park Broomhouse	B7	IN2, B5	Bus stops within 200 walk	124	1,651m <sup>2</sup>	7.5
Gregory Hills Hotel	B5	IN1	Frontage bus stop.	165	1,689m <sup>2</sup>	9.8
Peachtree Hotel	IN1	IN1, RE1	Frontage bus stop.	142 + 20 overflow spaces	1,394m <sup>2</sup>	11.7
				<b>Average</b>		<b>9.6</b>

By applying the average car parking rates (in consideration of the additional comparable site) to the subject development, it is still noted there is a significant shortfall in car parking (refer to **Table 5** below).

**Table 5 – Shortfall of car parking.**

Area / Location	Total Floor Area	Car Parking Rate	Car Parking Requirement
Bottle Shop	56.00m <sup>2</sup>	1 space per 30m <sup>2</sup> of Gross Floor Area. <i>Note: This is based on the rate for a retail premises as per the Camden DCP.</i>	1.88
Gaming Room	265.10m <sup>2</sup>	9.6 spaces per 100m <sup>2</sup> of public floor area.	25.44
Broomhouse	259.57m <sup>2</sup>	9.6 spaces per 100m <sup>2</sup> of public floor area.	24.92
Kids Indoor Play area	79.5m <sup>2</sup>	9.6 spaces per 100m <sup>2</sup> of public floor area.	7.63
<b>Total</b>	660.17m <sup>2</sup>	As above.	<b>59.87 car parking spaces (60)</b>

In addition to the above, the Traffic and Parking Assessment has failed to consider that the Marsden Park Brewhouse will benefit from the overflow car park that has been allocated to the Function Centre and Brewery which is also located on the same site. As such, the site containing the Marsden Park Brewhouse has provided a total of 266 spaces and therefore, the additional spaces can be used for patrons of the Brewhouse when the Function Centre is not operating at full capacity and/or in use.

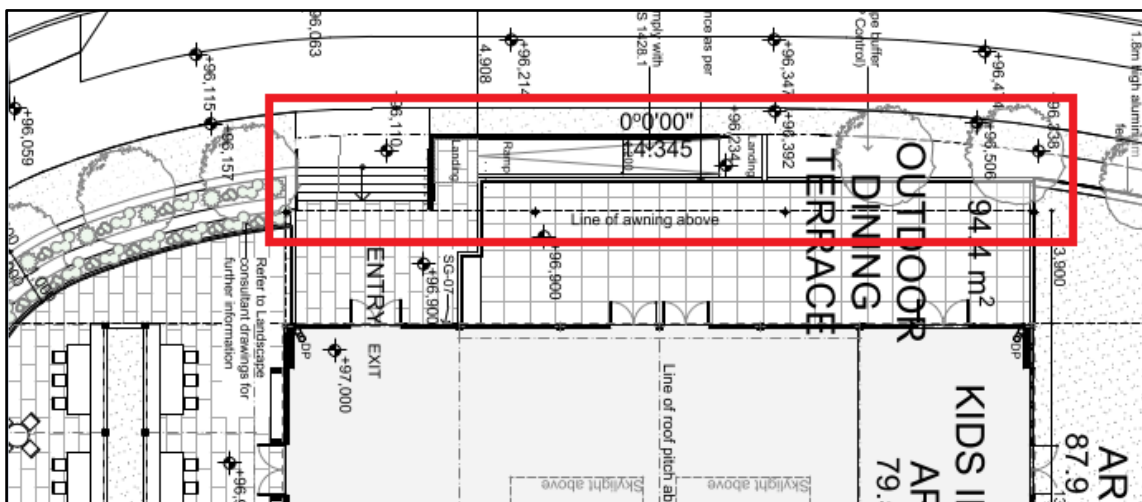
Given the significant shortfall in car parking when compared to the Camden DCP and the reduced rate determined by the independent traffic expert, the variation is not supported by Council staff.

Variations supported by Council staff

**Section 7.0 Setbacks – Part B5 Oran Park DCP**

*DCP Control – Minimum building setbacks are to be in accordance with Figure 6. Where a 2m setback is applied on the Building Setback Plan in Figure 6 (Under Part B5 of the Oran Park DCP), an active street frontage must be provided.*

*Variation Details – Part B5, Section 7.0, Figure 6 of the Oran Park DCP requires a minimum 3m setback off all boundary lines. The proposed development seeks a minimum setback of 2.19m from the western boundary line (Southwell Road) to the face of the outdoor dining terrace (see **Figure 13** below), contravening the planning control by up to 810mm. The development is compliant with the minimum setback controls on all other frontages.*



**Figure 13** – The extent of the variation proposed regarding the front setback off Southwell Road.

Council staff have assessed the variation in detail and recommend that it be supported for the following reasons:

- The extent of the variation only applies to the outdoor terrace (as outlined above), which is an open area, that will be regularly occupied by patrons during the operation. Consequently, this will still encourage an active street frontage.
- The western elevation of the built form (and ancillary structures) are well articulated to reduce the overall dominance of the development from the public domain. As such, the extent of the variation is unlikely to add to the overall bulk of

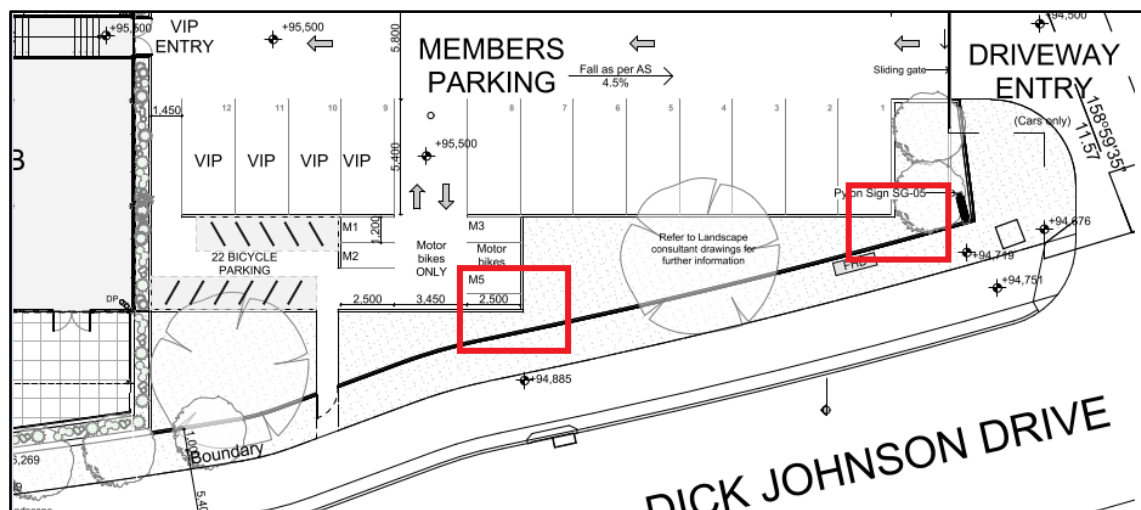
the development and therefore is unlikely to compromise the overall streetscape amenity. Further, the extent of the variation is considered to be relatively minor and therefore achieving compliance will have no greater benefit to the site and/or surrounding land uses.

- The allotment directly opposite the site (on Southwell Road) is subject to a 2m setback (as per the Oran Park DCP). Therefore, the proposed building line will generally align with future developments located directly opposite the site.
- The western elevation will contain significant landscape features to soften the appearance of the development when viewed from the public domain. As such, the proposal (via this elevation) will still encourage an attractive and visually consistent streetscape that will further encourage pedestrian activity within and surrounding the subject site.

**Section 7.0 Setbacks or Section 11.0 Landscaping – Part B5 Oran Park DCP**

*DCP Control – Section 7.0, Control No. 4 states ‘Front setbacks are to be landscaped in accordance with Section 11 ‘Landscaping’. Section 11.0, Control No. 4(a) states setback areas facing the primary street are to be provide with a landscape buffer with a minimum depth of 3m when boundary fencing is proposed to the street or minimum depth of 2m when no boundary fencing is proposed.’*

*Variation Details – A 1.8m high black open palisade style fence along the car parking boundary only (fronting Dick Johnson Drive) is sought, at a 1m landscape boundary setback. Boundary fencing is not sought for the remaining front setback (Dick Johnson Drive). The development proposes a modulated landscape buffer, ranging from 1.2m to 7.5m in front of the car parking area (via Dick Johnson Drive) and will therefore result in a non-compliance by up to 1.8m. The extent of the non-compliance is shown below in Figure 14.*



**Figure 14 – The extent of the variation proposed regarding the landscape setback.**

Council staff have assessed the variation in detail and assess that it can be supported for the following reasons:

- The variation only applies to a small portion of the front boundary line and only applies the concrete area. Therefore, achieving compliance with the minimum standards will have no greater benefit the site and/or surrounding land uses.

- Either side of where the non-compliances have been identified provides significantly larger landscape setbacks to compensate for where the variation is sought.
- The front setback will still consist of buffer planting (as per the Camden DCP requirements) to the edge of the carpark that screen the development via the public domain. Therefore, the proposal will still achieve a high level of visual amenity and character of the area.
- In response to the above, the development is considered to still be generally consistent the objectives contained in Section 11.0 of the Oran Park DCP.

**Section 11.0 Landscaping – Part B5 Oran Park DCP**

*DCP Control – Section 11.0, Control No.7 states ‘carparks within the Employment Area must provide for the planting of trees and shrubs in accordance with Section 2.18.3 of Car parking design criteria. In particular, car parking areas within the Business Development Area (identified in Figure 2) must provide a 2.5m wide landscape bay between every 6-8 car parking spaces, provide a minimum 1m landscaping strip at the end of parking aisles, be landscaped in accordance with Figure 2-12 ‘Design features of carpark’ of Section 2.18.3 of this DCP such that mature tree clusters are located at highly visible corners; and ensure that landscaping does not impede sightlines of drivers.’*

*Variation Details – The development proposes 1m wide landscape bays every 8-10 car parking spaces. Although this results in a non-compliance, the variation is supported by Council staff for the following reasons:*

- The development site is proposed to be screened by high quality landscaping on all frontages, which will reduce the dominance of the development when viewed from the public domain.
- Given the provision of soft landscaping and level of articulation to the built form proposed, the proposal is still able to achieve a high level of visual amenity, which will contribute to the overall character of the wider Oran Park Employment Area.
- Landscape species can still be delivered with the proposed landscape bays that do not compromise sightlines for vehicles and/or pedestrians.
- The proposal has been designed where it will still achieve water sensitive urban design and will therefore have minimal adverse impacts to adjoining allotments and surrounding local road networks.

In response to the above, the proposed variations to the minimum setback and landscaping controls are supported by Council staff.

***(a)(iii) the provisions of any planning agreement that has been entered into under section 7.4, or any draft planning agreement that a developer has offered to enter into under section 7.4***

No relevant planning agreement or draft planning agreement exists or has been proposed as part of this DA.



**(a)(iv) the regulations (to the extent that they prescribe matters for the purposes of this paragraph)**

The *Environmental Planning and Assessment Regulation 2021* prescribes several matters that can be addressed via conditions of consent.

**(b) the likely impacts of the development, including environmental impacts on both the natural and built environments, and social and economic impacts in the locality**

A number of concerns have been raised by Council staff that are likely to result in unreasonable adverse impacts to the surrounding natural and built environment. The key aspects of the development that are likely to result in this outcome are further discussed below.

Councils Engineer has reviewed the subject proposal and has raised concerns regarding the design of the slip lane. Specifically, it was determined that the dish crossing could not be supported as Dick Johnson Drive is a 60km/hour road and therefore poses a safety concern for motorists and pedestrians. Pursuant to Section 2.1.1 of Camden Councils Engineering Specifications, the key objectives relating to road design specifically focus on ensuring a 'high level of amenity and safety' and 'safety for drivers, pedestrians and cyclists who enjoy the use of the road and protection of the adjoining property'. To prevent safety concerns, it was determined that the slip lane should have 3% crossfall with the kerb and gutter to match the existing conditions. The plans have not been updated to reflect a more compliant design.

Given that the design of the slip lane poses safety concerns, it is considered that the development is inconsistent with Section 2.1 of Camden Councils Engineering Specifications. As such, the development is not supported.

In addition to the above, the shortfall in car parking (as detailed above) is likely to have unreasonable adverse impacts to the surrounding local road network and land uses.

Given that the development is likely to have unreasonable adverse impacts to the site and surrounding land uses, it is recommended that the Panel refuse this DA.

**(c) the suitability of the site for the development**

As demonstrated by the above assessment, the site is considered to be unsuitable for the proposed development. Whilst the proposed use of the site is suitable for the locality, the significant shortfall in car parking and proposed driveway design of the slip lane is inappropriate for the site. As such, the suitability of the proposed development on the subject site is not considered appropriate.

**(d) any submissions made in accordance with this Act or the regulations**

The DA was publicly exhibited for a period of 28 days in accordance with Camden Community Participation Plan 2021. The exhibition period was from 26 July 2021 to 23 August 2021 and no submissions were received.

**(e) the public interest**

The public interest is served through the detailed assessment of this DA under the *Environmental Planning and Assessment Act 1979*, the *Environmental Planning and Assessment Regulation 2021*, environmental planning instruments, development

control plans and policies. Based on the above assessment, the proposed development is not considered to be in the public interest and it is recommended that the Panel refuse the DA.

**EXTERNAL REFERRALS**

The external referrals undertaken for this DA are summarised in the following table:

External Referral	Response
<b>Transport for NSW</b>	Concerns were initially raised by TfNSW regarding the provision of the slip lane and outstanding traffic modelling. Full details are provided above.
<b>Camden Police Area Command</b>	No response was provided.

**FINANCIAL IMPLICATIONS**

This matter has no direct financial implications for Council.

**CONCLUSION**

The DA has been assessed in accordance with Section 4.15(1) of the *Environmental Planning and Assessment Act 1979* and all relevant instruments, plans and policies. The DA is recommended for refusal for the reasons outlined below.

**RECOMMENDED**

**That the Panel refuse DA/2021/1220/1 for the construction of the Oran Park Tavern with associated site works at 2 Porter Street, Oran Park for the reasons outlined below.**

1. The proposed development does not comply with the minimum car parking rates prescribed in Section 2.18.2 of the Camden Development Control Plan 2019. Further, the provided Traffic and Parking Assessment has failed to sufficiently justify the extent of the variation and/or demonstrate that a sufficient level of car parking has been provided for the proposal.
2. The proposed development does not comply with the Council’s Engineering Specifications as prescribed in Section 2.3 of the Camden Development Control Plan 2019. The design of slip lane poses safety concerns for motorists and pedestrians travelling along Dick Johnson Drive.
3. The proposed development is considered to result in unacceptable impacts to adjoining land uses and the local street network given the significant shortfall in on-site car parking.
4. It is considered that the site is not suitable for the proposed development in that the development (as proposed) is likely to have significant impacts to the site and surrounding land uses.
5. In consideration of the aforementioned reasons, the development is an unsuitable use of the site, and its approval is not in the public interest.

ATTACHMENTS

1. Growth SEPP Assessment Table
2. SEPP 64 Assessment Table
3. Oran Park DCP Assessment Table
4. Camden DCP Assessment Table
5. John Coady Traffic & Parking Assessment
6. McLaren Traffic Impact Assessment
7. Council's External Consultant Parking Assessment (TPE)
8. Architectural Plans

CLPP01

State Environmental Planning Policy (Sydney Region Growth Centres) 2006 (Growth SEPP)  
Assessment Table

CLPP01

Attachment 1

Clause	Assessment	Compliance?
<p><b>Part 2 Permitted or Prohibited Development</b></p> <p>The site is zoned <b>B5 Business Development</b>.</p> <ul style="list-style-type: none"> <li>- To enable a mix of business and warehouse uses and specialised retail uses that require a large floor area, in locations that are close to, and that support the viability of, centres.</li> <li>- To provide for a wide range of employment generating development.</li> <li>- To provide for a mix of ancillary uses to support the primary function of providing employment generating development.</li> <li>- To maintain the economic strength of centres by limiting the retailing of food, clothing and convenience shopping.</li> <li>- To provide for a range of uses, including recreational uses and function centres, that complement other permissible employment generating land uses within the zone.</li> </ul>	<p>The proposed development relates to a '<b>pub</b>', which is the dominant land use. A '<b>pub</b>' is a permitted use within the B5 zone. The Standard Instrument defines a 'pub' as a '<i>licensed premises under the <u>Liquor Act 2007</u> the principal purpose of which is the sale of liquor for consumption on the premises, whether or not the premises includes hotel or motel accommodation and whether or not food is sold, or entertainment is provided on the premises</i>'. Although there are other uses sought as part of the proposal (including a retail premises and food and drink premises), these uses are considered to be ancillary to the operations of the pub.</p> <p>The development site is situated in the vicinity of the Oran Park Town Centre and will surround a range of land uses that differ from the subject proposal. The proposal is anticipated to further support the viability of the wider area and will provide a range of employment opportunities during the construction and operational phases of the development. Consequently, it is considered that the development will continue to maintain the economic strength of the wider Employment Centre.</p>	Yes
<p><b>Part 4 Principal Development Standards</b></p> <p><b>4.3 Height of Buildings</b></p> <p><i>Except as provided by this Clause, the height of a building on any land is not to exceed the maximum height shown for the land on the Height of Buildings Map.</i></p>	The site is not subject to a building height control.	N/A
<p><b>5.10 Heritage conservation</b></p> <p><i>The objectives of this Clause to conserve the Environmental Heritage of the Oran Park Precinct and the Turner Road Precinct.</i></p>	The site is not located on or in the immediate vicinity of a heritage significant property.	N/A
<p><b>Part 6 Additional Local Provisions</b></p> <p><b>6.1 Public Utility Infrastructure</b></p> <p><i>The consent authority must not grant development consent to development on land to which this Precinct Plan applies unless it is satisfied that any public utility infrastructure that is essential for the proposed development is available or that adequate arrangements have been made to make that infrastructure available when required.</i></p> <p><b>6.6 Development in Special Areas</b></p> <p>The consent authority must not grant development consent for development on land in a special area unless a development control plan that provides for detailed development controls has been prepared for the land.</p>	<p>The site is adequately serviced to support the proposed development. A standard condition is recommended to ensure ongoing compliance.</p> <p>The site is subject to Part B5 Oran Park Employment Area, contained within the Oran Park Development Control Plan. A detailed assessment against this part of the DCP is provided as a separate attachment to this report.</p>	Yes

State Environmental Planning Policy No.64 Advertising and Signage 2004

Schedule 1 Assessment Criteria		
Assessment Criteria	Response	Compliance
<p><b>1 Character of the Area</b></p> <p><i>Is the proposal compatible with the existing or desired future character of the area or locality in which it is proposed to be located?</i></p> <p><i>Is the proposal consistent with a particular theme for outdoor advertising in the area or locality?</i></p>	<p>Proposed signs generally consist of neutral colours and are of an appropriate size given the scale of the overall development. As the site and surrounding allotments are projected for business and commercial uses, proposed signs are considered to be compatible with the desired future character of the area and the wider locality.</p> <p>As noted above, lots with a frontage to Dick Johnson Drive are anticipated for business purposes and therefore are likely to contain a number of business identification signs within the front setback. Whilst the proposal is one of the first to be constructed within the street, proposed signs will consist of a neutral colour tone and finish. It is therefore considered that the development will be sympathetic with the theme for outdoor advertising in the area. As such, the development is compliant with this subsection.</p>	<p>Yes</p> <p>Yes</p>
<p><b>2 Special Areas</b></p> <p><i>Does the proposal detract from the amenity or visual quality of any environmentally sensitive areas, heritage areas, natural or other conservation areas, open space areas, waterways, rural landscapes or residential areas?</i></p>	<p>The development is located wholly within the property boundaries and is sufficiently setback from surrounding key areas. Whilst illumination is sought for all signs, the wider streetscape is anticipated for future business and commercial uses and is located a sufficient distance from the nearest residential areas (further south of Dick Johnson Drive). To ensure the adverse impacts are kept to a minimal for motorists and the nearest sensitive receivers, a condition should be imposed as part of any consent granted that requires all proposed illumination to be in accordance with AS 4282 and AS 1158.</p>	<p>The development is compliant.</p>
<p><b>3 Views and Vistas</b></p> <p><i>Does the proposal obscure or compromise important views?</i></p> <p><i>Does the proposal dominate the skyline and reduce the quality of vistas?</i></p> <p><i>Does the proposal respect the viewing rights of other advertisers?</i></p>	<p>The signs do not protrude above the proposed built form and are not located in the immediate vicinity of any significant sites (as noted above). Consequently, it is unlikely that the sign will compromise significant views identified across the Camden Local Government Area.</p> <p>As noted above, the signs could be subject to a condition of consent, which specifies that illumination sought must adhere to AS 4282 and AS 1158.</p> <p>As the signs are located wholly on the development, will not flash and illuminated can be controlled via a recommended condition of consent, it is considered that the sign will respect the viewing rights of future advertisers in the immediate vicinity of the site.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p>
<p><b>4 Streetscape, Setting or Landscape</b></p> <p><i>Is the scale, proportion and form of the proposal appropriate for the streetscape, setting or landscape?</i></p> <p><i>Does the proposal contribute to the visual interest of the streetscape, setting or landscape?</i></p>	<p>The proposed signs are in proportion to the scale of the proposed building and wider streetscape. It is therefore unlikely the proposed sign will dominate any of the building frontages.</p> <p>A total of 12 signs are sought across the entire development site, that will advertise the use of the premises. Proposed signs will consist of a neutral colour scheme, which is sympathetic with the projected future character of the area. Consequently, the signs are likely to contribute with</p>	<p>Yes</p> <p>Yes</p>

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Attachment 2

State Environmental Planning Policy No.64 Advertising and Signage 2004

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Attachment 2

Schedule 1 Assessment Criteria		
Assessment Criteria	Response	Compliance
Does the proposal reduce clutter by rationalising and simplifying existing advertising?	the overall visual interest of the site and wider streetscape.  The designs of all signs are generally simplistic and will consist of neutral colours. Given the scale of the proposal, the number of signs is unlikely to result in excess clutter that will dominate the site via the public domain or compromise the viewing rights of future advertisers in the immediate vicinity.	Yes
Does the proposal protrude above buildings, structures or tree canopies in the area or locality?	No. The proposed signs do not protrude, or project above buildings, surrounding structures and/or tree canopies.	Yes
<b>5 Site and Building</b>		
Is the proposal compatible with the scale, proportion and other characteristics of the site or building, or both, on which the proposed signage is to be located?	Proposed signs have been designed to reflect the projected scale and business character of the site and surrounding allotments. Specifically, the signs do not dominate the built form or other developments in the vicinity of the site due to their proposed design, size and sitting across the development site. As such, the development is considered to be compatible with the overall scale of the site.	Yes
Does the proposal respect important features of the site or building, or both?	External finishes, scale and height of the signs has appropriately considered developments (approved/proposed) in proximity to the site and will not overly dominate the subject development.	Yes
Does the proposal show innovation and imagination in its relationship to the site or building, or both?	Proposed signs are considered to be an innovative representation of the business and clearly reflect the existing and projected character of the overall employment area.	Yes
<b>6 Illumination</b>		
Would illumination result in unacceptable glare? Would illumination affect safety for pedestrians, vehicles or aircraft? Would illumination detract from the amenity of any residence or other form of accommodation? Is the illumination subject to a curfew?	As noted above, a condition could be imposed in any consent granted, which requires the development to be carried out in accordance with the relevant Australian standards. Given that surrounding allotments are projected for business/commercial uses of a similar scale, proposed illumination is unlikely to impact the viewing rights of future advertisers and/or detract from the overall streetscape amenity.	The proposal is compliant.
<b>7 Safety</b>		
Would the proposal reduce the safety for any public road?	Proposed signs will not impact upon motorists travelling along on Dick Johnson Drive.	The proposal is compliant.
Would the proposal reduce the safety for pedestrians or bicyclists?	Given the siting, size and design of the signs, it is unlikely there will be any adverse impacts to the safety of pedestrians or bicyclists.	The proposal is compliant.
Would the proposal reduce the safety for pedestrians, particularly children, by obscuring sightlines from public areas?	Given the size of the development, it is unlikely the sign will obscure any sightlines from public areas or have any adverse impacts the safety of pedestrians and motorists.	The proposal is compliant.

### Oran Park Development Control Plan

<i>Oran Park Development Control Plan, as amended</i>		
Control	Assessment	Compliance
<b>2. The Oran Park Precinct</b>		
<p><b>2.1 Indicative Layout Plan</b>  <i>All development is to be undertaken generally in accordance with the Indicative Layout Plan (ILP) at Figure 2, subject to compliance with the objectives and development controls set out in this DCP.</i></p>	<p>The site is situated within the employment zone of the Oran Park ILP. The proposed development is anticipated to generate a high number of employment opportunities during the construction and operational phases, in the immediate vicinity of Oran Park Town Centre. Therefore, the proposal is considered to be generally consistent with the land use identified for the site in the Oran Park ILP.</p>	Yes
<p><b>2.2 Vision and Development Objectives</b>  <i>The development must be consistent with the key development objectives of the Oran Park Precinct.</i></p>	<p>The development is of high quality design that will contribute to the overall amenity of the wider area. The development will facilitate employment opportunities (during the operational and construction phase of the proposal) in a location that is easily accessible for all. Further, the proposal can encourage pedestrian activity on site and across the wider employment zone, which will therefore contribute to a vibrant neighbourhood centre.</p> <p>Whilst the above has been noted, there are concerns that the applied car parking rates are not acceptable for a development of this nature. Specifically, it was noted that not all public areas within the premises have been included as part of the total car parking calculations, leaving a significant short fall. Consequently, this may result in adverse impacts to the operation of future neighbouring businesses and local road networks. As this will impact the overall quality of the wider urban environment, the proposal is considered to be inconsistent with the vision and development objectives of the Oran Park Precinct.</p>	<b>No – The development is not compliant.</b>
<p><b>2.4 Infrastructure Delivery and Development Staging</b>  <i>Core infrastructure, services and facilities are to be established at the early stages of development, consistent with the Special Infrastructure Contributions Practice Note and the Oran Park and Turner Road Contributions Plan.</i></p>	<p>All infrastructure on and surrounding the site has been approved and/or constructed in accordance with the relevant Contributions Plan. No further infrastructural works to surrounding public areas and local road networks are required to facilitate the development.</p>	Yes
<p><b>2.5 Hierarchy of Centres and Employment Areas</b>  <i>Development is to be consistent with Table 1 and Figure 4.</i></p>	<p>The development forms part of the Oran Park Employment Area where there is a focus on delivering a range of employment opportunities within the Oran Park Precinct. The proposal is expected to generate a high number of employment opportunities that require various skills during the construction and operational phases. Consequently, the proposed development is considered to be consistent with the characteristics of the Oran Park Employment Area.</p>	Yes
<b>3. Access and Movement</b>		
<p><b>3.1 Street Network and Design</b>  <i>Street network is to be provided generally in accordance with Figure 2 and 5.</i></p>	<p>These matters were considered and addressed within the parent subdivision approval. No further infrastructural works are sought and/or required to facilitate this development.</p>	Not applicable to the subject DA.

## Oran Park Development Control Plan

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Oran Park Development Control Plan, as amended		
Control	Assessment	Compliance
<b>5. Special Area Design Principals</b>		
<b>5.3 Oran Park Employment Area</b> <i>The Oran Park Employment Area is to be approximately 15ha in area and be located in accordance with the Figure at Appendix B.</i>	As noted, the site forms part of the Oran Park Employment Area. A detailed assessment against all relevant planning controls and objectives for the Oran Park Employment Area is provided within this assessment report.	Yes
<i>Council shall not grant consent for any development within the Oran Park Employment Area unless the development is for the purposes of remediation, environmental landscape works or other minor works, that in the opinion of Council, do not predetermine an outcome on the land covered by Section 2 of the Camden DCP.</i>	As noted in the base of this report, DA/2019/498/1 approved the remediation of land and Torrens title subdivision over the parent lot. As this DA included the subject site, it is considered that all required earthworks and remediation works have been carried out.	Yes
<i>The Oran Park Employment Area is to be consistent with the following principles:</i>		
<b>Function and Uses:</b>		
- <i>Provide a diverse range of employment generating development.</i>	The development can provide a range of employment opportunities (that require a range of skills) during the operational and construction phases of the development.	Yes
- <i>Front the east west road with active uses to activate the street.</i>	The site fronts Dick Johnson Drive which is an east west road. The proposal seeks the provision of a sports and outdoor terrace, where there is direct pedestrian access to Dick Johnson Drive. The pedestrian access has potential to generate a high level of pedestrian activity to create an active street frontage.	Yes
<b>Built form and Design</b>		
- <i>A maximum of 15m is permitted for development fronting the East-West Road.</i>	The development seeks a maximum building height of 6.9m which is considered appropriate for the site.	Yes
- <i>Provide setbacks appropriate to the proposed use of the land and characteristics of the location. Setbacks should allow for adequate landscaping to reduce the bulk and scale of buildings and enhance streetscape amenity.</i>	The development is generally consistent with the relevant setback controls imposed under Section 7.0 of the Oran Park DCP, with the exception of the western elevation. However, a detailed assessment by Council staff has determined that the variation to the western elevation has merit given that the adverse impacts to the site and overall streetscape is expected to be minimal. Full details regarding the proposed variation is provided in the main assessment report. Overall, proposed setbacks are considered appropriate for the site and wider employment zone.	Yes
- <i>Buildings are to be designed to incorporate articulation, as well as a variety of colours, materials and finishes in order to provide a high level of visual amenity when viewed from the public domain and roadways.</i>	The external facades of the building have avoided long expansions of blank walls by incorporating sufficient articulation and a range of colours and finishes. The design features break up the bulk of the development which contributes to the overall amenity of the site when viewed from the public domain.	Yes
<b>Residential Interface</b>		
- <i>All developments are to be operated to minimise impacts on adjacent residential areas in terms of noise, traffic and circulation, emissions and bulk and scale.</i>	A riparian corridor is located opposite the site which separates the development from the nearest residential allotments. The DA was accompanied by an Acoustic Report and Traffic Impact Assessment which determined that the overall adverse impacts to the nearest residential receivers would be minimal. The application was reviewed by Councils Environmental Health Officer where it was determined that the existing background noise levels would be maintained, subject to the	<b>No – The development is not compliant.</b>



## Oran Park Development Control Plan

Oran Park Development Control Plan, as amended		
Control	Assessment	Compliance
	imposition of attenuation measures along the Dick Johnson Drive frontage. However, Councils Traffic Engineers and the external consultant did raise concerns that the proposed car parking rate will have significant adverse impacts to the operation of surrounding land uses and local street networks. Specifically, there would be a reliance for on-street car parking, which is likely to overflow to the residential street networks in the immediate vicinity of the site. As this would impact the overall residential interface, the proposal is considered to be inconsistent with the objectives of this control.	
<ul style="list-style-type: none"> <li>- <i>Site servicing and loading facilities, waste storage and other infrastructure are to be designed to minimise visual impact on the public domain and impacts on neighbours.</i></li> </ul>	Swept path diagrams have been provided for a private 11m waste vehicle which indicates an encroachment over car parking space No. 124 and 125 on collection days. However, a condition can be included in any determination, that restricts waste collection during peak periods. Further, the occupation of these car parking spaces can be restricted to a specific time frame in order to prevent conflict from occurring.	<b>No, however this matter can be resolved via a condition of consent.</b>
<b>Pedestrian Amenity and Public Domain</b>		
<ul style="list-style-type: none"> <li>- <i>Incorporate the CPTED and Safer by Design Principles into all developments within the employment area.</i></li> </ul>	<p>A CPTED report was provided with the development application, demonstrating how the proposal sufficiently achieves a building design that addresses all risk associated with the development. The development has incorporated the following to ensure crime is prevented on site:</p> <ul style="list-style-type: none"> <li>- clear sightlines between public and private places,</li> <li>- effective lighting within public areas,</li> <li>- landscaping incorporated where it will not block any public areas within the site,</li> <li>- CCTV surveillance, which is to operate 24 hours, 7 days,</li> <li>- wayfinding signage, and</li> <li>- access control to restrict movements within certain parts of the site.</li> </ul> <p>The development is considered to have responded appropriately to the relevant CPTED principles.</p>	Yes
<ul style="list-style-type: none"> <li>- <i>Provide small areas of high quality public domain or breakout spaces for the amenity of workers.</i></li> </ul>	An indoor, back of house breakout area is provided within the built form for staff members.	Yes
<ul style="list-style-type: none"> <li>- <i>Provide street trees and open space planting that establishes generous shades for pedestrians.</i></li> </ul>	Street trees are provided on the Dick Johnson Drive frontage, which is likely to create sufficient shading areas for pedestrians.	Yes
<ul style="list-style-type: none"> <li>- <i>Design all signage and advertising in a coordinated manner.</i></li> </ul>	A detailed assessment of proposed signs is provided as a separate attachment. The only contention relates to illumination of signage, which can be mitigated via a condition of consent.	Yes
<b>Parking and Access</b>		
<ul style="list-style-type: none"> <li>- <i>Off street parking is to be provided in accordance with Section 2.18 of the Camden DCP. At grade car parking areas are to be located to minimise visual impacts. Large parking areas are to be located behind the building lines.</i></li> </ul>	The application was accompanied by a Traffic and Parking Assessment, that seeks to justify that the required car parking rates in the Camden Development Control Plan 2019 (Camden DCP) are not appropriate for a development of this nature. In response to this, comparable sites (being the Marsden Park Brewhouse and the Gregory Hills Hotel) were used to demonstrate that a much lesser car parking rate (of 8.7 spaces per 100m <sup>2</sup> of public floor area) should apply to the subject development. As noted in this assessment report, Councils Traffic Engineers and the external independent expert engaged by Council have raised concerns with the	<b>No, the development is not compliant.</b>

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## Oran Park Development Control Plan

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Oran Park Development Control Plan, as amended		
Control	Assessment	Compliance
- Direct vehicular access from The Northern Road is not permitted.	<p>proposed car parking rate for the subject development. In particular, it was specified that all public areas within the premises have not been considered, creating a significant shortfall with the justified rate of 8.7 spaces per 100m<sup>2</sup> of public floor area. Further, the independent traffic consultant considered an additional comparable site which determined that a car parking rate of 9.6 spaces per 100m<sup>2</sup> of public floor area would be more appropriate. In consideration of the revised rate, the development would require an additional 60 car parking spaces, creating a significant shortfall. Consequently, this is likely to have significant adverse impacts to the site, surrounding land uses and local road networks, the proposal in its current form is not supported.</p> <p>Direct access via The Northern Road is not proposed.</p>	Yes
<b>6. Environmental Management</b>		
<b>6.1 Riparian Corridors</b>		
<i>Development in and adjoining riparian corridors shall be consistent with Part B2 of this DCP. In the event of any inconsistencies between this DCP and the Waterfront Land Strategy, the Waterfront Land Strategy prevails.</i>	The site is located directly opposite a riparian corridor (Anthony Creek), however is further separated by a four-lane road (Dick Johnson Drive). Given the setback to the riparian corridor, no further assessment is required.	Yes
<b>6.2 Flooding and Watercycle Management</b>		
<i>Management of minor and major flows shall be in accordance with Councils Engineering Specifications.</i>	The site is proposed to drain on the southeastern end of the of the site via drainage pipes. Further, the application was accompanied by a Stormwater Management Report, which denotes an impervious area of less than 90%, therefore not requiring water quality and OSD measures. Overall, the proposed management of flows have been designed generally in accordance with Councils Engineering Specifications. The application was reviewed by Councils Engineering team where no concerns were raised.	Yes
<b>6.3 Salinity and Soil Management</b>		
<i>A Salinity Management Plan is required.</i>	This was provided and approved with the parent subdivision. Conditions can be included in the consent to ensure ongoing compliance with the relevant strategies contained within this plan.	Yes
<b>6.7 Contamination Management</b>		
<i>Stage 1 Preliminary environmental site contamination investigation required where redevelopment is proposed.</i>	Remediation of land was considered with the parent subdivision approval. It was noted that remediation works are not sought in the immediate vicinity of the subject site. As such, no further actions are required to facilitate this development.	Yes
<b>6.8 Odour</b>		
<i>For industrial and employment developments, the emission of all air impurities is to be strictly controlled in accordance with the Protection of the Environment Operations (Clean Air) Regulations and must not exceed the prescribed standard concentration and emission rate.</i>	It was determined that odour generated from the proposed Smokehouse and use of the Pizza Oven can be controlled via conditions of consent. Specifically, this includes the use of woodfire pizza oven requiring separate approval and the extract and treatment of the exhaust from smokehouse being regulated through conditions of consent.	<b>Yes – Subject to the imposition of appropriate conditions of consent.</b>

### Oran Park Development Control Plan

<b>Oran Park Development Control Plan, as amended</b>		
<b>Control</b>	<b>Assessment</b>	<b>Compliance</b>
<p><b>6.9 Acoustic</b> All industrial / commercial / employment development is to comply with the Industrial Noise Policy.</p>	<p>A number of mitigation measures are recommended within the Acoustic Report which is supported by Councils Environmental Health Officer. Ongoing compliance can be assured via a conditions of consent (if the DA is approved).</p>	Yes
<b>8. Amenity and Environmental Management</b>		
<p><b>8.2 Stormwater and Construction Management</b></p> <p>A stormwater concept plan is to be submitted with each building DA indicating how stormwater will be managed and disposed of.</p> <p>All development shall be carried out in accordance with an approved Soil and Water Management Plan prepared in accordance with Managing Urban Stormwater – Soils and Construction, Landcom 4<sup>th</sup> Edition March (The Blue Book).</p>	<p>A stormwater plan has been submitted with the DA and reviewed by Councils Engineers. The proposed catchment plan indicates that stormwater lines will connect to the existing drainage network located on the south eastern end of the site. No issues were raised by Councils Engineers, subject to the imposition of recommended conditions of consent.</p> <p>The provided soil and water management plan is sufficient for the subject proposal. Compliance with this plan can be assured via a condition of consent (should the DA be approved).</p>	Yes  Yes
<p><b>8.3 Waste Management</b></p> <p>A Waste Management Plan is to be submitted with all DAs.</p> <p>All business and industrial operations are to provide adequate on-site waste storage facilities that are readily accessible and appropriately screened from public view.</p> <p>Development must demonstrate that the design takes into account refuse storage and collection without reducing the amenity of neighbouring lots.</p> <p>Storage areas for rubbish bins are to be located away from the front of the development where they have significant impacts on the streetscape, the visual presentation of the building entry and on the amenity of residents, building users and pedestrians.</p>	<p>A waste management plan has been submitted with the Development Application, demonstrating that all waste will be collected and disposed of in accordance with Councils Waste Management Policy. Councils waste officer has reviewed the subject proposal and has raised no concerns, subject to the imposition of recommended conditions of consent.</p> <p>Waste storage areas are located on the northwestern end of the site, which can be screened from the public domain with soft landscaping. As such, the adverse amenity impacts via the public domain are anticipated to be minimal.</p> <p>As detailed above, waste can be sufficiently screened to prevent any adverse impacts to the public domain or the neighbouring allotments.</p> <p>Storage areas for rubbish is located near the car parking area, away from the development. Screening will be used to prevent any adverse amenity impacts to the site or adjoining allotments.</p>	Yes  Yes  Yes  Yes
<p><b>8.4 Site Facilities</b></p> <p>Underground services are required for all domestic servicing utilities, including electrical services.</p> <p>Garbage, mailbox structures, service meters and the like are to be integrated with the overall design of buildings and / or landscaping.</p>	<p>A condition can be included in any consent issued to ensure the site is adequately serviced by relevant utilities.</p> <p>All ancillary structures to the development are appropriately incorporated into the building design.</p>	Yes  Yes

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Oran Park Development Control Plan, as amended		
Control	Assessment	Compliance
<b>8.6 Safety and Surveillance</b>		
<i>Buildings should be designed to overlook streets, lanes and other public or communal areas to provide casual surveillance. In the case of corner lots, habitable windows are also to be orientated to overlook the side street.</i>	As noted above, windows outside habitable areas and pedestrian access into the premises is provided on the northern, eastern and southern elevations, where it is anticipated for regular pedestrian activity. In addition to this, outdoor dining and terrace areas are provided on southern and western elevations, which can further contribute to passive surveillance onto Dick Johnson Drive and Southwell Road. The design of the built form has sufficiently considered eyes on the street.	Yes
<i>Developments are to avoid creating areas for concealment and blank walls facing the street.</i>	All facades are sufficiently articulated and consist of various finishes to break up the overall bulk of the development.	Yes
<i>All developments are to incorporate Crime Prevention Through Environmental Design (CPTED).</i>	A CPTED report was provided with the development application, demonstrating how the proposal sufficiently achieves a building design that addresses all risk associated with the development. The development has incorporated the following to ensure crime is prevented on site: <ul style="list-style-type: none"> <li>- Clear sightlines between public and private places,</li> <li>- Effective lighting within public areas,</li> <li>- Landscaping incorporated where it will not block any public areas within the site,</li> <li>- CCTV surveillance, which is to operate 24 hours, 7 days,</li> <li>- Wayfinding signage, and</li> <li>- Access control to restrict movements within certain parts of the site.</li> </ul> <p>The development is considered to have responded appropriately to the relevant CPTED principles.</p>	Yes

Part B5 Controls Oran Park Employment Area		
Control	Assessment	Compliance
<b>3.0 Land Uses</b>		
<i>Development is to be undertaken generally in accordance with the preferred land use plan in Figure 2 subject to consistency with the objectives and development controls set out in this DCP.</i>	The site is identified as the preferred location for a tavern. A detailed assessment against all relevant controls contained within this part of the DCP is provided below.	Yes
<i>Any development that fronts or presents to Dick Johnson Road should architecturally activate the streetscape and provide visual interest. Use may include but are not strictly limited to neighbourhood shops, take away food and drink premises and ancillary retail uses that serve the Employment Area and local open space.</i>	The development is for a pub only with ancillary retail and food and drink uses are also proposed within the site. The Dick Johnson Drive frontage contains an outdoor dining and sports terrace area (outside the pub), which is expected to have consistent pedestrian activity. Therefore, these uses will contribute to passive surveillance onto the Dick Johnson Drive frontage.	Yes
<b>5.0 Movement Network</b>		
<i>Heavy vehicle is not permitted direct access to lots from Dick Johnson Drive.</i>	The site will be serviced by a private waste contractor where direct access can be restricted via the northern entrance only. Standard conditions can be included in any consent granted to ensure direct access via Dick Johnson Drive is restricted for waste trucks.	Yes – Subject to the imposition of appropriate condition of consent.
<i>Off-street car parking must be provided in accordance with Section 2.18.2 of the Camden DCP 2019. At-grade parking areas must be suitably located to</i>	As noted above, the application was accompanied by a Traffic and Parking Assessment, which detailed that the proposed car parking rate in the Camden DCP is unreasonable for the subject proposal. As such, a car parking rate was	<b>No – The development is not compliant.</b>

## Oran Park Development Control Plan

<b>Part B5 Controls Oran Park Employment Area</b>		
<b>Control</b>	<b>Assessment</b>	<b>Compliance</b>
<i>minimise visual impacts to the street and public domain.</i>	determined based on more comparable sites and applied to the subject proposal. Specifically, it was determined that a rate of 8.7 spaces per 100m <sup>2</sup> of public floor area should apply. However, concerns were raised by Councils Traffic Engineers and the external/independent traffic consultant as there were a number of areas excluded from being classified as a 'public floor area'. Specifically, the bottle shop, the gaming room and the brewhouse were excluded in that they were considered as 'ancillary' to the proposed development. If the proposed rate of 8.7 spaces per 100m <sup>2</sup> of public floor area was to extend across all these areas (being the brewhouse, the gaming room, private dining areas and the bottle shop), an additional 53 spaces would be required, creating a significant shortfall. Further, the external consultant determined that an additional site should be used to provide a more accurate/appropriate rate. Following the detailed study of Peachtree Hotel, it was determined a rate of 9.6 spaces per 100m <sup>2</sup> of public floor area would be more appropriate. Consequently, this rate would require a minimum of 60 parking spaces. Given the significant shortfall and the likely impacts to surrounding land uses and road networks, it is recommended that this DA be refused.	
<b>6.0 Site Planning</b>		
<i>Development must be consistent with the front setbacks shown in Figure 6.</i>	The development via the Dick Johnson Drive and Southwell road frontages are subject to a setback control of 3m. The built form via the western elevation is setback a minimum of 2.19m, creating an 810mm deficit. The development via all other frontages are compliant with this control. A detailed assessment by Councils staff has noted that this variation has merit and can be supported. Full details are provided in the main assessment report.	<b>No – Refer to the main assessment report for further justification.</b>
<i>Front setback areas are to be designed and not dominated by parking areas or loading facilities.</i>	Car parking is proposed on the northern end and along the eastern boundary of the site. Whilst some car parking spaces generally align with the proposed built form, landscaping treatment is sought along all frontages to reduce the dominance of the development via the public domain. Consequently, it is unlikely to dominate the sites frontage.	Yes
<i>Where car parking is proposed forward of the building line, the car parking must:</i> <ul style="list-style-type: none"> <li>- Not encroach on the minimum front landscaping setback,</li> <li>- Be suitably located away from building entrance points so that the building entrance remains visible from the street.</li> <li>- Be adequately screened and broken up with suitable landscaping and / or fencing while maintaining view lines between the street and the building.</li> <li>- Designed to be provided convenient, safe and comfortable pedestrian access to the building entrance.</li> </ul>	As noted above, car parking is generally situated behind the front building line (with the exception of the northern elevation, via Porter Street). The boundaries of the site are proposed to be screened with landscaping and boundary fencing to reduce the dominance of the car parking area from the public domain. In addition to this, further tree planting will be delivered around the perimeter of the site and is appropriately spaced to maintain views to and from the site, via the public domain. This matter was approved as part of the original subdivision approval for the site (DA/2019/498/1).  The design of the development has provided separate pedestrian entrances that are easily visible via the street and wider public domain.	Yes

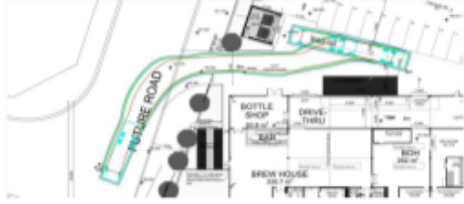
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Part B5 Controls Oran Park Employment Area		
Control	Assessment	Compliance
<p>Development sites must be designed to minimise the impacts of stormwater run-off on adjoining riparian corridors and demonstrate compliance with Section 8.2 Stormwater and Construction Management of Part A of this DCP.</p>	<p>The provided stormwater design integrates the endorsed water cycle management strategy for the Oran Park precinct. The design has been reviewed by Councils engineer where no issues were raised (subject to the imposition of recommended conditions of consent).</p>	<p>Yes</p>
<p><b>6.1 Public Domain and Special Interface Areas</b></p>		
<p>The principles of Crime Prevention Through Environmental Design (CPTED) and the Safer by Design (NSW Police) must be incorporated into the design of all development within the employment area.</p>	<p>The DA was accompanied by a CPTED assessment, which determined that the proposal has been designed in a way where all relevant principals have been incorporated.</p>	<p>Yes</p>
<p>Development must address the following general design consideration set out below:</p> <ul style="list-style-type: none"> <li>- Building elevations must be sited to positively address the street frontage.</li> <li>- Long unbroken walls (i.e. walls in excess of 15m in length) generally must be avoided.</li> <li>- Provide visual interest passive surveillance by incorporating design features.</li> </ul>	<p>The architectural plans demonstrate that all elevations sufficiently address the public domain by avoiding long expansions of long, blank and unbroken walls through the use of articulation, modulation, mixture of building materials and colours, use of glazing and awnings, roof design and landscaping.</p>	<p>Yes</p>
<p>Provide a coordinated landscape theme that is consistent with the provisions of Section 7 and Section 11.</p>	<p>A variation is sought to the minimum landscape setback on the southern elevation. Discussion of the proposed variation is provided in the main assessment report.</p>	<p><b>No – Refer to the main assessment report for further justification regarding the non-compliance.</b></p>
<p>Provide a clear, articulated public entry that is visible from the street and clearly indicate entry/exit access pathways.</p>	<p>Clear vehicular and pedestrian entrances are proposed on the southern, western and northern ends of the site that are clearly visible via the public domain.</p>	<p>Yes</p>
<p>Provide a delineated and safe pedestrian pathway to the building entrance, separate from vehicular access.</p>	<p>Safe pedestrian pathways are proposed within the car parking area that directly connect to the main entrances into the premises.</p>	<p>Yes</p>
<p>Ensure that site servicing, loading facilities and waste storage are suitably located and designed such that they do not dominate the streetscape.</p>	<p>The site will be sufficiently serviced, where waste storage and loading facilities are located in an area where there can be minimal adverse impacts to pedestrians and motorists. However, the provided Traffic Impact Assessment (which includes swept path diagrams for a 11m waste truck), indicates that during waste collection (whilst car parking space No. 127 and 128 are occupied), there would be conflict (see <b>Figure 1</b> below). To prevent this from occurring, a condition can be included in any consent granted stating that waste collection must occur outside hours of operation.</p>	<p><b>No, however this matter could be resolved via a condition of consent.</b></p>
	 <p>The diagram shows a site plan with a building footprint, parking spaces, and a green line representing the swept path of a waste collection vehicle. The path starts at a 'WASTE ROAD' on the left, moves around the building, and then loops back to the road. Labels include 'BOTTLE SHOP', 'DRIVE THRU', and 'BREW HOUSE'.</p>	
	<p><b>Figure 1 – Swept path diagram for a waste collection vehicle.</b></p>	
<p>Ensure that utilities are suitably located to the site or rear of the building where</p>	<p>Utilities are located on the side of the building and well-integrated into the design to minimise the</p>	<p>Yes</p>

## Oran Park Development Control Plan

<b>Part B5 Controls Oran Park Employment Area</b>		
<b>Control</b>	<b>Assessment</b>	<b>Compliance</b>
<p>possible and designed to minimise visual impact on the public domain. Where proposed, utilities should have:</p> <ul style="list-style-type: none"> <li>- Colour treatments compatible with the building façade, and</li> <li>- Be suitable screened from the street with landscaping.</li> </ul> <p>Consistent open style fencing is preferred.</p>	<p>adverse visual impacts. Additional landscaping is provided around the perimeter of the building to further mitigate the potential amenity impacts.</p> <p>Open style fencing is proposed on the Dick Johnson Drive frontage.</p>	Yes
<p><b>6.2 Topography Cut and Fill</b></p> <p>Development applications must illustrate where it is necessary to cut and/or fill and provide justification for the proposed changes to the land levels.</p> <p>Retaining walls visible from the street and public domain should be a maximum height of 1m before incorporating a landscape step and all earthwork batters are to be landscaped to mitigate adverse visual impacts.</p>	<p>Minor cut and fill works are sought to accommodate the development, however most earthworks were approved as part of the subdivision DA. Cut and fill works proposed are relatively minor and are unlikely to have significant adverse impacts to adjoining land uses.</p> <p>Retaining walls are sought within the car parking area. Given the siting of the retaining wall, it is unlikely they will be seen from the public domain. Therefore, adverse visual impacts are expected to be kept to a minimal.</p>	<p>Yes</p> <p>Yes</p>
<p><b>7.0 Setbacks</b></p> <p>Minimum setbacks are to be in accordance with Figure 6.</p>	<p>A detailed assessment regarding the minimum setbacks is provided above.</p>	<p><b>No – Refer to the main assessment report for further justification regarding the non-compliance.</b></p>
<p><b>8.0 Height of Buildings</b></p> <p>Building heights are to be in accordance with Figure 7. In the area marked 'Periphery Area,' a maximum building height of 15m is permitted for development fronting Dick Johnson Drive.</p>	<p>The development proposes a maximum building height of 6.9m which is considered appropriate for the site.</p>	Yes
<p><b>9.0 Overshadowing</b></p> <p>Development should provide adequate solar access to outdoor amenity areas.</p> <p>Development should minimise impact of overshadowing on neighbouring solar panels.</p>	<p>Shadow diagrams have been provided, demonstrating that sufficient solar access to outdoor amenity areas will be achieved.</p> <p>Adjoining allotments are currently vacant and no development application has been lodged regarding potential future development. This matter could not be considered.</p>	<p>Yes</p> <p>Yes</p>
<p><b>10.0 Building Design</b> <b>10.1 Building Form and Material</b></p> <p>A mix of materials, colours and architectural features are to be used. A schedule of colours and finishes must be submitted with the Development Application.</p> <p>The office component of any development must be incorporated into the overall design of the building and generally located along the street frontage.</p> <p>The built form and architecture of development on 'Key Sites' identified in</p>	<p>A detailed schedule of colours and finishes has been provided, noting that the external façade will consist of metal cladding, stone and brick.</p> <p>The office component is located on the northern end of the site, away from Dick Johnson Drive. Nevertheless, other active uses are proposed along this frontage to contribute to passive surveillance. Therefore, the location of the office space is considered appropriate for the site.</p> <p>The development has incorporated various design elements to ensure each frontage is appropriately</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p>

## Oran Park Development Control Plan

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Attachment 3

Part B5 Controls Oran Park Employment Area		
Control	Assessment	Compliance
<p>Figure 2 must enhance its location and positively respond to and emphasise the corner.</p> <p>Blank building facades facing the primary street frontage are not permitted.</p> <p>Design should consider and reflect passive design principles including thermal mass, solar screening, solar access / shading to outdoor amenities, insulation and ventilation where possible.</p>	<p>addressed, contributing to the overall amenity of the site.</p> <p>No blank facades are sought to any elevation.</p> <p>Appropriate materials are used to ensure that sufficient air quality and solar access is achieved within the development and around the site.</p>	<p>Yes</p> <p>Yes</p>
<p><b>11.0 Landscaping</b></p> <p>A concept landscape plan must be submitted with the development application.</p> <p>Proposed landscaping must demonstrate the provision of plantings that assist in creating a human scale, reducing visual bulk to the streetscape and/or within the site and offering amenity.</p> <p>For corner lots:</p> <ul style="list-style-type: none"> <li>- setback areas facing the primary street are to be provide with a landscape buffer with a minimum depth of 3m when boundary fencing is proposed to the street or minimum depth of 2m when no boundary fencing is proposed.</li> <li>- Adequate and quality landscape treatment for the landscape buffer must be demonstrated through the Concept Landscape Plan and must be maintained for the life of the development.</li> </ul> <p>Proposed landscaping forward of the building line should maintain open view lines between the street and the building use.</p> <p>Where practical, tree planting within car park areas is encouraged to provide shade.</p> <p>Native and low water usage plant species are preferred.</p>	<p>A landscape plan has been provided with the development application, generally consistent with the relevant landscaping principles.</p> <p>Buffer planting (consisting of westringia, lagerstroemia, viburnum, rice flowers and ginkgo trees) between the public domain and car parking area is provided. These plant species will reduce the overall bulk to the streetscape, whilst still permitting for views to the public street and contributing to the overall amenity of the site.</p> <p>Variations to the landscaping buffer is sought to the Dick Johnson Drive frontage as follows:</p> <ul style="list-style-type: none"> <li>- 3m in front of the garden terrace,</li> <li>- 2.205m in front of the sports terrace, and</li> <li>- Modulated landscape buffer between 1.2m and 7.5 m in front of the car parking area.</li> </ul> <p>Detailed justification regarding the noncompliance is provided within the main part of this report.</p> <p>The landscape plan has appropriately detailed quality and adequate landscaping species (including westringia, lagerstroemia, viburnum, rice flowers and ginkgo trees) to ensure the overall amenity is retained along the streetscape.</p> <p>As detailed, landscaping species noted in the landscape plan will maintain views to and from the development and the public domain.</p> <p>Sufficient tree planting is provided within the car parking area, where appropriate.</p> <p>Native and low water usage species proposed.</p>	<p>Yes</p> <p>Yes</p> <p><b>No – Refer to the main assessment report for further justification regarding the non-compliance.</b></p> <p>Yes</p> <p>Yes</p> <p>Yes</p>
<p><b>13.0 Fencing</b></p> <p>Where fencing is proposed forward of the building line to a street it should be:</p> <ol style="list-style-type: none"> <li>a) black open-style steel palisade fencing with a maximum height of 2.4m, measured from ground level (finished); and</li> <li>b) sited so it does not impede sight lines for drivers.</li> </ol>	<p>The application did detail that fencing is to be provided along the Dick Johnson Drive frontage and around the perimeter of the kids play area (western frontage). Fencing will consist of aluminium, with a maximum building height of 1.8m. Proposed fencing design includes louvers that are sufficiently spaced, to permit for direct overlooking onto the public street via the development.</p>	<p>Yes</p>



Oran Park Development Control Plan

<b>Part B5 Controls Oran Park Employment Area</b>		
<b>Control</b>	<b>Assessment</b>	<b>Compliance</b>
<p><b>14.0 Employment Operations</b></p> <p><i>Access, parking and loading to be in accordance with 'Part 2.18.2 Off Street Car parking rates/requirements' of Camden DCP 2019.</i></p> <p><i>Development applications must provide the following details:</i></p> <p><i>a) proposed hours of operation;</i></p> <p><i>b) number and timing of deliveries expected per day;</i></p> <p><i>c) nature, frequency and routes of heavy vehicles expected to access the premises;</i></p> <p><i>d) nature of machinery proposed to be operated at the premises, including noise levels generated and noise attenuation measures proposed to be implemented; and</i></p> <p><i>e) proposed locations and hours of operation of external light sources and the extent of light spillage outside of the new subject property.</i></p>	<p>As detailed above, a variation to this control is sought, however a number of concerns were raised by Councils Traffic Engineers. Given that the proposed car parking rate is still not sufficient for the subject proposal, it is recommended that this application be refused. Full details are provided within the base of this report.</p> <p>Operation Hours for the development is as follows:</p> <ul style="list-style-type: none"> <li>- Monday to Saturday: 7am to 2:00am (with a midnight lockout).</li> <li>- Sunday and Public Holidays: 7am to 12:00am.</li> </ul> <p>Deliveries to the premises will only occur from 7:00am to 6:00pm, Mondays to Saturdays. No deliveries will occur on Sundays. Specific time and days to reduce conflict with other vehicles and peak patronage can be controlled via a condition of consent.</p> <p>No machinery will be used during the operational phase of the development that would result in significant acoustic impacts to surrounding land uses. However, noise attenuation measures will be implemented along the Dick Johnson Drive frontage as it is a main road and will adjoin an area of the development that is expected to have high pedestrian activity.</p> <p>As the nearest residential allotment is located a minimum 95m away, the establishment of an external light strategy is deemed not necessary as the impacts are expected to be minimal.</p>	<p><b>No – The application is not supported. Refer to the main assessment report for further details regarding the non-compliance.</b></p> <p>Yes</p>
<p><b>15.0 Outdoor Storage</b></p> <p><i>External storage of goods, materials and equipment such as garbage bins is not permitted where visible from the public domain.</i></p> <p><i>Waste collection and outdoor storage areas must be located behind the building line, be screened with landscaping and fencing and have a sealed ground surface.</i></p>	<p>External storage of waste is sought on the north-western end of the site. Waste storage will be suitably screened to ensure it is not greatly visible from the public domain.</p> <p>Waste storage is located behind the building line that can be enclosed and screened by landscaping.</p>	<p>Yes</p> <p>Yes</p>
<p><b>16.0 Acoustics</b></p> <p><i>Development Applications proposed on lot/s where operational noise limits and individual project noise levels have already been determined (and accepted by the Consent Authority), must submit with their application written certification from a qualified acoustic consultant. The written certification must confirm or demonstrate that the operation of the proposed development will comply with relevant noise levels.</i></p>	<p>An acoustic report was prepared for the application where a number of acoustic measures were recommended as a mitigation strategy. In response to this, acoustic fencing is sought on the southern end of the site, outside the garden, kids outdoor area, outdoor dining terrace, garden terrace and sports terrace as these areas are expected to have high active pedestrian usage. The acoustic measures proposed are supported by Councils Environmental Health Officer subject to recommended conditions of consent.</p>	<p>Yes</p>

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Attachment 3

## Camden Development Control Plan 2019 (Camden DCP) Assessment Table

Control	Assessment	Compliance
<p><b>2.0 General Land Use Controls</b></p> <p><b>2.1 Earthworks</b></p> <p><i>Building work should be designed to respond to the natural topography of the site wherever possible, minimising the extent of cut and fill.</i></p> <p><i>Building work must be designed to ensure minimal cut and fill is required for its construction phase.</i></p> <p><i>All retaining walls are to be of masonry construction (or the like).</i></p> <p><i>All retaining walls proposed are to be identified in the development application.</i></p> <p><i>The maximum height of a single retaining wall is 1 metre.</i></p> <p><i>All land forming operations should involve the use of clean fill (also known as Virgin Excavated Natural Material or 'VENM').</i></p>	<p>As noted in the base of this report, a separate DA has been approved over the development site, which includes the carrying out of bulk earthworks. To achieve a generally flat pad and to accommodate the proposed building footprint, minor cut and fill works are also required as part of this proposal. The extent of earthworks required to accommodate the proposed development is considered to be appropriate for the subject site.</p> <p>As noted above, the proposed built form has been designed to respond to the natural topography of the site and the existing conditions. As such, the extent of cut and fill works are considered appropriate for the subject site.</p> <p>Retaining walls are sought across the development site and are proposed to consist of masonry construction.</p> <p>The proposed location of retaining walls is clearly shown on the engineering plans (where relevant).</p> <p>The retaining walls sought across the development site seek a maximum building height of 650mm.</p> <p>A standard condition can be included in any consent granted to ensure that any external fill transported on site is clean.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes – Subject to a condition of consent.</p>
<p><b>2.2 Salinity Management</b></p> <p><i>Groundwater recharge is to be minimised.</i></p> <p><i>All sediment and erosion controls are to be installed prior to the commencement of any works and maintained throughout the course of construction until disturbed areas have been revegetated/ established.</i></p>	<p>The drainage system is proposed to be directed into lined stormwater drains that are sought across the development site. Further, soft landscaping is sought around the boundaries of the subject site which will further contribute to minimising groundwater recharge.</p> <p>A standard condition can be included in the consent to ensure that the provision of sediment and erosion control measures are implemented prior to the commencement of works.</p>	<p>Yes</p> <p>Yes – Subject to a condition of consent.</p>
<p><b>2.3 Water Management</b></p> <p><i>All development must demonstrate compliance with the relevant provisions of Council's Engineering Specifications including requirements for detention, drainage and water sensitive urban design.</i></p>	<p>Councils engineers have raised some concerns regarding the likely safety impacts of motorists due to the provision of the dish crossing over the slip lane. It was recommended that a 3% crossfall be established to match the existing kerb and gutter (via Dick Johnson Drive). This was not provided. In addition to this, sufficient evidence has not been provided to demonstrate that proposed amendments of drainage lines on Dick Johnson Drive will not adversely impact the gutter flow capacity on Dick Johnson Drive. Given the potential safety impacts and insufficient information provided with the DA, the development is considered to be inconsistent with this planning control.</p>	<p><b>No – The development is not compliant.</b></p>
<p><b>2.4 Trees and Vegetation</b></p> <p><i>A person must not cut down, fell, uproot, kill, poison, ringbark, burn or otherwise destroy a tree or vegetation without approval from Council authorising such works.</i></p>	<p>As noted in the base of this report, the site is currently clear of any vegetation and tree planting.</p>	<p>Not applicable as the site is clear.</p>
<p><b>2.12 Acoustic Amenity</b></p> <p><i>Acoustic reports (where required), must be prepared by a suitably qualified consultant.</i></p>	<p>The application was accompanied by an Acoustic Report, prepared by Renzo Tonin who are a qualified acoustic consultancy. The acoustic report was reviewed by Councils Environmental Health Officer where it was agreed that the subject proposal would comply with the appropriate noise mitigation measures and the adopted noise criteria. Ongoing compliance can be assured via conditions of consent.</p>	<p>Yes</p>

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Attachment 4

## Camden Development Control Plan 2019 (Camden DCP) Assessment Table

<p>Noise attenuation measures must not adversely impact upon passive surveillance, active street frontages and energy efficiency.</p>	<p>Glass attenuation walls are proposed along the southern boundary (frontage to Dick Johnson Drive) to ensure passive surveillance and achieving an active street frontage is not compromised.</p>	
<p><b>2.13 Air Quality and Odour</b> Development that is likely to result in the emission of atmospheric pollutants, including odours, as determined by Council must include operating practices and technology to ensure that such emissions are acceptable. Details of these measures are to be provided at development application stage.</p>	<p>It is noted that proposed activities within the Brewhouse and Smokehouse is likely to produce odour as a result to their production process. The DA was accompanied by an Odour Impact Assessment which detailed recommendations for ongoing treatment to prevent any adverse impacts on surrounding land uses. The application was reviewed by Councils Environmental Health Officer, where it was confirmed that most areas of concern could be controlled via a condition of consent.</p>	<p>Yes – Subject to a condition of consent.</p>
<p><b>2.14 Waste Management</b> A Waste Management Plan (WMP) must be submitted for all new development, including demolitions, construction and the ongoing (or change of) use.</p>	<p>A waste management plan has been submitted with the DA, noting that all waste is proposed to be collected by a private contractor. The provided waste management plan denotes the proposed location of waste and recycling areas, the proposed collection point and a path of travel for collection vehicles. However, the provided swept path diagrams indicate that the waste truck will encroach over Car Parking Space No. 124 and 125. A condition can be included in any consent granted which will restrict waste collection outside the proposed operation hours. Subject to the imposition of a condition, the development would be compliant with this control.</p>	<p>Yes – Subject to a condition of consent.</p>
<p><b>2.17 Signage</b> <b>2.17.1 General Requirements for Signage</b> The location, quantity, type, colour, design and size of all signage must not detract from the amenity and character of the land or building to which it relates.</p> <p>All signage must be consistent with the scale of the building or the property on which it is located.</p> <p>All signage must align with an approved or exempt land use being conducted on the land to which the sign is displayed.</p> <p>All signage must remain within the property boundary except in the case of a sign attached to an awning over the footpath.</p> <p>Signage must not interfere with road and pedestrian safety and must adhere to the controls in the Camden DCP.</p>	<p>As noted as a separate attachment to this assessment, the provision of 12 business identification signs are sought across the development site. Proposed signs generally consist of neutral colours and is of an appropriate size given the scale of the overall development. As the site and surrounding allotments are projected for business and commercial uses, proposed signs are considered to be compatible with the desired future character of the area and the wider locality.</p> <p>Proposed signs have been designed to reflect the projected scale and business character of the site and surrounding allotments.</p> <p>As noted, development consent is sought for all 12 business identification signs across that seek to advertise the proposed development.</p> <p>All signs are located wholly within the property boundaries.</p> <p>Given the size of the development, it is unlikely the sign will obscure any sightlines from public areas or have any adverse impacts the safety of pedestrians and motorists.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p>
<p><b>2.17.2 Commercial and Mixed Use Zones</b> The total combined signage area on a building elevation must not exceed 20% of that building elevation that is visible from a public place.</p> <p>With the exception of under awning signs, all signs must be located wholly within the property boundaries.</p> <p>All Illumination signage must comply with AS 1158 - Lighting for Roads and Public Spaces and AS 4282 - Control of the Obtrusive Effects of Outdoor Lighting.</p> <p>A maximum of one pole or pylon sign per street frontage, not exceeding 6m above existing ground level is permitted.</p>	<p>Signs sought on the external walls of the building will not exceed 20% of that building elevation. As such, the signs are unlikely to dominate the streetscape via the public domain.</p> <p>As noted, all signs are located wholly within the property boundaries.</p> <p>Compliance with this control can be assured via a condition of consent.</p> <p>One pylon sign is sought on the southern elevation, via Dick Johnson Drive.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p>

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Attachment 4

## Camden Development Control Plan 2019 (Camden DCP) Assessment Table

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Attachment 4

<p><b>2.18 Traffic Management and Off-Street Car Parking</b>  <b>2.18.2 Off Street Car Parking Rates / Requirements</b></p> <p><b>Retail Premises / Shop / Kiosk</b>  <i>Less than 200m<sup>2</sup> – 1 car parking space per 30m<sup>2</sup> GFA.</i></p> <p><b>Pub / Registered Club / Restricted Premises</b>  <i>A detailed car parking submission is required.</i></p> <p><i>Provision for coaches to pick up and set down may also be required for large establishments.</i></p> <ul style="list-style-type: none"> <li>- 1 car parking space per 2m<sup>2</sup> GFA of public bar area and</li> <li>- 1 car parking space per 5m<sup>2</sup> GFA of lounge, beer garden, auditorium, billiard room, restaurant and</li> <li>- 25 car parking spaces per 100m<sup>2</sup> of remaining public floor area.</li> <li>- Developments must also accommodate:</li> <li>- 1 bicycle space per 25 car parking spaces in excess of the first 25 car parking spaces; and</li> <li>- 1 motorcycle space per 50 car parking spaces in excess of the first 50 car parking spaces.</li> </ul>	<p>As noted in the base of this report, the application proposes a variation to the minimum car parking rates as required by this control. Pursuant to the rates in the Camden DCP, the development generates a demand for 614 car parking spaces. The proposed development seeks the provision of 127 car parking spaces, contravening the development standard by 487 spaces. The DA was accompanied by a Traffic and Parking Assessment, which includes a written request to support a variation to this planning control. Specifically, the written request justifies that a car parking rate of 8.7 spaces per 100m<sup>2</sup> of public floor area would be most appropriate for the site. This was justified by preparing a parking assessment against two comparable sites being the Marsden Park Brewhouse and the Gregory Hills Hotel. A review by Council staff and an external independent expert has noted a number of concerns, specifically in relation to:</p> <ul style="list-style-type: none"> <li>- The identified public floor areas across the development site. In particular, areas such as the gaming room and brewhouse should be considered as public floor area rather than ancillary spaces.</li> <li>- The number of comparable sites considered in the parking assessment to determine the more 'appropriate' rate.</li> </ul> <p>Due to the above findings and the significant shortfall in parking, Council staff do not support the proposed variation. For this reason, it is recommended that the application be refused.</p>	<p><b>No – The development is not compliant.</b></p>
<p><b>2.19 Landscape Design</b>  <i>A landscape plan is to be submitted for all development that, in Council's opinion, will significantly alter the existing and intended landscape character of the land.</i></p>	<p>The DA was accompanied by detailed landscape plans however they are not reflective of the revised design. If the DA is to be approved, a condition must be included in the consent, which requires updated landscape plans to be prepared, prior to the issue of a Construction Certificate.</p>	<p><b>No, however can be resolved via a condition of consent.</b></p>



**PROPOSED NEW PUB DEVELOPMENT  
AT ORAN PARK**

**TRAFFIC AND PARKING ASSESSMENT**

21 June 2021  
Ref: 17007

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Attachment 5

## 1. Introduction

This report has been prepared to accompany a development application to Camden Council for the proposed development of a new pub located on the north-eastern corner of Dick Johnson Drive and Steward Drive at Oran Park. The site location is shown on the Oran Park Town Masterplan in the following pages.

The proposed development site is located in the Oran Park Precinct: Employment Area in the north-western periphery of the Oran Park Town Centre. It occupies Tranche TR22 in the Oran Park Precinct and the land is zoned IN1 – General Industrial under SEPP (*Sydney Regional Growth Centre*) 2006. Pursuant to a new Planning Proposal, the site is currently being rezoned to B5 – Business Development consistent with the zone which applies to the adjoining land to the east. The site is proposed Lot 2201 and has a total area of 9293m<sup>2</sup> with frontages (including splay) of approximately 95m to Dick Johnson Drive, approximately 110m to Steward Drive, and approximately 80m to Future Road (North). A Survey Plan of the site is reproduced in Appendix A.

There is no existing development on the proposed development site.

The proposed pub is a single level building with a total gross floorarea of approximately 2598.9m<sup>2</sup>, and a total *public floorarea*<sup>1</sup> of approximately 2438.2m<sup>2</sup> comprising 1735.9m<sup>2</sup> of indoor floorspace and 702.3m<sup>2</sup> of outdoor floorspace, disaggregated as follows:

ORAN PARK PUB – PUBLIC FLOORAREA (m <sup>2</sup> )		
<b>1</b>	<b>INDOOR</b>	
	▪ Foyer	83.1m <sup>2</sup>
	▪ Bistro	665.0m <sup>2</sup>
	▪ Private Dining	37.4m <sup>2</sup>
	▪ Sports Bar and TAB	330.5m <sup>2</sup>
	▪ Gaming Room	265.1m <sup>2</sup>
	▪ Brew House	303.7m <sup>2</sup>
	▪ Smoke House	51.1m <sup>2</sup>
	<i>Sub Total Indoor</i>	<i>1735.9m<sup>2</sup> (71%)</i>
<b>2</b>	<b>OUTDOOR</b>	
	▪ Outdoor Dining Terrace	94.4m <sup>2</sup>
	▪ Kids Outdoor Area	87.9m <sup>2</sup>
	▪ Garden Terrace	385.0m <sup>2</sup>
	▪ Sports Terrace	135.0m <sup>2</sup>
	<i>Sub Total Outdoor</i>	<i>702.3m<sup>2</sup> (29%)</i>
<b>3</b>	<b>TOTAL</b>	<b>2438.2m<sup>2</sup></b>

<sup>1</sup> The term **public floorarea** is used to describe areas generally available to pub patrons when participating in pub activities. It includes bars, lounges, gaming rooms, billiard rooms, TAB areas, dining areas, function rooms, etc but excludes behind bar/server areas, amenities, food preparation areas, office floorspace, etc.

JOHN COADY CONSULTING  
Townplanning and Traffic Consultant

Provision is made for a drive-through bottle shop on the northern side of the pub building with a *public floorarea* of approximately 50m<sup>2</sup>, although the bottle shop is not included in the table on the previous page because its patrons can also include persons who are not patrons of the proposed pub.

The proposed pub is served by a total of 132 off-street parking spaces in 5 carparks:

Upper Level Parking	33 spaces
Lower Level Parking	46 spaces
North-Eastern Parking	6 spaces
North-Western Parking	28 spaces
South-Eastern Parking	19 spaces
<b>Total</b>	<b>132 spaces</b>

Additional provision is made for 44 bicycle and 8 motor bike dedicated parking spaces.

Vehicular access for the proposed pub is via a left-turn *entry only* driveway off Dick Johnson Drive preceded by a deceleration lane 25m long, a combined entry/exit driveway off Future Road (North), and a left-turn *exit only* driveway to Steward Drive.

A loading area is located at-grade on the northern side of the proposed pub building. The largest truck which will be involved in making deliveries to the proposed pub is a 12.5m long heavy rigid vehicle, and the turning path of this truck accessing the loading area is included in Appendix A. The turning path of the 85<sup>th</sup> percentile vehicle accessing the bottle shop is also shown on that Drawing (see Appendix A).

Relevant plans of the proposed pub prepared by Archebiosis Design, and turning path diagrams showing vehicular access to, and circulation within, the site are included in Appendix A.

The hours of operation of the proposed pub are:

Monday – Saturday	7.00am – 3.00am
Sunday	7.00am – 12 midnight

The proposed pub is expected to employ a typical maximum workforce of approximately 15 persons on the site at any time, increasing up to 30 persons on Friday/Saturday nighttime if live entertainment is provided, with the following typical attendance pattern (ie number of staff on the premises):



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WORKFORCE CATEGORY	DAYTIME			NIGHTTIME		
	Mon-Thu	Fri-Sat	Sun	Mon-Thu	Fri-Sat	Sun
Manager & Admin	4	3	2	3	2	1
Bar/Dining	5	6	7	7	10-18	6
Cleaning	2	2	2	-	2-5	-
Security	-	-	-	1	2-5	2
<b>TOTAL</b>	<b>11</b>	<b>11</b>	<b>11</b>	<b>11</b>	<b>16-30<sup>1</sup></b>	<b>9</b>

<sup>1</sup>If live entertainment is provided on Friday and Saturday nights, the bar/dining and security staff will be increased such that the total number of staff on the premises will increase to 30 persons

The *maximum permitted* patronage to be accommodated in the proposed pub is 850 persons.

The purpose of this report is to assess the traffic and parking implications of the proposed pub.

CLPP01

Attachment 5

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Attachment 5  
CLPP01



## 2. Parking Assessment

Table 2.5 – *Schedule of Cars, Bicycle and Motorcycle Parking Requirements* in Chapter 2.18.2 of *Camden Development Control Plan 2019* specifies the following off-street parking requirement for pubs/registered clubs/restricted premises:

A detailed carparking submission is required.

Provision for coaches to pick up and set down may also be required for large establishments.

1 carparking space per 2m<sup>2</sup> GFA of public bar area; and

1 carparking space per 5m<sup>2</sup> GFA of lounge, beer garden, auditorium, billiard room, restaurant; and

25 carparking spaces per 100m<sup>2</sup> of remaining public floor area.

Developments must also accommodate:

1 bicycle space per 25 carparking spaces in excess of the first 25 carparking spaces; and

1 motorcycle space per 50 carparking spaces in excess of the first 50 carparking spaces.

That parking requirement specified by Council's DCP is excessive and inappropriate for the proposed pub because:

- the requirement of 1 space per 2m<sup>2</sup> GFA for public bar areas substantially overstates the parking demand potential of those areas in contemporary pubs. In fact, it would be surprising if the patron density in any part of the proposed pub would be in the order of 1 carparking space per 2m<sup>2</sup>, let alone the patron parking demand potential. Similarly, while a patron density of 1 person per 5m<sup>2</sup> GFA is appropriate for some parts of the proposed pub at different times of the day and night, it is unlikely that the overall patron density of the proposed pub would be 1 person per 5m<sup>2</sup> *public floorarea* under *routine* trading conditions at any time. The only time that such intensive patron densities are likely to be experienced is on limited occasions when the *maximum permitted* patronage is achieved
- by way of example, as noted in the Introduction to this report the proposed pub is to have a *maximum permitted* patronage of 850 persons. If the general parking rate of 1 carparking space per 5m<sup>2</sup> GFA is applied to the total *public floorarea* of the proposed pub (2438.2m<sup>2</sup>), the consequent parking requirement is 488 spaces, equivalent to a cardriver rate of 57%. While it is not impossible for the proposed pub to experience a patron cardriver rate in the order of 57% from time to time, that will certainly not happen at times when the *maximum permitted* patronage of 850 persons is experienced.

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That type of patronage will only be experienced late at night, and only if live entertainment is provided

- the parking requirement specified by Council's DCP is "out of date" in that it assumes that all parts of the proposed pub will be strongly patronized at all times. In fact, contemporary pubs such as this proposal provide a range of activities, not dissimilar to clubs, and many pub patrons participate in more than one activity during the course of one visit. Accordingly, it is highly unlikely that, say, the dining areas of the pub will be filled to capacity at the same time as, say, the Brew House is filled to capacity. Relevantly, the RTA Guidelines<sup>2</sup> do not specify a parking requirement for clubs but recommend that comparisons be made with other clubs.

The RTA Guidelines identify the typical parking demand characteristics for a variety of landuse and development which are typically used in the preparation of Parking Assessment reports. However, the Guidelines do not provide information on the typical parking demand characteristics of pubs, noting instead that:

*"The RTA's research on parking has found no strong relationship between peak carparking accumulation and floorarea, or function room capacity, at 10 pub sites surveyed.*

*The range in parking demand rates resulting from early research was broad, making it difficult to generalize. This variation was due to factors such as the location and age of the building, the internal design, the provision of live music and other facilities. Since the surveys were undertaken, behavioral changes have occurred in the use of pubs, due to factors such as the introduction of random breath testing. These changes have generally served to reduce parking demand rates. **It is recommended that proposed pub developments be compared to similar existing developments, noting the existing supply of, and demand for parking in the area, and the peak parking periods of individual facilities within the pub. (Emphasis added)***

*When a proposed development includes a function room for live music performances (or a nightclub), particular attention must be paid to parking requirements to meet peak demands".*

It is generally accepted that for design purposes the parking demand potential of a pub should be based on the *average maximum* operating condition, and the RTA Guidelines specify that requirement for the purposes of estimating the parking demand potential of licensed clubs. It is reasonable to assume that the same criteria would apply to the determination of an

<sup>2</sup> RTA "Guide to Traffic Generating Developments. Section 5 – Parking Requirements for Specific Landuse" October 2002

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appropriate parking requirement for pubs. While the RTA Guidelines do not define how to calculate the *average maximum* parking demand potential, it is generally accepted that this should be based on the 85<sup>th</sup> percentile patronage condition.

John Coady Consulting, and its predecessor companies John Coady Consulting Pty Ltd and Project Planning Associates, have prepared Traffic and Parking Assessment reports for a large number of pub and club development/redevelopment projects. That experience has revealed that the best approach to determining an appropriate parking requirement for pubs is to:

- i). Estimate the *average maximum* patron accumulation in the premises. The *average maximum* patron accumulation is that accumulation which is exceeded for only a few hours per week.
- ii). Apply a typical patron cardriver rate to the *average maximum* patron accumulation to determine the *average maximum* patron parking demand.
- iii). Estimate the workforce parking demand and add that to the *average maximum* patron parking demand to identify the total *average maximum* parking demand of the development.

As noted in the Introduction to the report, the *maximum permitted* patronage to be accommodated in the proposed pub is 850 persons. A maximum patronage of this level is unlikely to be achieved on a continuous basis, and is most likely to be achieved on only a limited number of occasions throughout the year, most likely at nighttime. A maximum patronage of 850 persons was therefore adopted as the *maximum permitted* patronage for the proposed pub.

The maximum weekly patronage is likely to be achieved during the nighttime dining period (say between 6.00 – 8.00pm) on Thursday, Friday and Saturday nights. Nighttime patronage can be expected to be lower on other nights of the week, as can daytime patronage. Weekday daytime pub patronage can be expected to peak around the lunchtime dining period (Midday – 2.00pm), and be lower outside that peak period.

An appropriate method of estimating the *average maximum* patron accumulation for the proposed pub is to review the *average maximum* patron accumulations, and the underlying patron densities, of other pubs and select the patron density most appropriate for the proposed

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pub taking into account its particular circumstances. The selected patron density can then be applied to the *public floorarea* of the proposed pub to determine its *average maximum* patronage. In this case, the particular circumstances that should influence selection of the appropriate patron density concern the relatively large size of the pub, the relatively high proportion of outdoor *public floorarea* where patronage can be affected by climactic conditions, the diversity of patron activity areas in the pub, and the live entertainment that will most likely be provided at the pub on Thursday, Friday or Saturday nights.

The *average maximum* patron accumulations and underlying patron densities are listed on the AVERAGE MAXIMUM PATRON ACCUMULATIONS table reproduced in the following pages for a selection of pubs surveyed by John Coady Consulting Pty Ltd/Project Planning Associates Pty Ltd. The *average maximum* patron accumulation and underlying patron densities are shown for the weekday and weekend day, daytime and nighttime trading condition. As can be observed, none of the surveyed pubs had a *public floorarea* of the same size as that of the proposed pub (2438.2m<sup>2</sup>), the closest being the Mona Vale Hotel (1622m<sup>2</sup>) suggesting that the *average maximum* patron accumulation and underlying patron density characteristics of that pub is likely to provide the most accurate indication of the patron density and *average maximum* patron accumulation characteristics of the proposed Oran Park Pub.

Relevantly, the *average maximum* patron accumulation rates recorded for the Mona Vale Pub tend to be significantly higher than those recorded for the other smaller pubs included in the table indicating that patron accumulation rates are typically higher (and patron density is typically lower) with increasing pub size (ie increasing *public floorarea*). These circumstances suggest that the patron accumulations in the proposed Oran Park Pub should relate more closely to the patron accumulation rates recorded for the Mona Vale Pub than to other pubs in the survey sample.

The survey of the operating characteristics of the Mona Vale Pub also provides data which is particularly relevant to the proposed Oran Park Pub in that the surveys were conducted over an 8 day trading period between Saturday 27 February – Saturday 5 March 2016. The surveys were taken over the 8 day period instead of the usual 7 day (one week) period to compile information on patronage and travel characteristics on 2 Saturday nights:

- i) Saturday 27 February, when live entertainment in the form of a particularly popular DJ performed between 2.00pm – 1.00am attracting *unusually* heavy patronage, with a peak

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patronage of 921 persons recorded around 10.00pm on that night. The results of the survey conducted on Saturday 27 February at the Mona Vale Pub are particularly relevant to the operating characteristics of the proposed Oran Park Pub which has a *maximum permitted* patronage of 850 persons.

- ii) Saturday 5 March, when *normal* Saturday patronage peaking at 101 persons at 11.00pm was experienced.

Because the results of the survey conducted at the Mona Vale Pub are so relevant to the operating characteristics of the proposed Oran Park Pub, the detailed results of the patron accumulation and interview surveys conducted at the Mona Vale Pub are reproduced in Appendix B to this report.

The patron densities selected for the weekday/weekend, daytime/nighttime periods and the *maximum permitted* patronage trading condition can then be applied to the total *public floorarea* of the proposed pub (2438.2m<sup>2</sup>) to determine the *average maximum* patron accumulation for those periods for the proposed pub. The estimated *peak* patron accumulation can be derived from the *average maximum* patron accumulations on the basis that they represent approximately 85% of the *peak* patron accumulation. This calculation is set out in the projected *average maximum* and *peak* patron accumulation tables.

AVERAGE MAXIMUM PATRON ACCUMULATIONS									
Pub	Total Public Floorarea (m <sup>2</sup> )	Average Maximum Patron Accumulations							
		Daytime				Nighttime			
		Weekday		Weekend		Weekday		Weekend	
		No.	Rate	No.	Rate	No.	Rate	No.	Rate
Woollahra Hotel, Queen/Moncur St Woollahra	270	30	1:9.0	50	1:5.4	145	1:1.9	135	1:2.0
Oxford Tavern, William/Piper Streets, Bathurst	430	15	1:28.7	80	1:5.4	20	1:21.5	80	1:5.4
Centennial Hotel, Oxford/Victoria Streets	500	50	1:10.0	100	1:5.0	140	1:3.6	150	1:3.3
PJ Gallaghers, 13-17 Lyons Road, Drummoyne	568	40	1:14.2	60	1:11.4	120	1:4.7	70	1:8.1
New Brighton Hotel, Manly	700	50	1:14.0	60	1:11.6	120	1:5.8	140	1:5.0
Oatley Hotel, Oatley Avenue, Oatley	850	30	1:28.3	80	1:10.6	140	1:6.0	240	1:3.5
Terrigal Hotel, Terrigal Dr/Havenview Rd, Terrigal	906	60	1:15.1	100	1:9.1	140	1:6.5	140	1:6.5
Mona Vale Hotel, 2 Park Street, Mona Vale	1622	50	1:32.4	75	1:21.6	110	1:14.8	80	1:20.3

PUB PATRON CAR DRIVER RATES					
Pub	Total Public Floorarea (m <sup>2</sup> )	Patron Car Driver Rate			
		Daytime		Nighttime	
		Weekday	Weekend	Weekday	Weekend
Woollahra Hotel, Queen/Moncur St Woollahra	270	27.7%	21.7%	19.2%	23.4%
Oxford Tavern, William/Piper Streets, Bathurst	430	42.1%	27.2%	27.2%	15.6%
Centennial Hotel, Oxford/Victoria Streets	500	21.8%	17.9%	19.0%	21.0%
New Brighton Hotel, Manly	700	9.2%	10.9%	11.6%	9.6%
Oatley Hotel, Oatley Avenue, Oatley	850	52.9%	40.4%	37.4%	30.3%
Terrigal Hotel, Terrigal Dr/Havenview Rd, Terrigal	906	50.8%	42.9%	39.3%	28.5%
Mona Vale Hotel, 2 Park Street, Mona Vale	1622	50.0%	40.4%	43.8%	18.5%

PROJECTED AVERAGE MAXIMUM AND PEAK PATRON ACCUMULATIONS FOR THE PROPOSED PUB					
	Daytime		Nighttime		<i>Maximum Permitted Patronage – 850 Persons</i>
	Weekday	Weekend	Weekday	Weekend	
Patron Density	1:20m <sup>2</sup>	1:15m <sup>2</sup>	1:10m <sup>2</sup>	1:8m <sup>2</sup>	1:3.4m
<i>Average Maximum Patron Accumulation</i>	120	165	245	305	720
<i>Peak Patron Accumulation</i>	145	190	285	360	850

Once again, a guide to the appropriate patron car driver rate for the proposed pub is provided by the patron car driver rates revealed by the surveys of a sample of pubs, modified and corrected to reflect the particular circumstances of the proposed pub. The patron car driver rates identified by the surveys of a sample of hotels are listed in the PUB PATRON CAR DRIVER RATES table reproduced in the foregoing.

The patron car driver rates for the various time periods adopted for the proposed pub are:

Daytime		Nighttime	
Weekday	50%	Weekday	40%
Weekend	45%	Weekend	30%
		720**/850* persons	15%

\*Maximum permitted patron accumulation;

\*\*Equivalent average maximum permitted patron accumulation.

The patron parking demand potential of the proposed pub is calculated below by applying the adopted patron car driver rates to the adopted *average maximum* and *peak* patron accumulations for the various time periods:

PATRON PARKING DEMAND POTENTIAL		
DAYTIME	WEEKDAY	WEEKEND
<i>Peak Attendance</i>	145 persons x 50% = 75 vehicles	190 persons x 45% = 85 vehicles
<i>Average Maximum Attendance</i>	120 persons x 50% = 60 vehicles	165 persons x 45% = 75 vehicles
NIGHTTIME		
<i>Peak Attendance</i>	205 persons x 40% = 115 vehicles	360 persons x 30% = 110 vehicles
<i>Average Maximum Attendance</i>	285 persons x 40% = 100 vehicles	305 persons x 30% = 95 vehicles
MAXIMUM PERMITTED PATRON ACCUMULATION – 850 PERSONS		
<i>Peak Attendance</i>	850 persons x 15% = 130 vehicles	850 persons x 15% = 130 vehicles
<i>Average Maximum Attendance</i>	720 persons x 15% = 110 vehicles	720 persons x 15% = 110 vehicles

Also, allowance should be made for workforce parking demand. The total parking demand potential of the proposed pub, including workforce parking, is therefore:



TOTAL PARKING DEMAND POTENTIAL						
	Patron Parking Demand Potential		Workforce Parking Demand Potential		Total Parking Demand Potential	
	Weekday	Weekend	Weekday	Weekend	Weekday	Weekend
<b>DAYTIME</b>						
<i>Peak Attendance</i>	75	85	5	5	80	90
<i>Average Maximum Attendance</i>	60	75	5	5	65	80
<b>NIGHTTIME</b>						
<i>Peak Attendance</i>	115	110	10	15	125	125
<i>Average Maximum Attendance</i>	100	95	10	15	110	110
<b>MAXIMUM PERMITTED PATRON ACCUMULATION – 850 PERSONS</b>						
<i>Peak Attendance</i>	130	130	20	20	150	150
<i>Average Maximum Attendance (Nighttime)</i>	110	110	20	20	130	130

As noted in the foregoing, the proposed Oran Park Pub will have access to a total off-street parking provision of 132 spaces. That off-street parking provision should be adequate to accommodate the projected *peak* and *average maximum* parking demand potential of the expanded pub at all relevant times, except for the limited number of times per year when the *peak* attendance of 850 persons is experienced. This is expected to occur at nighttime when a parking shortfall of up to 16 parking spaces is indicated. Notwithstanding, on the limited number of times per year when the *average maximum* peak nighttime patron accumulation of 720 persons is experienced, the *average maximum* parking demand potential of 130 spaces would be satisfied by the off-street parking provision of 132 parking spaces.

The proposed pub also includes a bottle shop which has both a *gross and public floorarea* of 50.m<sup>2</sup>. That bottle shop is served by a drive-through facility which makes provision for an *additional 2* off-street parking spaces. Although the bottle shop can be expected to draw a high proportion of its patronage from patrons of the proposed pub, it is available to the general public and can therefore attract some patrons who are not also patrons of the proposed pub. *Table 2.5: Schedule of Car, Bicycle and Motorcycle Parking Requirements* in Chapter 2.18.2 of *Camden Development Control Plan 2019* specifies a parking requirement of 1 carparking space per 30m<sup>2</sup> GFA, plus 1 bicycle space per 25 carparking spaces in excess of the first 25 carparking spaces, and 1 motorcycle space per 50 carparking spaces in excess of the first 50 carparking spaces. Application of those parking requirements to the bottle shop incorporated in the proposed pub development (50m<sup>2</sup>) yields a parking requirement of 1.7 carparking spaces, plus 5 bicycle parking spaces, plus 2 motorcycle parking spaces. However, in circumstances where a significant proportion of the car/bicycle/motorcycle parking demand potential of the bottle shop can be expected to be drawn from patrons of the proposed pub (say 50%), the *additional* parking requirement for the bottle shop reduces to 1

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carparking space, plus 2-3 bicycle parking spaces, plus 1 motorcycle parking space. Relevantly, the additional carparking requirement for the proposed bottle shop is satisfied by the 2 parking spaces located in the drive-through facility, while the bicycle and motorcycle parking requirement represents the parking requirement for the whole of the proposed pub. In these circumstances, the bottle shop incorporated in the proposed pub development has no significant parking implications.

Also, the proposed development site has frontages (including splays) of approximately 95m to Dick Johnson Drive, approximately 110m to Steward Drive and approximately 110m to Future Road (North) and it is inevitable that if kerbside parking is permitted on those street frontages patrons of the proposed pub will park in those parking spaces. Because the proposed pub is located within a zoned industrial area, it is likely that on-street parking will be available, particularly during the nighttime and during the daytime on weekends, relevantly the times when the parking demand potential of the proposed pub is likely to be greatest. While it is unlikely that kerbside parking will be permitted on Dick John Drive, there appears to be no sound reason why it would not be permitted on the Future Road frontages of the site. If that is the case, it is likely that up to an additional 20 parked vehicles could be accommodated in those parking spaces. While it is not suggested that these on-street parking spaces can be considered to form part of the proposed pub's parking provision, they do provide a useful *factor-of-safety* during periods of heavy parking demand by the proposed pub.

In the circumstances it can be reasonably concluded that the proposed pub has no unacceptable parking implications.

### 3. Public Transport, Walking and Cycling

A comprehensive description of the public transport, walking and cycling arrangements which are/will be available to serve the Oran Park Precinct: Employment Area in which the proposed pub is located is set out in the report:

AECOM "*Oran Park Precinct: Employment Area – Traffic Report*" 08 March 2019

Chapter 2.5 – *Public Transport* of that report provides details of public transport services which are/will be available to serve the Employment Area and therefore the proposed pub, while Chapter 2.6 – *Walking and Cycling* provides details of the pedestrian/bicycle network which is/will be available to serve the precinct and the proposed pub. Those chapters of the AECOM report are reproduced in Appendix C to this report for convenience.

As can be observed, the proposed pub enjoys convenient access to the existing/proposed bus routes planned to serve the Oran Park Town Masterplan Area generally, and the Oran Park Precinct: Employment Area in particular with the Oran Park DCP proposed route which runs along the Dick Johnson Drive frontage of the proposed pub site. Similarly, the proposed pub site enjoys convenient access to pedestrian/cycleway infrastructure planned for the Oran Park Town Masterplan with a pedestrian/cycleway route located within the Dick Johnson Road Reserve which runs along the frontage of the proposed pub site.

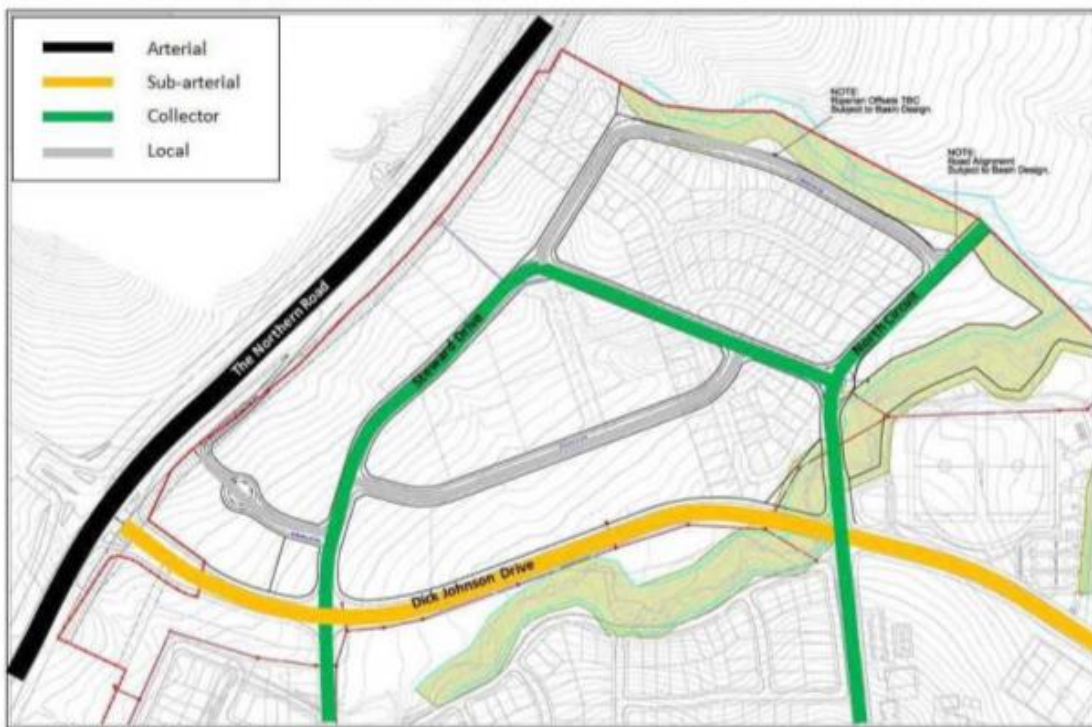
In summary, the proposed pub site enjoys convenient access to public transport, walking and cycling arrangements planned to serve the Oran Park Town Masterplan in general, and the Oran Park Precinct: Employment Area in particular.

## 4. Traffic Assessment

### Existing/Proposed Road Network

The existing/proposed road network to serve the Oran Park Precinct: Employment Area generally and including the proposed pub site located on the north-eastern corner of Dick Johnson Drive and Steward Drive is shown on Figure 4 – *Oran Park Employment Area Road Network* which forms part of the AECOM report, and a copy of that drawing is reproduced below.

Figure 4 Oran Park Employment Area road network



Source: Design + Planning, 2019; modified by AECOM, 2019

As can be observed, the proposed pub site enjoys convenient access to the higher order road network planned to serve the Employment Area in the form of:

- Dick Johnson Drive which is a designated sub-arterial road which is planned as a 4-lane carriageway providing an east-west link through the Oran Park Precinct. It provides access to the wider, strategic road network via connections to The Northern Road (to the west), Leppington (to the north-east), and Camden Valley Way (to the east). It will also accommodate bus services to the Town Centre and surrounding areas through bus stops located at the eastern end of the Employment Area

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- Steward Drive which is a designated collector road planned as a 2-lane carriageway providing a north-south link between Peter Brock Drive and Dick Johnson Drive. Steward Drive (north of Dick Johnson Drive) will primarily serve industrial vehicles and people employed within the area, connecting them to the sub-arterial road network.

The main feature of the existing/planned traffic controls on the road network serving the proposed pub site is the roundabout located at the intersection of Dick Johnson Drive and Steward Drive. As noted in the foregoing, vehicular access for the proposed pub is via a left-turn *entry-only* driveway off Dick Johnson Drive preceded by a deceleration lane 25m long, and a combined entry/exit driveway off Future Road (north). Relevantly, the left-turn *entry-only* driveway off Dick Johnson Drive which will provide access for the proposed pub is located within close proximity (approximately 75-100m) of the roundabout at the intersection with Steward Drive such that vehicles approaching the proposed pub from the east will be able to make a U-Turn at the roundabout to enter the proposed pub site via the *entry-only* off Dick Johnson Drive. That *entry-only* driveway will also serve future industrial/business development on the adjacent site to the east.

#### ***Projected Traffic Generation Potential of the Proposed Pub***

Although the RTA Guidelines<sup>3</sup> specify typical traffic generation rates for a wide variety of landuse and development, they do not contain guidelines on the typical traffic generation characteristics of pubs noting instead that:

*“Original RTA research indicated a large variance in the traffic generation rates of hotels. This variation is due to factors such as the location and age of the building, its internal design, the provision of live music and other such facilities. Since these surveys were undertaken, some changes have occurred in the use of hotels, partly due to the introduction of random breath testing. These changes have generally reduced traffic generation rates of hotels. It is recommended that the analysis of proposed hotel development be based on surveys of similar existing hotels”.*

The main traffic implications of the proposed pub concern traffic that it generates during the weekday PM peak period. Pubs typically have no significant traffic generation potential during the weekday AM peak period, while traffic demand on the road network serving the proposed pub site (Dick Johnson Drive and Steward Drive) is likely to be relatively light during the nighttime and, to a lesser extent, during the weekend daytime.

<sup>3</sup> RTA “Guide to Traffic Generating Developments. Section 3 – Landuse Traffic Generation” October 2002

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Surveys of pub/club operations conducted by John Coady Consulting Pty Ltd/Project Planning Associates Pty Ltd indicate that pubs typically have a traffic generation potential during the weekday PM peak period in the order of 1.0 vtpm per parking space. Application of that rate to the *average maximum* and *peak* parking demand potential of the proposed development on weekdays and weekend days indicates that its traffic generation potential during the weekday PM peak period is in the order of 65-125 vtpm (say 100 vtpm), as calculated in the table below.

	Weekday Parking Demand Potential		Weekday PM Peak Period Traffic Generation Potential (vtpm)	
	Peak	Avg Maximum	Peak	Avg Maximum
Daytime	80	65	80	65
Nighttime	125	110	125	110

However, development of the proposed pub on the site will eliminate the potential for it to be used for Business Development purposes, and consequently also eliminate the traffic generation of that business use. An estimate of the development potential of the site for business use would be typically calculated by applying the maximum permitted floorspace ratio (FSR) to the total site area. In this case however the site is proposed to be zoned B5 Business Development and while that zone imposes a maximum height limit of 15m, it does not specify a maximum FSR. Notwithstanding, it is likely that development of the site for a business use would achieve a FSR of at least 1:1 yielding a GFA of 9293m<sup>2</sup>. The RTA Guidelines specify a weekday PM peak period traffic generation rate of 1.1 vtpm per 100m<sup>2</sup> of total GFA yielding a traffic generation potential for the alternative business development use in the order of 100 vtpm, that is the same weekday PM peak period traffic generation potential of the proposed pub.

In circumstances where the weekday PM peak period traffic generation potential of the proposed pub is the same as use of the site for business development, it can be reasonably assumed that there will be no significant difference in the traffic implications of the proposed pub and an alternative business development.

Relevantly, the CUBE traffic model which assessed the traffic implications of the whole of the Employment Area concluded that this development did not identify any road network capacity issues for any of the roads in the Employment Area in 2036. Specifically, the CUBE traffic model assessment of the Dick Johnson Drive/Steward Drive intersection under projected future traffic demand during the weekday PM peak period predicted a post-development operating performance for that intersection of LOS – A with an Average

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Vehicle Delay of 11.1 seconds and a Degree of Saturation of 0.394, representing *good* operating performance for the intersection under post-development (2036) traffic demand.

In circumstances where the use of the subject site for a pub has the same weekday PM peak period traffic generation potential as use of the site for business development, it can be reasonably assumed that the same conclusion would be reached if the proposed pub was developed on the site, in which case it can be reasonably concluded that the proposed pub has no unacceptable traffic implications.

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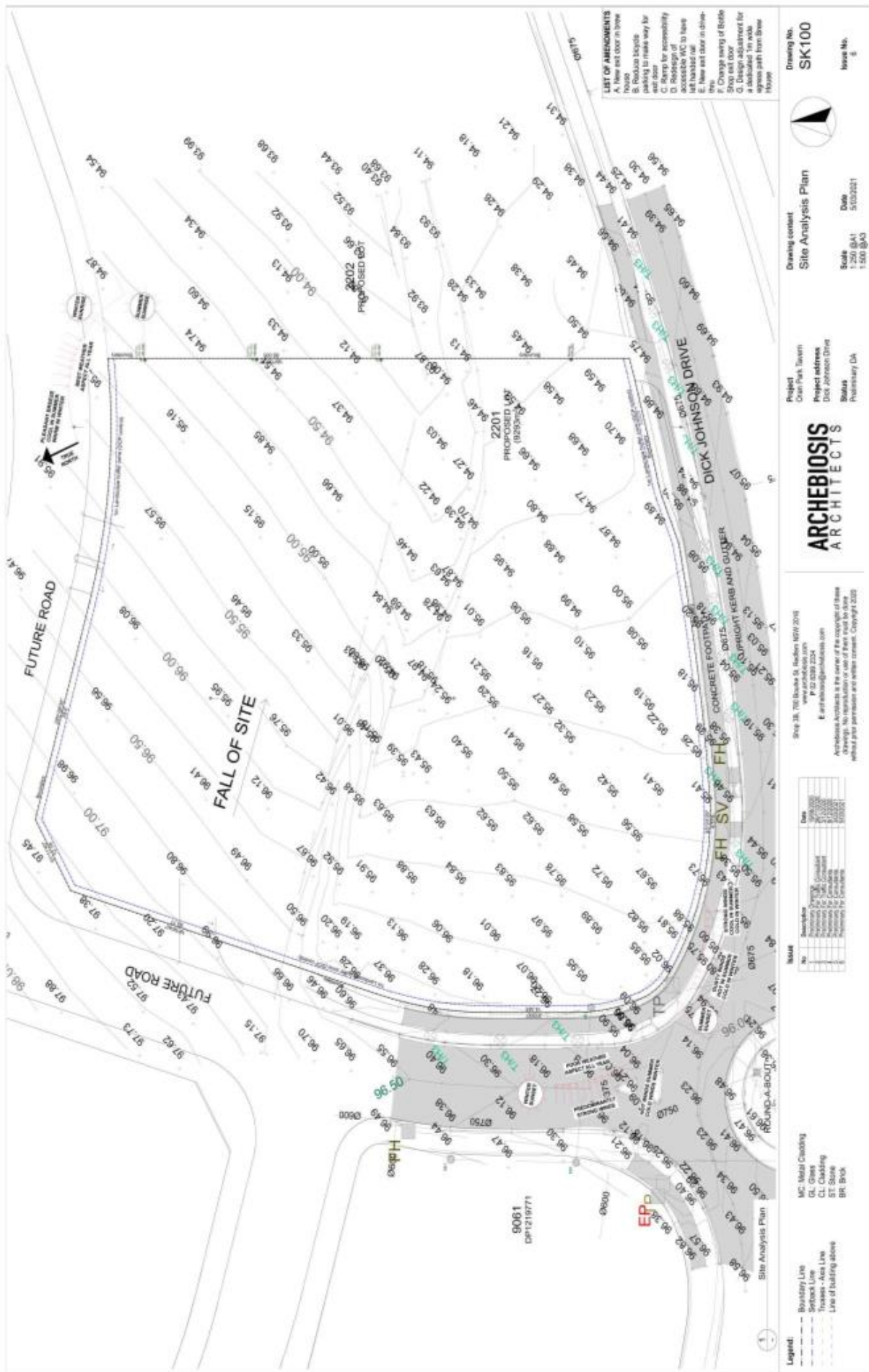
**Appendix A**  
**Selected Plans of the Proposed Development**  
**and Turning Path Diagrams**

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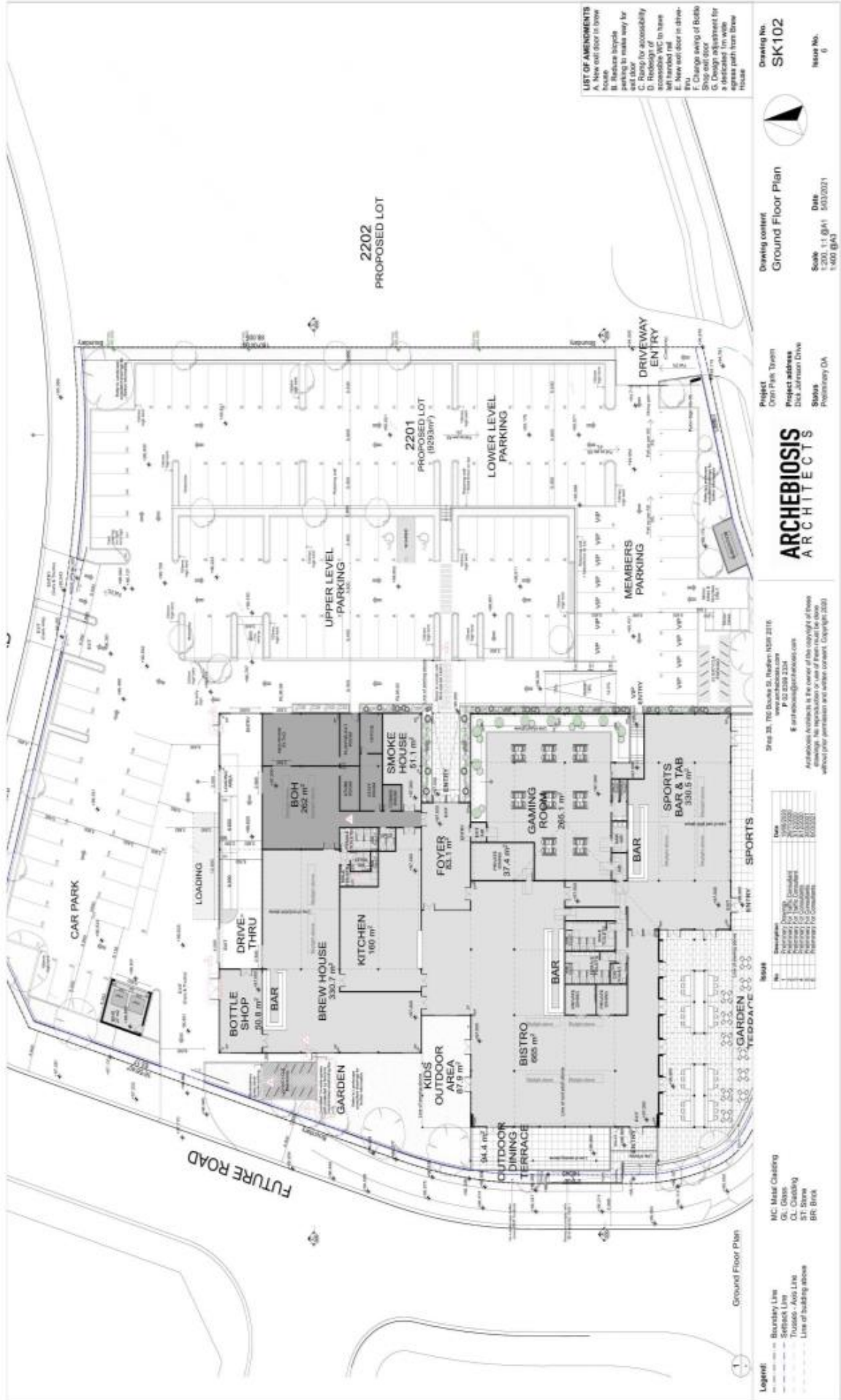


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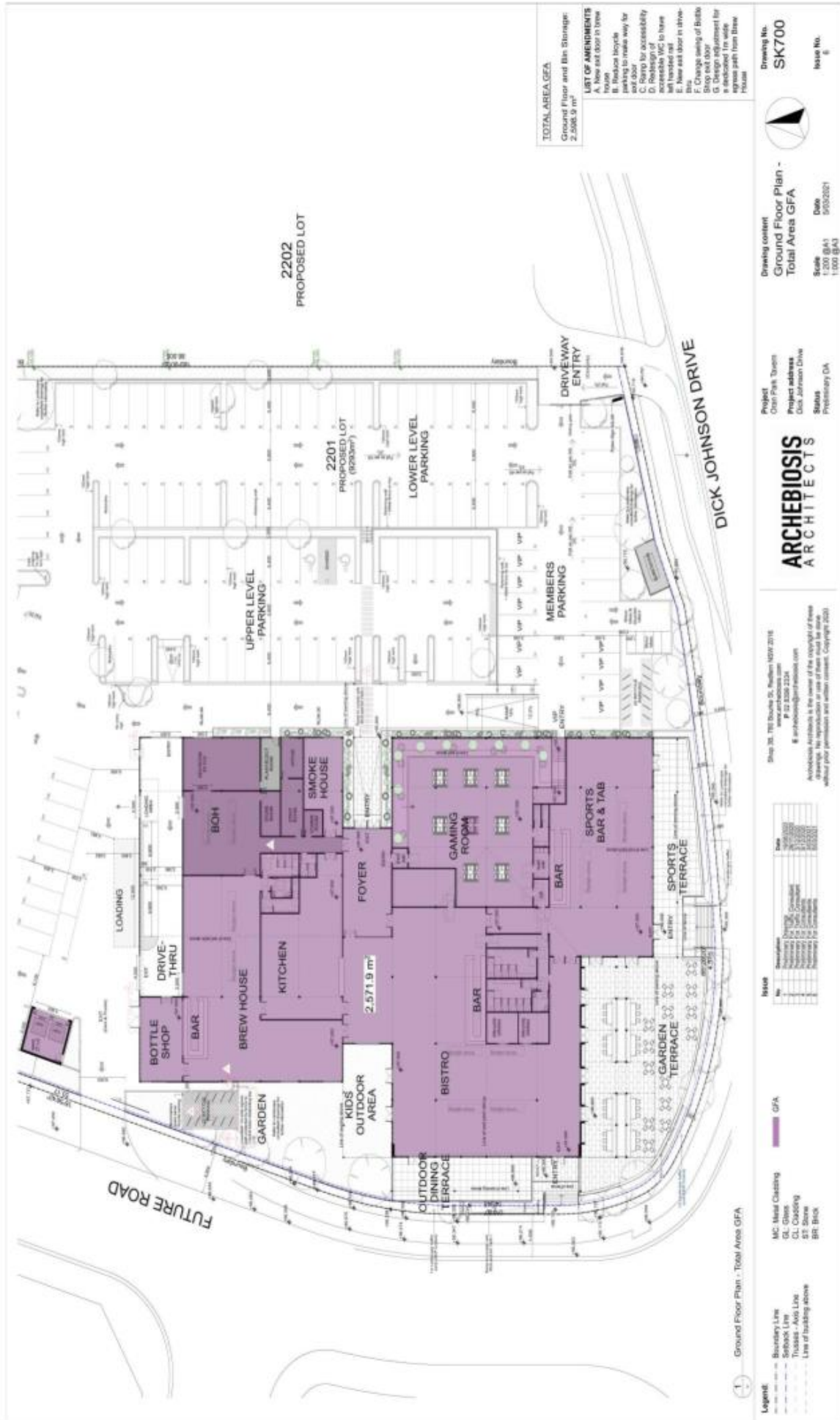


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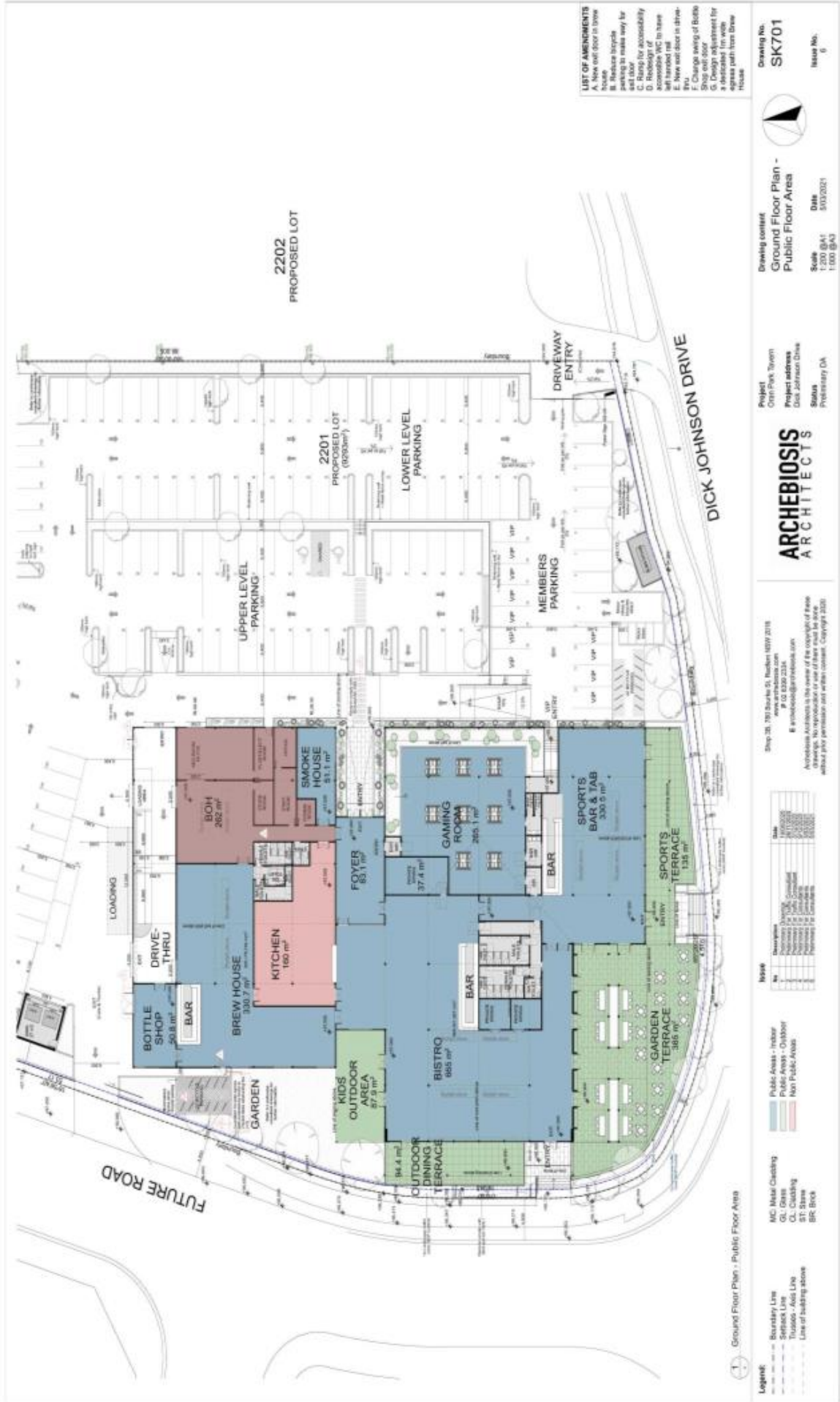
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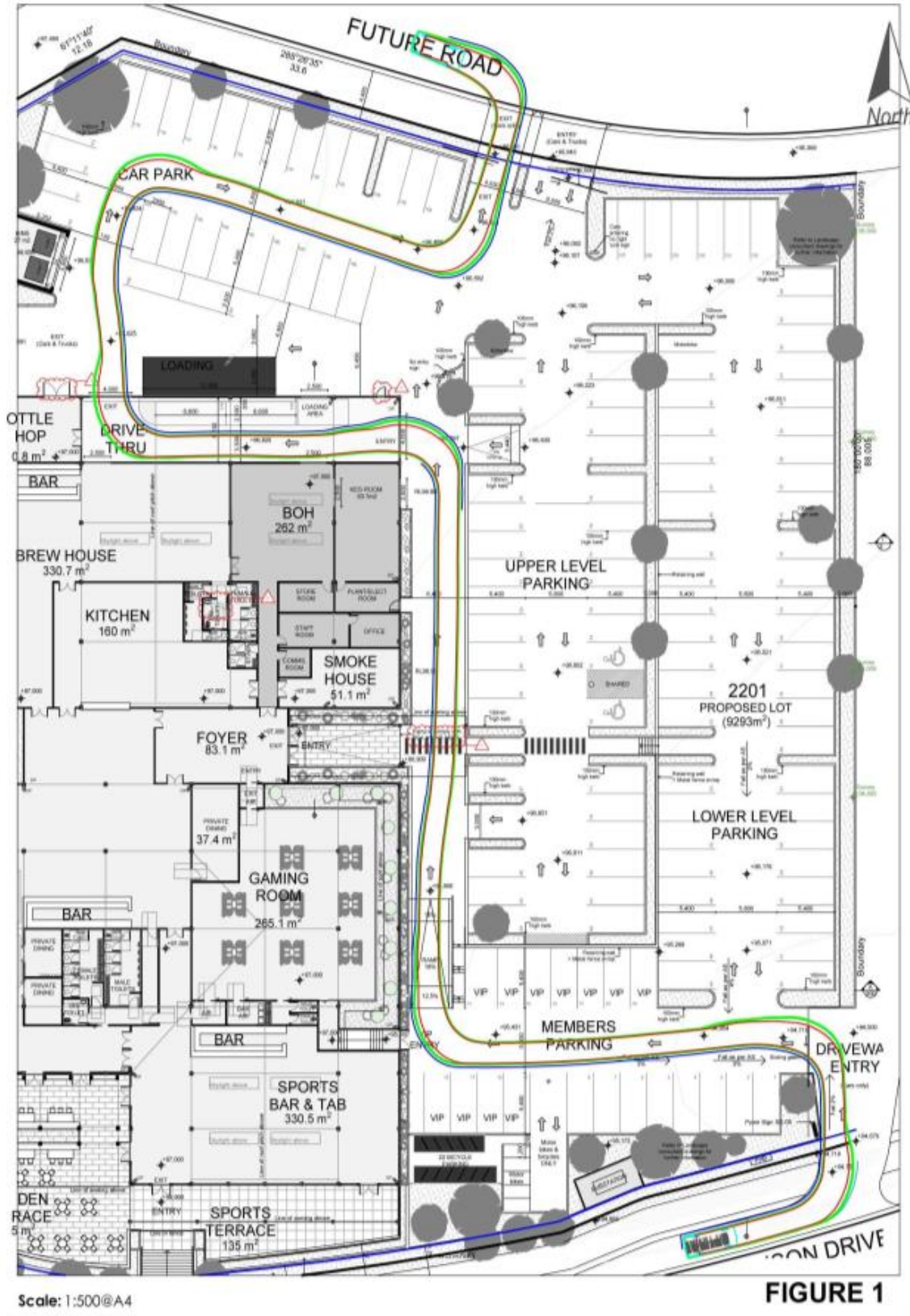
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# AS2890.1:2004 - B85 VEHICLE ORAN PARK PUB



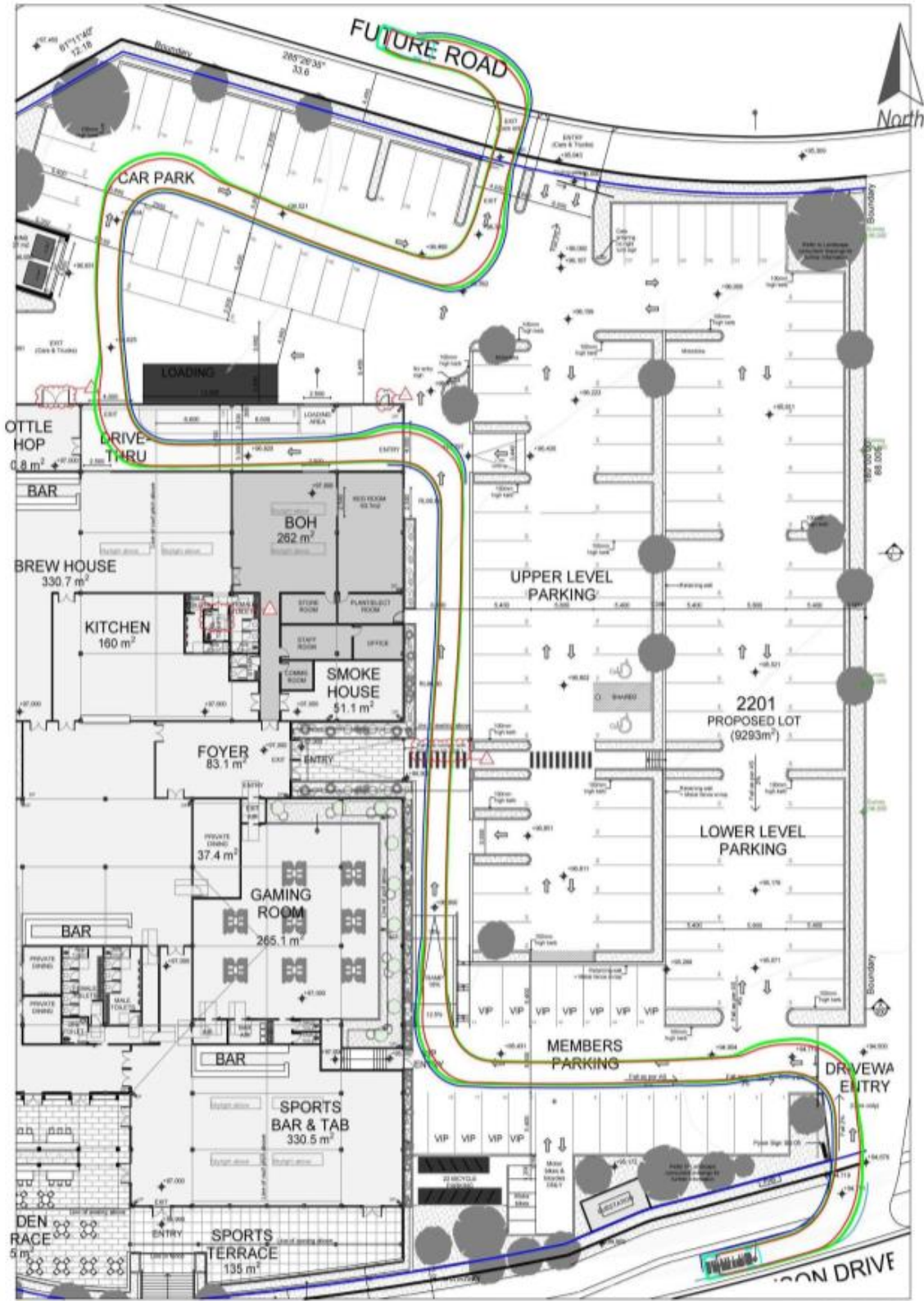
CLPP01

Attachment 5

# AS2890.1:2004 - B99 VEHICLE ORAN PARK PUB

CLPP01

Attachment 5

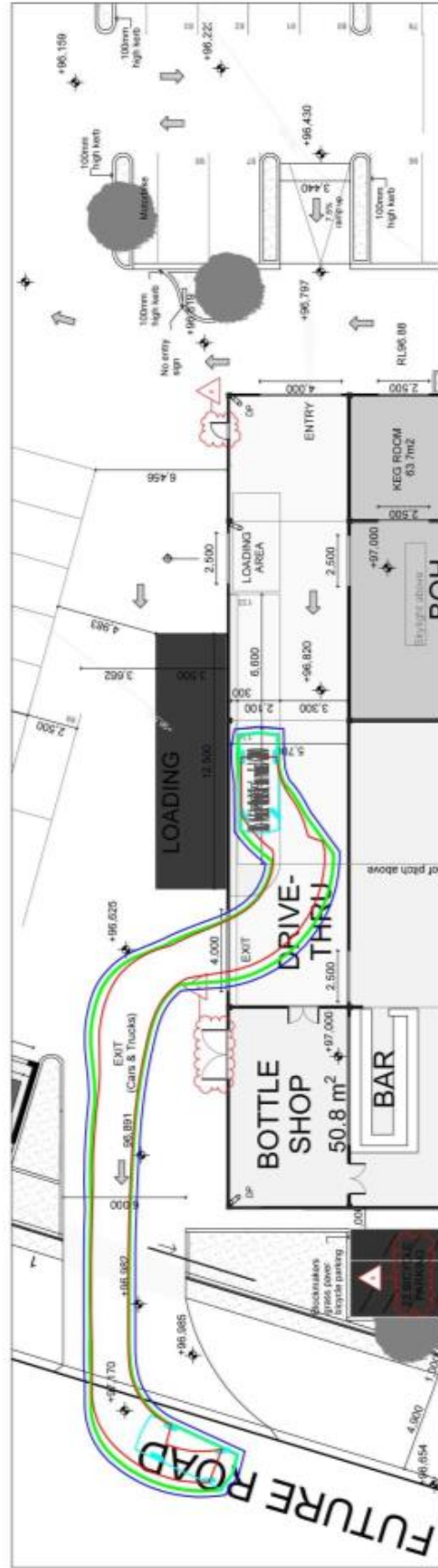
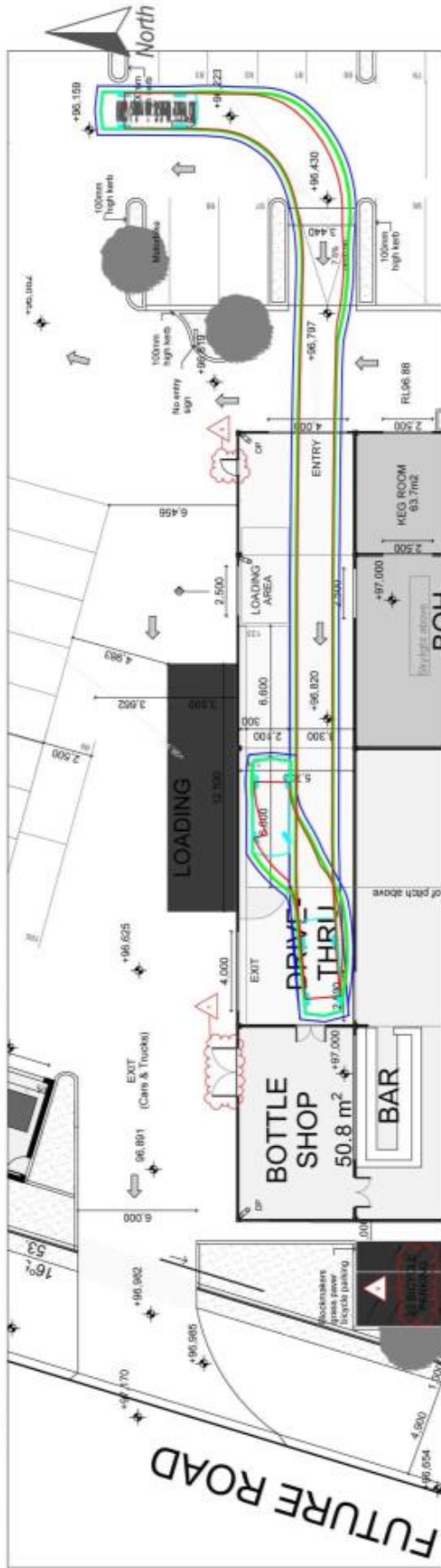


Scale: 1:500@A4

FIGURE 2

Date: 20/06/2021

# AS2890.1:2004 - B85 VEHICLE



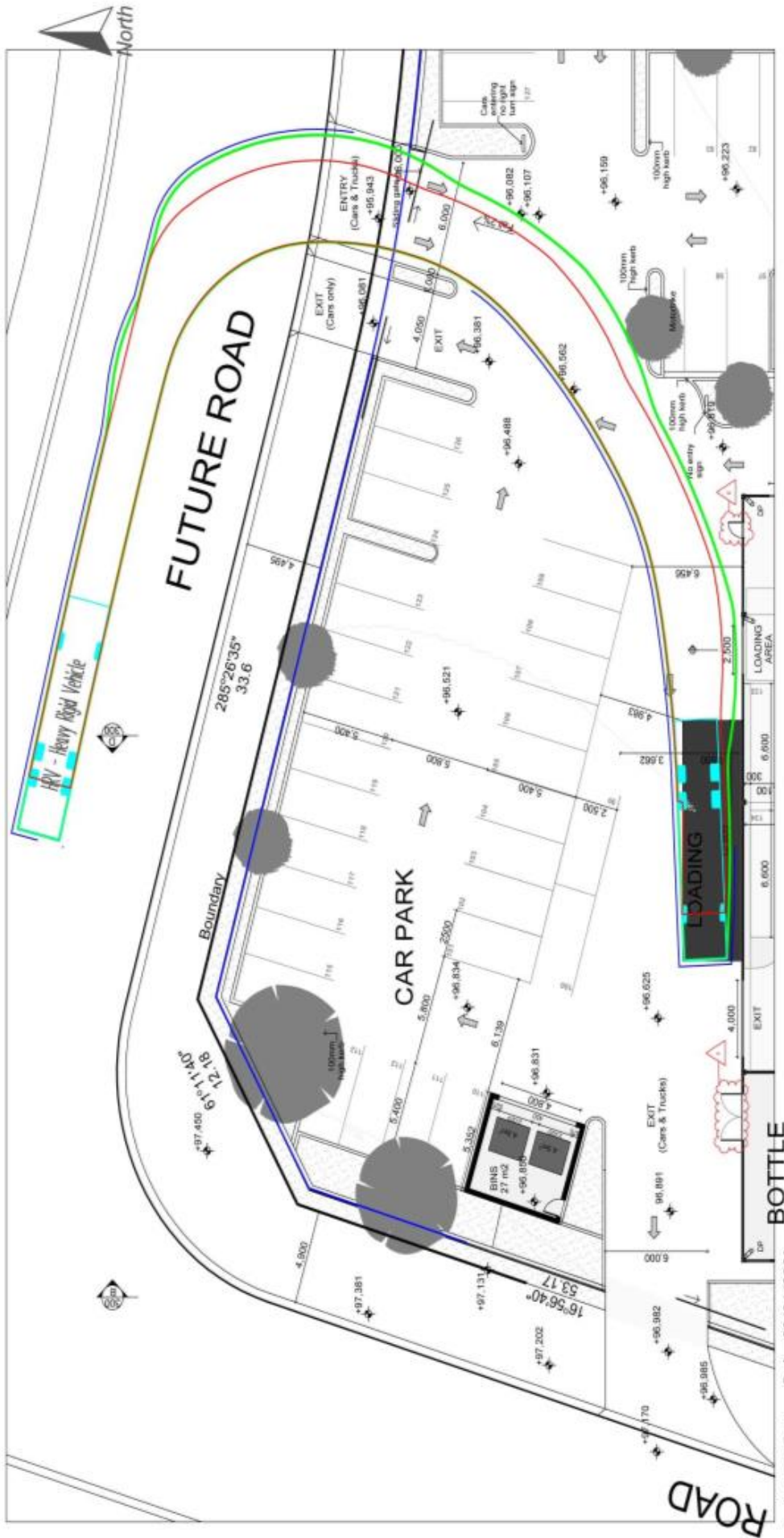
Scale: 1:300@A4 Date: 20/06/2021

Figure 3





# AS2890.2:2018 - HRV ENTRY



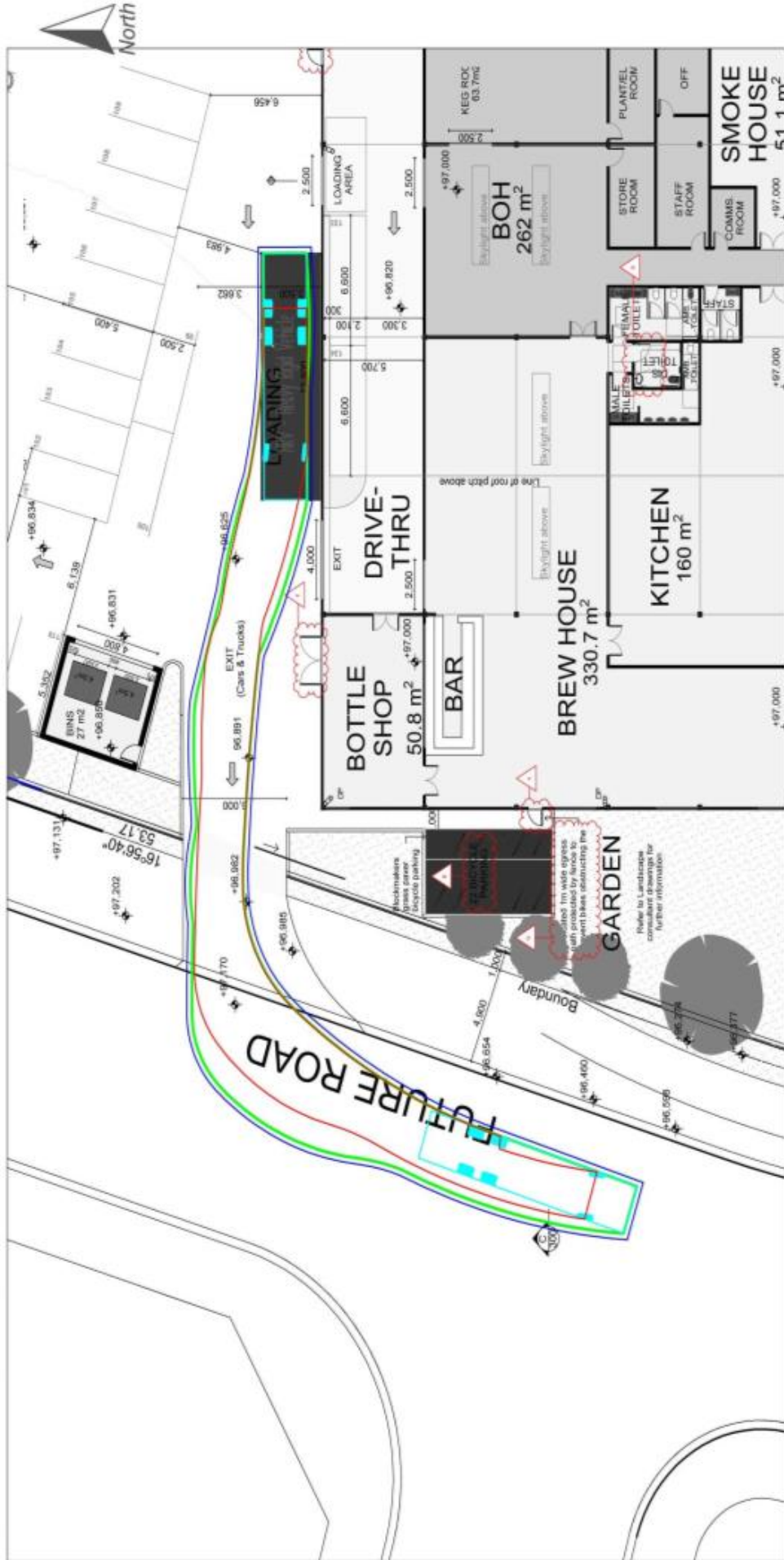
Scale: 1:300@A4 Date: 20/06/2021

Figure 5

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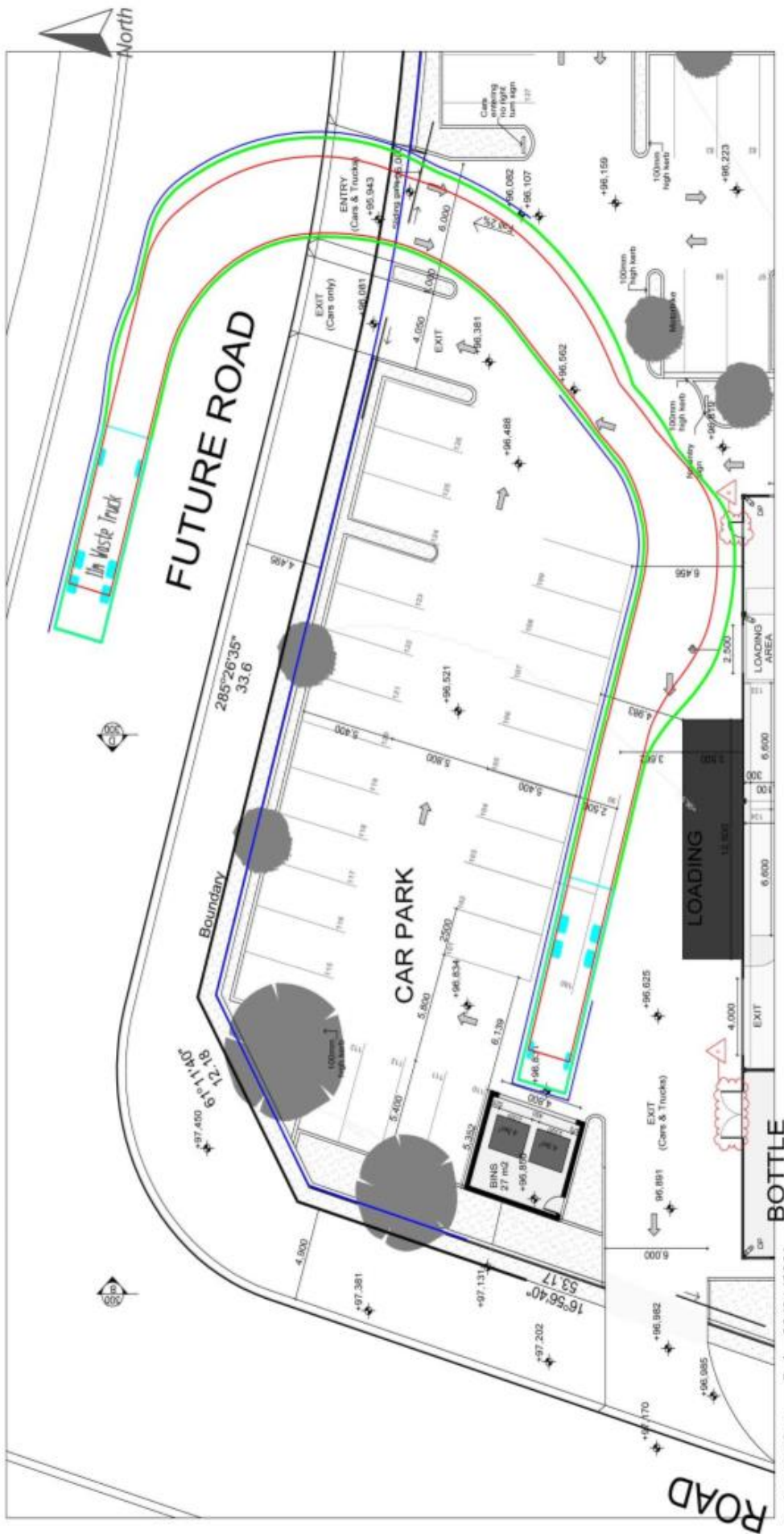
AS2890.2:2018 - HRV EXIT



Scale: 1:300@A4 Date: 20/06/2021

Figure 6

# 11m WASTE TRUCK - ENTRY



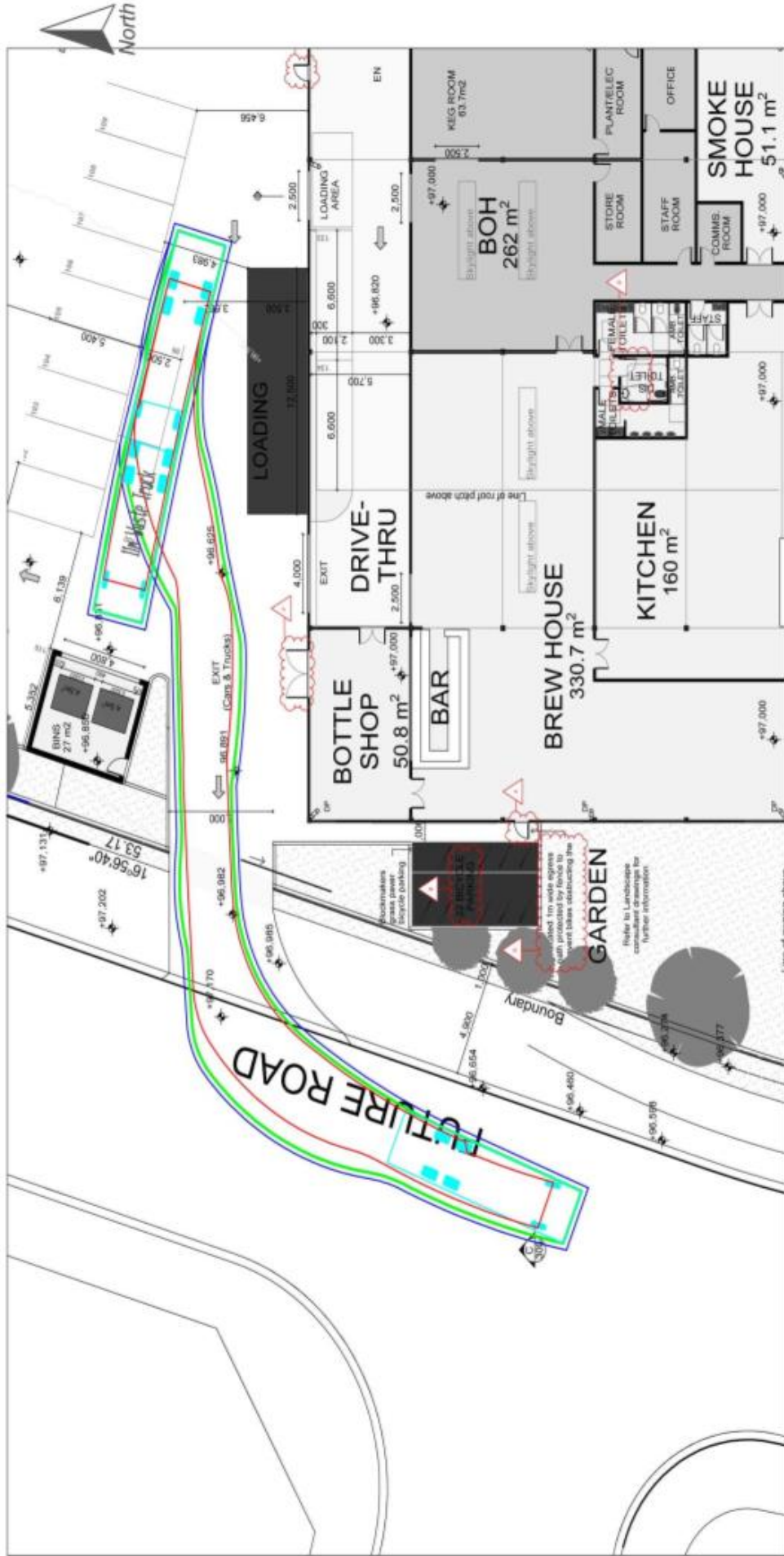
Scale: 1:300@A4 Date: 20/06/2021

Figure 7

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**11m WASTE TRUCK - EXIT**



Scale: 1 :300@A4 Date: 20/06/2021

**Figure 8**

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**Appendix B**  
**Results of the Patron Accumulation and Interview**  
**Surveys conducted at the Mona Vale Pub**

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**Attachment 5**

## 2. Operating Characteristics of Existing Hotel

The trading hours of the Mona Vale Hotel are currently 5.00am – 3.00am the following day, seven days per week.

The Mona Vale Hotel currently employs a total workforce of approximately 50 people with the following typical attendance pattern:

Workforce Category	Daytime			Nighttime			
	Mon-Fri	Sat	Sun	Mon-Wed	Thu-Fri	Sat	Sun
Manager	1	1	2	1	1-2	3	1
Admin	3	2	-	-	-	2	-
Bar/Dining Staff	5	6	6	7	7	10	5
Cleaners	2	2	2	-	-	-	-
Security	-	-	-	1	1-2	9-12	1
<b>Total</b>	11	11	10	9	9-11	24-27	7

During a normal week, the existing hotel workforce generates a daytime parking demand ranging between 3 vehicles on Sunday – Friday to 8 vehicles on Saturday. The typical nighttime parking demand ranges between 3 vehicles on Sunday – Thursday, to 5 vehicles on Friday, and a maximum of 8 vehicles on Saturday nights. On busy Saturday 27 February 2016, the maximum workforce on the hotel site was 26 persons during the daytime and 45 persons during the nighttime. The maximum workforce parking demand was 10 vehicles during the daytime and 15 vehicles during the nighttime.

The Hotel hosted the following activities throughout the survey period:

Saturday 27 <sup>th</sup> March	2.00pm – 1.00am	Particularly popular DJ
Tuesday 1 <sup>st</sup> March	6.00 – 9.00pm	Trivia Competition
Wednesday 2 <sup>nd</sup> March	6.00 – 9.00pm	2-4-1
Friday 4 <sup>th</sup> March	6.00 – 9.00pm	Love Local (family wife)
Saturday 5 <sup>th</sup> March	10.00 – 2.00am	DJ

A series of surveys of the operational characteristics of the existing hotel were undertaken over an 8-day trading period between Saturday 27<sup>th</sup> February – Saturday 5<sup>th</sup> March 2016. The surveys were taken over an 8 day period instead of the usual 7 day (1 week) period, to compile information on 2 Saturday nights:

- (i) Saturday 27 February, when live entertainment in the form of a particularly popular DJ performed between 2.00pm – 1.00am attracting *unusually* heavy patronage.
- (ii) Saturday 5 March, when *normal* Saturday patronage was experienced.

The surveys undertaken at the hotel include:

**Patron Accumulation Survey:** This survey involved a “headcount” of patrons in the *public floorarea* in the existing hotel on an hourly basis throughout the survey period to provide an indication of fluctuations in patron accumulations in the Hotel. The survey included separate “headcounts” in different parts of the hotel.

**Patron Interview Survey:** An interview survey of a sample of patrons in the Hotel was undertaken to provide information on patron travel mode and parking location. The survey also provided information on whether patrons lived or worked in the vicinity of the Hotel (ie within walking distance), and whether they visited the Hotel as a multi-purpose trip or as a specific trip purpose. The survey was conducted throughout the survey period.

As noted in the foregoing, the surveys were undertaken over an 8 day period instead of the usual one week period to compile information on a *normal* trading week (ie Sunday 28 February – Saturday 5 March, 2016) and on Saturday 27 February when a popular DJ performed between 2.00pm – 1.00am attracting *unusually* heavy patronage. The survey results have therefore been analysed for the *normal* trading week (Sunday 28 February – Saturday 5 March, 2016), and independently for Saturday 27 February when patronage was *unusually* heavy.

The results of the patron accumulation surveys are included in Appendix C and summarised on the graphs reproduced in the following pages revealing that:

- weekday daytime patron accumulations (prior to 5.00pm) were less than 60 persons, with higher patron accumulations on Saturday (74 persons at 1.00pm) and Sunday (124 persons at 2.00pm). The *average maximum*<sup>4</sup> daytime patron accumulation in the hotel was in the order of 50 persons on weekdays and 75 persons on weekend days
- nighttime patron accumulations in the hotel (ie after 5.00pm) were generally less than 110 persons, the exceptions being Friday night when a peak accumulation of 166 patrons was recorded at 7.00pm and on Wednesday night when a peak patron accumulation of 143 persons was recorded also at 7.00pm. The peak Saturday night patronage recorded by the survey was 101 persons at 11.00pm. The *average maximum* nighttime patron accumulation was therefore 110 persons on weekday nights, and approximately 80 persons on Saturday night

<sup>4</sup> The *average maximum* patron accumulation is that accumulation which is exceeded for only a few hours per week.

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- the *peak* daytime patron accumulation recorded for the hotel on busy Saturday 27 February 2016 was 116 persons around 4.00pm, while a *peak* nighttime patron accumulation of 921 persons was recorded around 10.00pm. The substantially increased patronage achieved by the Mona Vale Hotel on this busy Saturday night is illustrated on the graph which shows patron accumulations at the Mona Vale Hotel over the 8 day period between Saturday 27 February 2016 and Sunday 5 March 2016.

Accordingly, for the purposes of this assessment the following patron accumulation conditions in the existing hotel during the normal trading week (Sunday 28 February – Saturday 5 March 2016) have been assumed:

<b>DAYTIME (Prior to 5.00pm)</b>	<b>WEEKDAY</b>	<b>WEEKEND</b>
<i>Peak Attendance</i>	62 persons (Friday)	124 persons (Sunday)
<i>Average Maximum Attendance</i>	50 persons	75 persons
<b>NIGHTTIME (After 5.00pm)</b>	<b>WEEKDAY</b>	<b>WEEKEND</b>
<i>Peak Attendance</i>	166 persons (Friday)	101 persons (Saturday)
<i>Average Maximum Attendance</i>	110 persons	80 persons

By way of comparison, the attendance characteristics for the busy Saturday 27 February 2016 were:

<b>DAYTIME (Prior to 5.00pm)</b>	<b>SATURDAY 27 FEBRUARY 2016</b>
<i>Peak Attendance</i>	116 persons
<i>Average Maximum Attendance</i>	80 persons
<b>NIGHTTIME (After 5.00pm)</b>	
<i>Peak Attendance</i>	921 persons
<i>Average Maximum Attendance</i>	650 persons

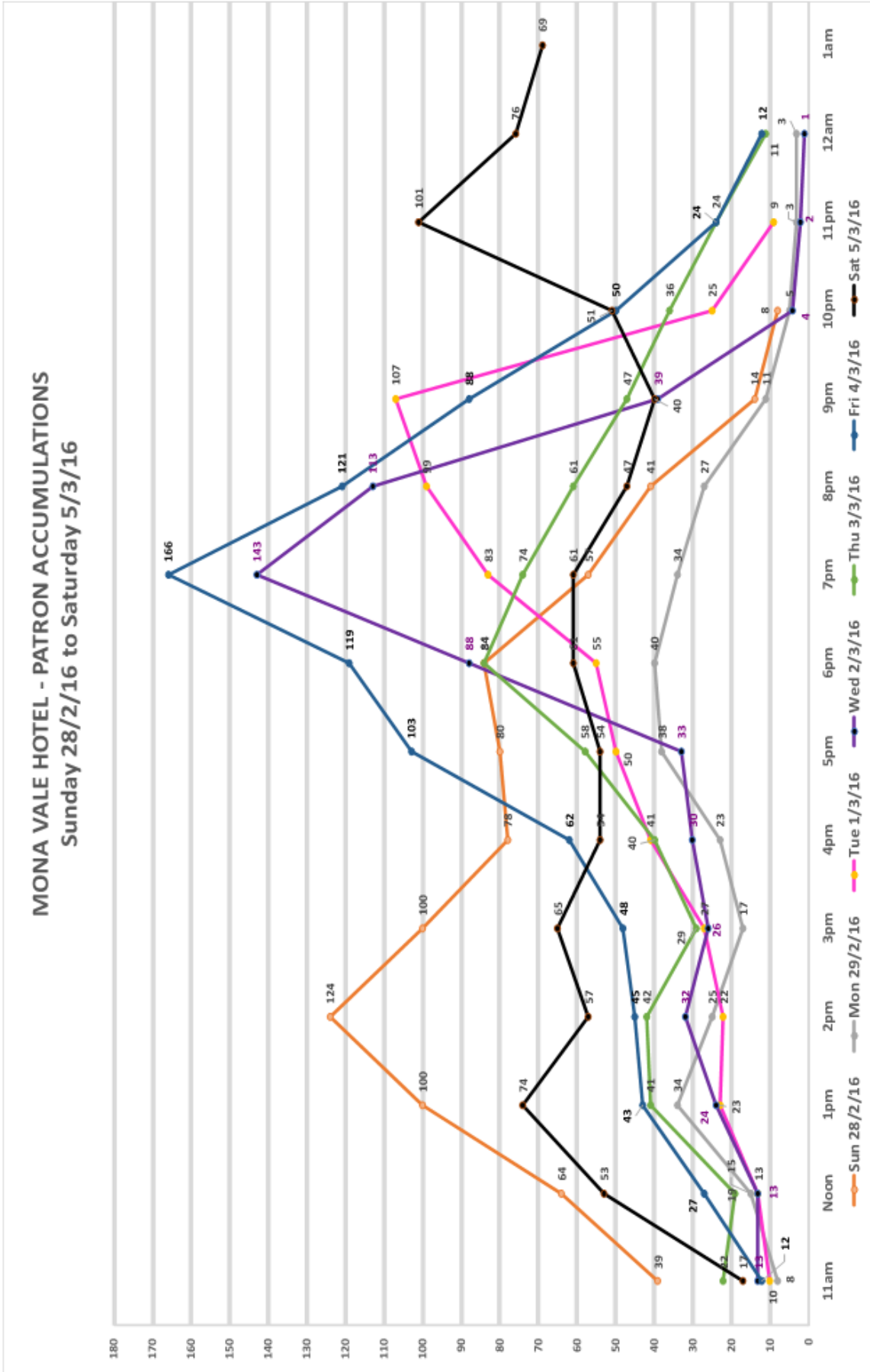
As can be observed, the significant increase in patronage at the Mona Vale Hotel on busy Saturday 27 February 2016 was generally confined to the nighttime period (ie after 5.00pm).

A total of 1044 persons responded to the interview survey during the normal trading week, (Sunday 28 February – Saturday 5 March 2016), while a total of 586 patrons responded on busy Saturday 27 February 2016. The response to the interview survey is summarised in table form in Appendix D revealing that:

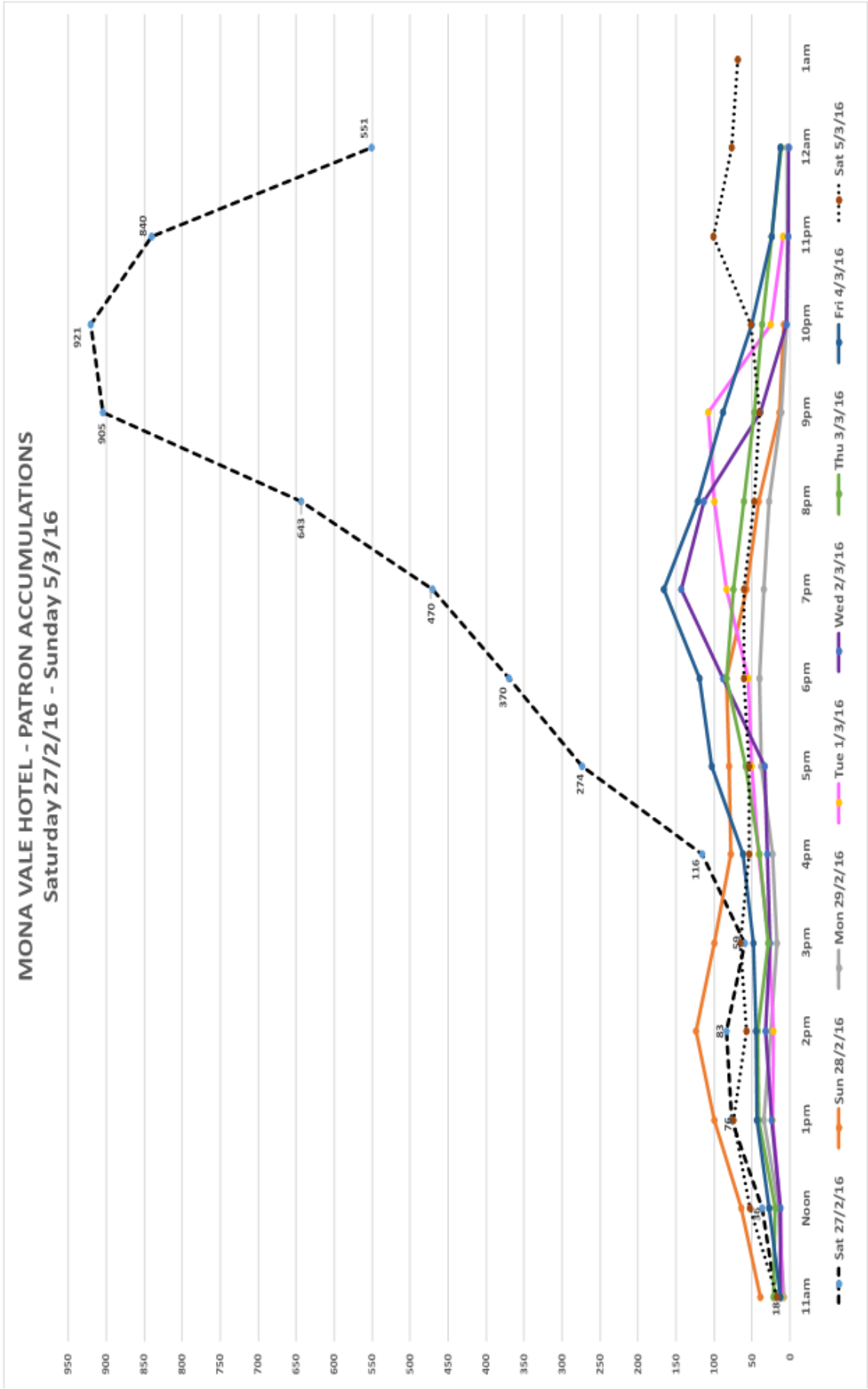
- an *average* of 45.8% of hotel patrons drove a car to/from the hotel during the daytime (prior to 5.00pm) over the full survey week, with 50.0% driving on weekdays and 40.4% driving on weekend days. The highest proportion (53.2%) drove a car on the Monday surveyed, while the lowest proportion (37.3%) drove a car on Saturday



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v

- an *average* of 34.5% of hotel patrons drove a car to/from the hotel at nighttime (after 5.00pm) over the full survey week between Sunday 28 February – Saturday 5 March 2016, with 43.8% driving a car on weekday nights and 18.5% driving on weekend nights. The highest proportion (53.2%) drove a car on the Monday night, while the lowest proportion (16.6%) drove on Saturday night
- in contrast, on busy Saturday 27 February 2016, an *average* of 17.1% of hotel patrons drove a car to/from the hotel, with 22.8% driving a car during the daytime (prior to 5.00pm) and only 11.3% driving a car during the nighttime (after 5.00pm). The much lower nighttime car driver rate recorded on busy Saturday 27 February 2016 when the peak nighttime patron accumulation was 921 persons, compared to the 16.6% of patrons who drove a car at nighttime on Saturday 5 March 2016 when the peak patron accumulation was substantially less at 101 persons, is consistent with the trend recorded in other surveys conducted by John Coady Consulting Pty Ltd
- the *average* travel mode of hotel patrons throughout the survey week (Sunday 28 February – Saturday 5 March 2016), compared to the travel mode of patrons on busy Saturday 27 February 2016 is shown on the table below confirming the significantly reduced patron cardriver rate on busy Saturday, 27 February 2016.

Travel Mode	Normal Week Sunday 28/2/16 to Saturday 5/3/16			Busy Saturday 27/2/16
	Weekday	Weekend	All Days	
Car Driver	46.3%	28.7%	39.3%	17.1%
Car Passenger	25.3%	31.1%	27.6%	16.4%
Car Passenger Dropped Off	2.1%	7.0%	4.0%	11.8%
Public Transport	5.2%	4.6%	5.0%	21.3%
Taxi	1.6%	5.8%	3.3%	16.0%
Walk	18.8%	22.4%	20.2%	17.4%
Other	0.8%	0.5%	0.7%	0.3%

The results of the patron accumulation and interview surveys indicate that the typical patron parking demand generated by the existing Mona Vale Hotel is:

NORMAL WEEK: SUNDAY 28/2/16 TO SATURDAY 5/3/16		
	WEEKDAY	WEEKEND
<b>DAYTIME</b>		
Peak Attendance	62 persons x 50.5% = 32 vehicles	124 persons @ 42.4% = 53 vehicles
Average Maximum Attendance	50 persons x 50.0% = 25 vehicles	75 persons @ 40.4% = 31 vehicles
<b>NIGHTTIME</b>		
Peak Attendance	166 persons x 37.2% = 62 vehicles	101 persons @ 16.6% = 17 vehicles
Average Maximum Attendance	110 persons x 43.8% = 49 vehicles	80 persons @ 18.5% = 15 vehicles

By way of comparison, the parking demand potential of the hotel on the busy Saturday 27 February 2016 calculated on the basis of the survey results is:

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SATURDAY 27/2/16	
<b>DAYTIME</b>	
Peak Attendance	116 persons x 22.8% = 27 vehicles
Average Maximum Attendance	80 persons x 22.8% = 19 vehicles
<b>NIGHTTIME</b>	
Peak Attendance	921 persons x 11.3% = 104 vehicles
Average Maximum Attendance	650 persons x 11.3% = 74 vehicles

These patron parking demand characteristics confirm that the effect of the increased patronage attracted to the Mona Vale Hotel on busy Saturday 27 February 2016, in terms of increased patron parking demand, was generally confined to the nighttime period.

Also, allowance should be made for workforce parking demand. The total parking demand potential of the existing hotel including workforce parking, is therefore:

NORMAL WEEK – SUNDAY 28/2/16 – SATURDAY 5/3/16						
	Patron Parking Demand Potential		Workforce Parking Demand Potential		Total Parking Demand Potential	
	Weekday	Weekend	Weekday	Weekend	Weekday	Weekend
<b>DAYTIME</b>						
Peak Attendance	32	53	3	8	35	61
Average Maximum Attendance	25	31	3	8	28	39
<b>NIGHTTIME</b>						
Peak Attendance	62	17	5	8	67	25
Average Maximum Attendance	49	15	5	8	54	23

and

SATURDAY 27/2/16			
	Patron Parking Demand Potential	Workforce Parking Demand Potential	Total Parking Demand Potential
<b>DAYTIME</b>			
Peak Attendance	27	10	37
Average Maximum Attendance	19	10	29
<b>NIGHTTIME</b>			
Peak Attendance	104	15	119
Average Maximum Attendance	74	15	89

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**Appendix C**  
**Results of Hotel Patron Accumulation Surveys**

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Time	Sat 27/2/16	Sun 28/2/16	Mon 29/2/16	Tue 1/3/16	Wed 2/3/16	Thu 3/3/16	Fri 4/3/16	Sat 5/3/16
11 am	18	39	8	10	13	22	12	17
Noon	36	64	15	13	13	19	27	53
1pm	76	100	34	23	24	41	43	74
2pm	83	124	25	22	32	42	45	57
3pm	59	100	17	27	26	29	48	65
4pm	116	78	23	41	30	40	62	54
5pm	274	80	38	50	33	58	103	54
6pm	370	84	40	55	88	84	119	61
7pm	470	57	34	83	143	74	166	61
8pm	643	41	27	99	113	61	121	47
9pm	905	14	11	107	39	47	88	40
10pm	921	8	5	25	4	36	50	51
11pm	840		3	9	2	24	24	101
12am	551		3		1	11	12	76
1 am								69

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MONA VALE HOTEL - PATRON ACCUMULATIONS														
Date: Saturday 27/2/16														
Time	Ground Floor						First Floor							
	Beer Garden	Plantation Lounge	Laneway Café	New Bar	Gaming Lounge	Sports Bar	Front Beer Garden	Other	Sub-Total	Attic	Rooftop	Other	Sub-Total	TOTAL
11.00am	0	0	0	0	4	4	10	0	18	0	0	0	0	18
Midday	1	0	0	2	14	10	9	0	36	0	0	0	0	36
1.00pm	23	2	10	1	10	16	14	0	76	0	0	0	0	76
2.00pm	9	5	7	0	16	32	14	0	83	0	0	0	0	83
3.00pm	1	0	0	0	18	21	19	0	59	0	0	0	0	59
4.00pm	30	8	3	1	19	20	24	0	105	0	11	0	11	116
5.00pm	125	40	25	3	21	22	23	0	259	0	15	0	15	274
6.00pm	178	35	41	4	18	28	30	0	334	0	36	0	36	370
7.00pm	254	53	16	24	6	16	40	0	409	0	61	0	61	470
8.00pm	380	117	44	14	18	24	21	0	618	0	25	0	25	643
9.00pm	600	70	21	4	26	22	19	9	771	46	88	0	134	905
10.00pm	180	350	70	13	22	30	28	56	749	94	78	0	172	921
11.00pm	224	320	48	0	21	28	25	0	666	146	28	0	174	840
Midnight	94	270	26	0	36	0	0	0	426	95	30	0	125	551
1.00am									0				0	0
2.00am									0				0	0
3.00am									0				0	0

MONA VALE HOTEL - PATRON ACCUMULATIONS															
Date: Sunday 28/2/16															
Time	Plantation		Lounge	Laneway Café	New Bar	Gaming Lounge	Sports Bar	Front Beer Garden	Other	Sub-Total	First Floor				
	Beer Garden	Lounge									Attic	Rooftop	Other	Sub-Total	TOTAL
11.00am	0	0	0	0	0	2	2	5	0	9	10	20	0	30	39
Midday	10	2	0	0	0	3	6	13	0	34	15	15	0	30	64
1.00pm	32	14	15	0	0	4	6	5	0	76	15	9	0	24	100
2.00pm	47	22	4	0	0	8	15	11	0	107	10	7	0	17	124
3.00pm	39	6	2	2	2	14	28	9	0	100	0	0	0	0	100
4.00pm	31	0	0	0	0	11	25	11	0	78	0	0	0	0	78
5.00pm	34	4	2	0	0	8	21	11	0	80	0	0	0	0	80
6.00pm	39	6	3	0	0	8	12	16	0	84	0	0	0	0	84
7.00pm	23	5	2	0	0	5	8	14	0	57	0	0	0	0	57
8.00pm	20	3	0	0	0	5	3	10	0	41	0	0	0	0	41
9.00pm	12	2	0	0	0	0	0	0	0	14	0	0	0	0	14
10.00pm	5	0	0	0	0	3	0	0	0	8	0	0	0	0	8
11.00pm										0				0	0
Midnight										0				0	0
1.00am										0				0	0
2.00am										0				0	0
3.00am										0				0	0



Date: Tuesday 1/3/16														
Time	Ground Floor						First Floor							
	Beer Garden	Plantation Lounge	Laneway Café	New Bar	Gaming Lounge	Sports Bar	Front Beer Garden	Other	Sub-Total	Attic	Roof-top	Other	Sub-Total	TOTAL
11.00am	0	0	0	0	4	1	5	0	10	0	0	0	0	10
Midday	0	0	1	0	8	4	0	0	13	0	0	0	0	13
1.00pm	3	3	3	0	4	2	8	0	23	0	0	0	0	23
2.00pm	8	1	1	0	6	0	6	0	22	0	0	0	0	22
3.00pm	2	3	0	0	10	7	5	0	27	0	0	0	0	27
4.00pm	5	2	3	0	13	8	10	0	41	0	0	0	0	41
5.00pm	8	2	3	0	12	17	8	0	50	0	0	0	0	50
6.00pm	10	4	4	1	9	7	20	0	55	0	0	0	0	55
7.00pm	15	45	7	0	4	5	7	0	83	0	0	0	0	83
8.00pm	36	54	2	0	3	0	4	0	99	0	0	0	0	99
9.00pm	11	80	0	0	16	0	0	0	107	0	0	0	0	107
10.00pm	7	11	0	0	7	0	0	0	25	0	0	0	0	25
11.00pm	0	6	0	0	3	0	0	0	9	0	0	0	0	9
Midnight									0					0
1.00am									0					0
2.00am									0					0
3.00am									0					0

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MONA VALE HOTEL - PATRON ACCUMULATIONS															
Date: Wednesday 2/3/16															
Time	Plantation			Ground Floor							First Floor				
	Beer Garden	Lounge	Laneway Café	New Bar	Gaming Lounge	Sports Bar	Front Beer Garden	Other	Sub-Total	Attic	Rooftop	Other	Sub-Total	TOTAL	
11.00am	0	0	0	0	7	3	3	0	13	0	0	0	0	13	
Midday	0	1	0	0	7	3	2	0	13	0	0	0	0	13	
1.00pm	7	5	0	0	8	1	3	0	24	0	0	0	0	24	
2.00pm	12	2	0	0	8	5	5	0	32	0	0	0	0	32	
3.00pm	11	1	0	0	8	6	0	0	26	0	0	0	0	26	
4.00pm	5	1	0	0	7	10	7	0	30	0	0	0	0	30	
5.00pm	3	0	0	0	8	11	11	0	33	0	0	0	0	33	
6.00pm	34	7	2	1	6	22	16	0	88	0	0	0	0	88	
7.00pm	77	16	9	1	9	12	19	0	143	0	0	0	0	143	
8.00pm	69	10	12	0	8	5	9	0	113	0	0	0	0	113	
9.00pm	31	1	2	0	5	0	0	0	39	0	0	0	0	39	
10.00pm	0	0	0	0	4	0	0	0	4	0	0	0	0	4	
11.00pm	0	0	0	0	2	0	0	0	2	0	0	0	0	2	
Midnight	0	0	0	0	1	0	0	0	1	0	0	0	0	1	
1.00am									0					0	
2.00am									0					0	
3.00am									0					0	

MONA VALE HOTEL - PATRON ACCUMULATIONS														
Date: Thursday 3/3/16														
Time	Plantation		Ground Floor							First Floor				
	Beer Garden	Lounge	Laneway Café	New Bar	Gaming Lounge	Sports Bar	Front Beer Garden	Other	Sub-Total	Attic	Rooftop	Other	Sub-Total	TOTAL
11.00am	12	0	0	0	5	2	3	0	22	0	0	0	0	22
Midday	6	2	0	0	5	6	0	0	19	0	0	0	0	19
1.00pm	17	5	0	0	10	5	4	0	41	0	0	0	0	41
2.00pm	17	4	0	0	10	9	2	0	42	0	0	0	0	42
3.00pm	1	0	0	0	12	12	4	0	29	0	0	0	0	29
4.00pm	2	0	0	0	12	14	12	0	40	0	0	0	0	40
5.00pm	1	1	0	1	11	12	32	0	58	0	0	0	0	58
6.00pm	25	5	0	3	7	18	26	0	84	0	0	0	0	84
7.00pm	26	4	0	1	6	18	19	0	74	0	0	0	0	74
8.00pm	15	5	0	1	7	17	16	0	61	0	0	0	0	61
9.00pm	14	2	0	2	7	10	12	0	47	0	0	0	0	47
10.00pm	3	4	0	0	4	10	15	0	36	0	0	0	0	36
11.00pm	0	0	0	0	6	4	14	0	24	0	0	0	0	24
Midnight	0	0	0	0	2	9	0	0	11	0	0	0	0	11
1.00am									0					0
2.00am									0					0
3.00am									0					0

MONA VALE HOTEL - PATRON ACCUMULATIONS																
Date: Friday 4/3/16																
Time	Plantation			Lounge	Laneway Café	New Bar	Gaming Lounge	Sports Bar	Front Beer Garden	Other	Sub-Total	First Floor				
	Beer Garden	Lounge										Attic	Rooftop	Other	Sub-Total	TOTAL
11.00am	0	0	0	0	0	1	3	6	2	0	12	0	0	0	0	12
Midday	9	1	0	0	0	0	5	10	2	0	27	0	0	0	0	27
1.00pm	19	7	0	0	0	0	6	4	7	0	43	0	0	0	0	43
2.00pm	29	1	1	0	0	0	7	7	0	0	45	0	0	0	0	45
3.00pm	19	1	0	0	0	0	10	9	9	0	48	0	0	0	0	48
4.00pm	15	1	0	0	1	1	10	18	17	0	62	0	0	0	0	62
5.00pm	20	11	1	1	1	1	17	28	25	0	103	0	0	0	0	103
6.00pm	26	10	2	0	0	0	15	31	35	0	119	0	0	0	0	119
7.00pm	68	17	3	1	1	1	14	25	38	0	166	0	0	0	0	166
8.00pm	57	2	0	0	0	0	17	18	27	0	121	0	0	0	0	121
9.00pm	18	11	0	1	1	1	19	15	24	0	88	0	0	0	0	88
10.00pm	6	0	4	0	0	0	11	21	8	0	50	0	0	0	0	50
11.00pm	0	0	0	0	0	0	3	4	17	0	24	0	0	0	0	24
Midnight	0	0	0	0	0	0	0	12	0	0	12	0	0	0	0	12
1.00am											0					0
2.00am											0					0
3.00am											0					0

MONA VALE HOTEL - PATRON ACCUMULATIONS															
Date: Saturday 5/3/16															
Time	Ground Floor							First Floor							
	Plantation		Beer Garden	Laneway Café	New Bar	Gaming Lounge	Sports Bar	Front Beer Garden	Other	Sub-Total	Attic	Rooftop	Other	Sub-Total	TOTAL
	Beer Garden	Lounge													
11.00am	0	0	0	0	0	2	6	9	0	17	0	0	0	0	17
Midday	18	7	0	0	0	9	4	15	0	53	0	0	0	0	53
1.00pm	37	2	8	0	0	5	11	11	0	74	0	0	0	0	74
2.00pm	13	2	0	0	0	14	11	17	0	57	0	0	0	0	57
3.00pm	14	0	0	0	1	13	16	21	0	65	0	0	0	0	65
4.00pm	14	0	0	0	1	9	18	12	0	54	0	0	0	0	54
5.00pm	13	0	0	0	0	15	20	6	0	54	0	0	0	0	54
6.00pm	20	4	0	0	0	10	16	11	0	61	0	0	0	0	61
7.00pm	25	8	0	0	0	4	6	18	0	61	0	0	0	0	61
8.00pm	20	7	0	0	0	6	8	6	0	47	0	0	0	0	47
9.00pm	3	0	0	0	0	16	13	8	0	40	0	0	0	0	40
10.00pm	9	0	0	0	2	9	17	14	0	51	0	0	0	0	51
11.00pm	20	6	2	0	0	6	41	26	0	101	0	0	0	0	101
Midnight	20	15	0	0	0	4	31	6	0	76	0	0	0	0	76
1.00am	39	30	0	0	0	0	0	0	0	69	0	0	0	0	69
2.00am										0				0	0
3.00am										0				0	0

JOHN COADY CONSULTING  
Townplanning and Traffic Consultant

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Attachment 5

**Appendix D**  
**Results of Patron Interview Surveys**

**Mona Vale Hotel**  
**Patron Interview Survey**  
**Sunday 28 February 2016**

		Total Patrons interviewed in the day											189		
Time	Work or Live in the vicinity		Travel Mode							Where did you park?				Did you make a special trip to Mona Vale today to visit the hotel or were you already in Mona Vale?	
	Work	Live	CD	CP	CPI	PT	T	W	O	Hotel Carpark	Other Public Carpark	Other Private Carpark	On-Street	Special Trip	Already in Mona Vale
1000-1700 hrs	10	36	50	35	8	5	1	17	2	12	17	5	17	88	10
%	8.5	30.5	42.4	29.7	6.8	4.2	0.8	14.4	1.7	23.5	33.3	9.8	33.3	74.6	8.5
1700-2200 hrs	12	31	16	24	0	4	1	26	0	6	5	0	5	24	23
%	16.9	43.7	22.5	33.8	0.0	5.6	1.4	36.6	0.0	37.5	31.3	0.0	31.3	51.1	48.9
Total	22	67	66	59	8	9	2	43	2	18	22	5	22	112	33
%	11.6	35.4	34.9	31.2	4.2	4.8	1.1	22.8	1.1	26.9	32.8	7.5	32.8	59.3	17.5

**Mona Vale Hotel**  
**Patron Interview Survey**  
**Monday 29 February 2016**

		Total Patrons interviewed in the day											94		
Time	Work or Live in the vicinity		Travel Mode							Where did you park?				Did you make a special trip to Mona Vale today to visit the hotel or were you already in Mona Vale?	
	Work	Live	CD	CP	CPI	PT	T	W	O	Hotel Carpark	Other Public Carpark	Other Private Carpark	On-Street	Special Trip	Already in Mona Vale
1000-1700 hrs	11	19	25	11	0	5	0	6	0	3	13	0	9	25	11
%	23.4	40.4	53.2	23.4	0.0	10.6	0.0	12.8	0.0	12.0	52.0	0.0	36.0	53.2	23.4
1700-2400 hrs	6	19	25	17	1	0	0	4	0	7	4	0	14	17	11
%	12.8	40.4	53.2	36.2	2.1	0.0	0.0	8.5	0.0	28.0	16.0	0.0	56.0	36.2	23.4
Total	17	38	50	28	1	5	0	10	0	10	17	0	23	42	22
%	18.1	40.4	53.2	29.8	1.1	5.3	0.0	10.6	0.0	20.0	34.0	0.0	46.0	44.7	23.4

**Mona Vale Hotel**

**Patron Interview Survey**

Tuesday 1 March 2016

Total Patrons interviewed in the day 74

Time	Work or Live in the vicinity		Travel Mode										Where did you park?				Did you make a special trip to Mona Vale today to visit the hotel or were you already in Mona Vale?	
	Work	Live	CD	CP	CPI	PT	T	W	O	Hotel Carpark	Other Public Carpark	Other Private Carpark	On-Street	Special Trip	Already in Mona Vale			
1000-1700 hrs	4	7	14	7	0	3	0	7	1	4	4	0	6	12	13			
%	12.5	21.9	43.8	21.9	0.0	9.4	0.0	21.9	3.1	28.6	28.6	0.0	42.9	37.5	40.6			
1700-2400 hrs	7	23	21	10	0	1	0	10	0	1	12	0	11	40	2			
%	16.7	54.8	50.0	23.8	0.0	2.4	0.0	23.8	0.0	4.2	50.0	0.0	45.8	95.2	4.8			
Total	11	30	35	17	0	4	0	17	1	5	16	0	17	52	15			
%	14.9	40.5	47.3	23.0	0.0	5.4	0.0	23.0	1.4	13.2	42.1	0.0	44.7	70.3	20.3			

**Mona Vale Hotel**

**Patron Interview Survey**

Wednesday 2 March 2016

Total Patrons interviewed in the day 114

Time	Work or Live in the vicinity		Travel Mode										Where did you park?				Did you make a special trip to Mona Vale today to visit the hotel or were you already in Mona Vale?	
	Work	Live	CD	CP	CPI	PT	T	W	O	Hotel Carpark	Other Public Carpark	Other Private Carpark	On-Street	Special Trip	Already in Mona Vale			
1000-1700 hrs	4	4	15	7	0	5	0	2	2	4	5	0	6	11	10			
%	12.9	12.9	48.4	22.6	0.0	16.1	0.0	6.5	6.5	26.7	33.3	0.0	40.0	35.5	32.3			
1700-2400 hrs	12	33	35	30	0	3	2	13	0	5	15	1	14	46	6			
%	14.5	39.8	42.2	36.1	0.0	3.6	2.4	15.7	0.0	14.3	42.9	2.9	40.0	55.4	7.2			
Total	16	37	50	37	0	8	2	15	2	9	20	1	20	57	16			
%	14.0	32.5	43.9	32.5	0.0	7.0	1.8	13.2	1.8	18.0	40.0	2.0	40.0	50.0	14.0			



**Mona Vale Hotel**  
**Patron Interview Survey**  
Thursday 3 March 2016

Total Patrons interviewed in the day **129**

Time	Work or Live in the vicinity		Travel Mode										Where did you park?				Did you make a special trip to Mona Vale today to visit the hotel or were you already in Mona Vale?	
	Work	Live	CD	CP	CPI	PT	T	W	O	Hotel Carpark	Other Public Carpark	Other Private Carpark	On-Street	Special Trip	Already in Mona Vale			
1000-1700 hrs	18	13	21	12	0	1	0	7	0	4	12	0	5	18	14			
%	43.9	31.7	51.2	29.3	0.0	2.4	0.0	17.1	0.0	19.0	57.1	0.0	23.8	43.9	34.1			
1700-2400 hrs	10	47	41	21	3	3	2	18	0	7	23	0	11	64	22			
%	11.4	82.5	46.6	23.9	3.4	3.4	2.3	20.5	0.0	17.1	56.1	0.0	26.8	72.7	25.0			
Total	28	60	62	33	3	4	2	25	0	11	35	0	16	82	36			
%	21.7	46.5	48.1	25.6	2.3	3.1	1.6	19.4	0.0	17.7	56.5	0.0	25.8	63.6	27.9			

**Mona Vale Hotel**  
**Patron Interview Survey**  
Friday 4 March 2016

Total Patrons interviewed in the day **218**

Time	Work or Live in the vicinity		Travel Mode										Where did you park?				Did you make a special trip to Mona Vale today to visit the hotel or were you already in Mona Vale?	
	Work	Live	CD	CP	CPI	PT	T	W	O	Hotel Carpark	Other Public Carpark	Other Private Carpark	On-Street	Special Trip	Already in Mona Vale			
1000-1700 hrs	28	41	49	18	2	4	2	22	0	4	31	1	13	87	10			
%	28.9	42.3	50.5	18.6	2.1	4.1	2.1	22.7	0.0	8.2	63.3	2.0	26.5	89.7	10.3			
1700-2400 hrs	21	45	45	26	7	8	4	29	2	2	11	0	32	96	90			
%	17.4	37.2	37.2	21.5	5.8	6.6	3.3	24.0	1.7	4.4	24.4	0.0	71.1	79.3	74.4			
Total	49	86	94	44	9	12	6	51	2	6	42	1	45	183	100			
%	22.5	39.4	43.1	20.2	4.1	5.5	2.8	23.4	0.9	6.4	44.7	1.1	47.9	83.9	45.9			

**Mona Vale Hotel  
Patron Interview Survey  
Saturday 5 March 2016**

		Total Patrons interviewed in the day 226										Did you make a special trip to Mona Vale today to visit the hotel or were you already in Mona Vale?				
Time	Work or Live in the vicinity		Travel Mode								Where did you park?				Special Trip	Already in Mona Vale
	Work	Live	CD	CP	CPI	PT	T	W	O	Hotel Carpark	Other Public Carpark	Other Private Carpark	On-Street			
1000-1700 hrs	6	10	28	40	0	1	3	3	0	7	8	0	12	17	18	
%	8.0	13.3	37.3	53.3	0.0	1.3	4.0	4.0	0.0	25.9	29.6	0.0	44.4	22.7	24.0	
1700-0200 hrs	11	32	25	30	21	9	19	47	0	1	7	0	16	93	7	
%	7.3	21.2	16.6	19.9	13.9	6.0	12.6	31.1	0.0	4.2	29.2	0.0	66.7	61.6	4.6	
Total	17	42	53	70	21	10	22	50	0	8	15	0	28	110	25	
%	7.5	18.6	23.5	31.0	9.3	4.4	9.7	22.1	0.0	15.7	29.4	0.0	54.9	48.7	11.1	

**Mona Vale Hotel  
Weekday Totals**

		Total Patrons interviewed on weekdays 629										Did you make a special trip to Mona Vale today to visit the hotel or were you already in Mona Vale?				
Time Period	Work or Live in the vicinity		Travel Mode								Where did you park?				Special Trip	Already in Mona Vale
	Work	Live	CD	CP	CPI	PT	T	W	O	Hotel Carpark	Other Public Carpark	Other Private Carpark	On-Street			
<1700 hrs	65	84	124	55	2	18	2	44	3	19	65	1	39	153	58	
%	26.2	33.9	50.0	22.2	0.8	7.3	0.8	17.7	1.2	15.3	52.4	0.8	31.5	61.7	23.4	
>1700 hrs	56	167	167	104	11	15	8	74	2	22	65	1	82	263	131	
%	14.7	43.8	43.8	27.3	2.9	3.9	2.1	19.4	0.5	12.9	38.2	0.6	48.2	69.0	34.4	
Total	121	251	291	159	13	33	10	118	5	41	130	2	121	416	189	
%	19.2	39.9	46.3	25.3	2.1	5.2	1.6	18.8	0.8	13.9	44.2	0.7	41.2	66.1	30.0	

**Mona Vale Hotel**

**Weekend Totals**

Total Patrons interviewed on weekends

415

Time Period	Work or Live in the vicinity		Travel Mode										Where did you park?				Did you make a special trip to Mona Vale today to visit the hotel or were you already in Mona Vale?	
	Work	Live	CD	CP	CPI	PT	T	W	O	Hotel Carpark	Other Public Carpark	Other Private Carpark	On-Street	Special Trip	Already in Mona Vale			
																Special Trip	Already in Mona Vale	
<1700 hrs	16	46	78	75	8	6	4	20	2	19	25	5	29	105	28			
%	8.3	23.8	40.4	38.9	4.1	3.1	2.1	10.4	1.0	24.4	32.1	6.4	37.2	54.4	14.5			
>1700 hrs	23	63	41	54	21	13	20	73	0	7	12	0	21	117	30			
%	10.4	28.4	18.5	24.3	9.5	5.9	9.0	32.9	0.0	17.5	30.0	0.0	52.5	52.7	13.5			
Total	39	109	119	129	29	19	24	93	2	26	37	5	50	222	58			
%	9.4	26.3	28.7	31.1	7.0	4.6	5.8	22.4	0.5	22.0	31.4	4.2	42.4	53.5	14.0			

**Mona Vale Hotel**

**Weekly Totals**

Total Patrons interviewed weekly

1044

Time Period	Work or Live in the vicinity		Travel Mode										Where did you park?				Did you make a special trip to Mona Vale today to visit the hotel or were you already in Mona Vale?	
	Work	Live	CD	CP	CPI	PT	T	W	O	Hotel Carpark	Other Public Carpark	Other Private Carpark	On-Street	Special Trip	Already in Mona Vale			
																Special Trip	Already in Mona Vale	
<1700 hrs	81	130	202	130	10	24	6	64	5	38	90	6	68	258	86			
%	18.4	29.5	45.8	29.5	2.3	5.4	1.4	14.5	1.1	18.8	44.6	3.0	33.7	58.5	19.5			
>1700 hrs	79	230	208	158	32	28	28	147	2	29	77	1	103	380	161			
%	13.1	38.1	34.5	26.2	5.3	4.6	4.6	24.4	0.3	13.8	36.7	0.5	49.0	63.0	26.7			
Total	160	360	410	288	42	52	34	211	7	67	167	7	171	638	247			
%	15.3	34.5	39.3	27.6	4.0	5.0	3.3	20.2	0.7	16.3	40.5	1.7	41.5	61.1	23.7			

**Mona Vale Hotel**  
**Patron Interview Survey**  
**Saturday 27 February 2016**

Total Patrons interviewed in the day

586

Time	Work or Live in the		Travel Mode							Where did you park?				Did you make a special trip to	
	Work	Live	CD	CP	CPI	PT	T	W	O	Hotel Carpark	Other Public Carpark	Other Private Carpark	On-Street	Special Trip	Already in Mona Vale
1000-1700 hrs	30	89	67	57	29	52	33	54	2	20	28	1	21	233	42
%	10.2	30.3	22.8	19.4	9.9	17.7	11.2	18.4	0.7	28.6	40.0	1.4	30.0	79.3	14.3
1700-0200 hrs	17	46	33	39	40	73	61	46	0	3	10	1	18	265	12
%	5.8	15.8	11.3	13.4	13.7	25.0	20.9	15.8	0.0	9.4	31.3	3.1	56.3	90.8	4.1
Total	47	135	100	96	69	125	94	100	2	23	38	2	39	498	54
%	8.0	23.0	17.1	16.4	11.8	21.3	16.0	17.1	0.3	22.5	37.3	2.0	38.2	85.0	9.2

JOHN COADY CONSULTING  
Townplanning and Traffic Consultant

**Appendix C**  
**Public Transport, Walking and Cycling**  
**Arrangements which are/will be Available to**  
**Serve the Proposed Pub**

**CLPP01**

**Attachment 5**

**DRAFT**

**2.5 Public transport**

The Oran Park public transport network has been developed to encourage the provision and use of public transport within Oran Park Precinct. Key bus routes have been identified in the Oran Park DCP, which aim to connect areas within the precinct to the Town Centre and nearby key local centres. This includes bus operations along Dick Johnson Drive, which fronts the Oran Park Employment Area.

**Figure 6** illustrates the bus routes within the vicinity of the Oran Park Employment Area.

**Figure 6 Public transport routes - Bus**



Source: Design + Planning 2019; modified AECOM, 2019

Design + Planning have developed the *Oran Park Bus Services Strategy (2017)* to rationalise the location of bus shelters within the precinct. The proposed locations for bus shelters in the vicinity of the Oran Park Employment Area are presented in **Figure 7**.

The provision of bus services to the Oran Park Employment Area with sheltered facilities would help encourage future workers and visitors to use of this mode of transport. Consideration should be given to providing a bus shelter on the southern side of Dick Johnson Drive in proximity to a pedestrian crossing facility.

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Revision B - 08-Mar-2019  
Prepared for - Greenfields Development Company No.2 - ABN: 31 133 939 965

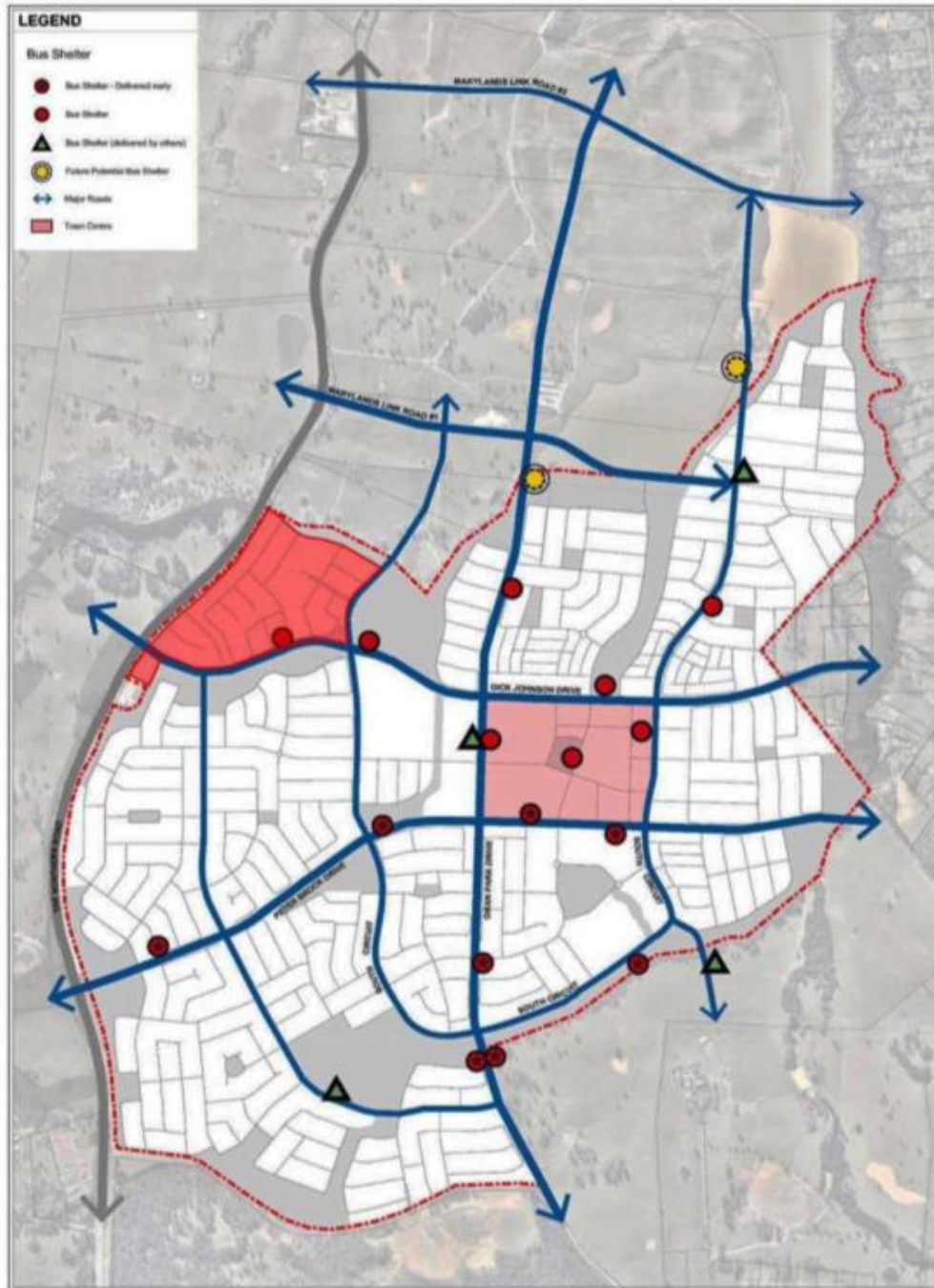
AECOM

Oran Park Precinct: Employment Area

8

**DRAFT**

Figure 7 Oran Park bus services strategy plan



Source: Design + Planning, 2018

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Revision B - 08-Mar-2019  
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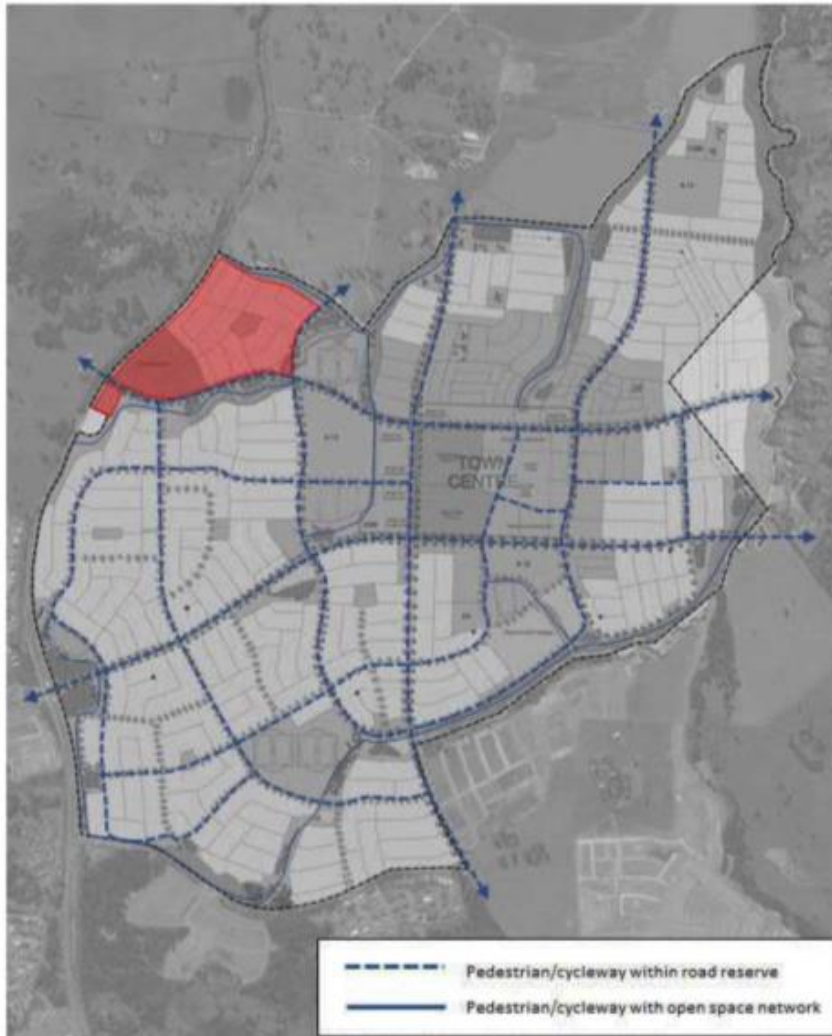
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**2.6 Walking and cycling**

The Oran Park Precinct incorporates integrated pedestrian and cycle routes throughout the precinct providing linkages to the broader network, promoting a high level of pedestrian permeability. Key pedestrian and cycleway routes have been identified in the Oran Park DCP.

The shared path network, presented in **Figure 8**, illustrates how the key routes connect the Oran Park Employment Area to the wider precinct. The shared path network will promote pedestrian and cycling activity to the area.

**Figure 8 Oran Park pedestrian and bicycle network**



Source: AECOM, 2018; modified by AECOM, 2019

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Attachment 5



AECOM

Oran Park Precinct: Employment Area

10

**DRAFT**

It is recommended the off-road shared path along Steward Drive is extended within the Oran Park Employment Area, as shown in **Figure 9**. This would provide direct connections and encourage walking and cycling to the area, as well as reduce the conflict of heavy vehicles and cyclists on internal roads.

Pedestrian footpaths are to be provided on at least one side of all roads with appropriate provision for safe crossings for pedestrians and cyclists within the Oran Park Employment Area, if required and warrants are met.

**Figure 9 Recommended extension of off-road shared path along Steward Drive**



Source: Design + Planning, 2019; modified by AECOM, 2019

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Attachment 6



4 August 2022

Reference: 220585.01FA

Momento Hospitality  
 C/O Archebiosis  
 Shop 3B, 780 Bourke Street, Redfern NSW 2016  
 Attention: Edith Clark

**LETTER OF ADVICE OF  
 THE PROPOSED PUB  
 AT 111 STEWARD DRIVE, ORAN PARK**

Dear Edith,

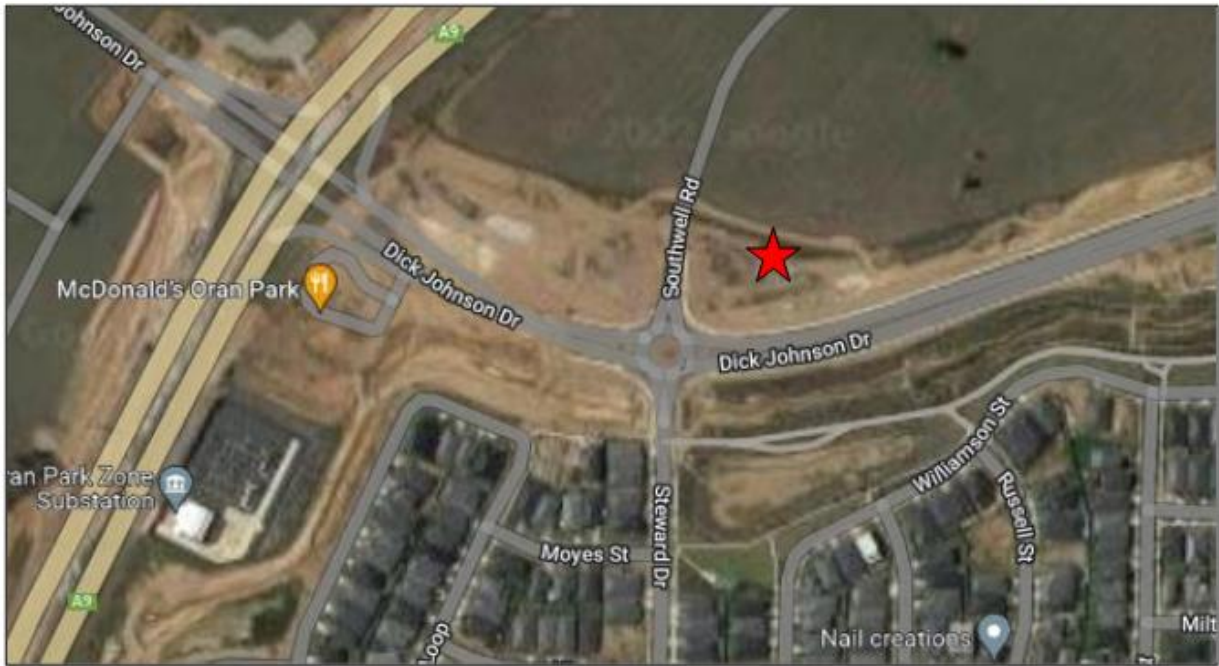
Reference is made to your request to provide a Letter of Advice for the Proposed Pub at 111 Steward Drive, Oran Park, with proposed plans depicted in **Annexure A** for reference. This letter outlines the application history surrounding car parking requirements on this site and sets out MTE's recommended approach to determine onsite car parking requirements.

**1 Site Context**

The site location is shown on aerial imagery and a map in **Figure 2 & Figure 3** respectively. The site is zoned B5, not IN1 as suggested in Council's RFI. The site is closely surrounded by IN1 zoning.

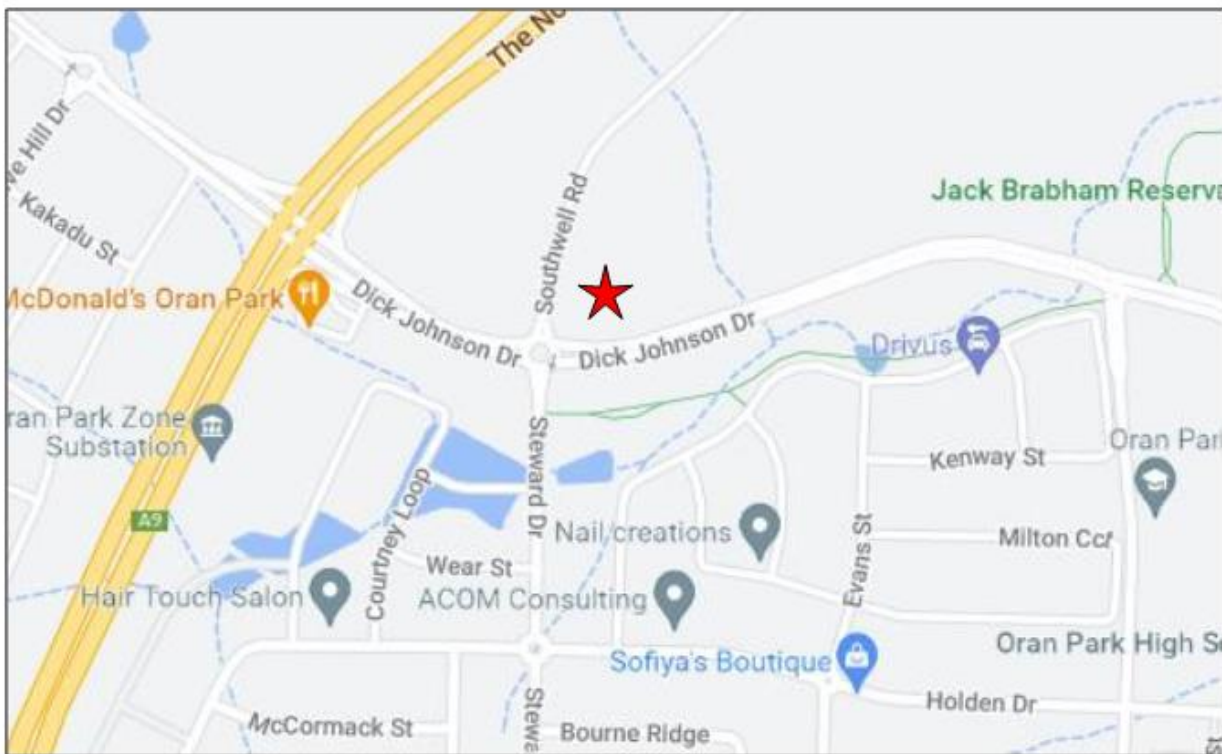


**FIGURE 1: LAND ZONING**



★ Site Location

FIGURE 2: SITE CONTEXT – AERIAL PHOTO



★ Site Location

FIGURE 3: SITE CONTEXT – STREET MAP

CLPP01  
Attachment 6

## 2 Previous Parking Demand Analysis

The RTA Guide to Traffic Generating Developments (RTA Guide), which is the foremost source of traffic and parking data for development in NSW, provides an off-street parking demand rate for 10 surveyed clubs in 1978. The RMS Guide states that behaviour of drivers to licensed hotels has changed since 1978 partly due to the introduction of random breath testing. The shifting of societal values and the introduction of blood alcohol driving limits has greatly changed the transport characteristics of patrons of licensed hotels. Therefore, the use of this guide is deemed inappropriate for the calculation of parking rates and as such has not been considered.

The Camden Council DCP 2019 states the following with regards to the variation of car parking rates from those provided:

*Council may consider variations to parking rates in certain circumstances that do not warrant demand and may be supported by a car parking and traffic impact assessment study submitted with a development application. Council will give consideration to other features of the development, such as proposed maximum staffing levels, expected customer levels etc. where warranted.*

The prior traffic reports utilised the Mona Vale Hotel as an example for a comparative study. Council disagreed with the use of this pub for comparative purposes.

## 3 Licensed Premises Parking Demand Study

The Council suggested that the applicant consider pubs within the LGA or similar such as the Marsden Brewhouse and the Gregory Hills Hotel. The key considerations for these developments are summarised in **Table 1**.

**TABLE 1: ORAN PARK PUB COMPARITIVE STUDY**

Site	Zoning	Surrounding Zoning	Courtesy Bus Operated	Public Transport	Parking Provision	Public Floor Area <sup>(1)(2)</sup>	Parking Provision (per 100m <sup>2</sup> )
Marsden Brewhouse	B7	IN2, B5	No	Bus stops within 200m walk	124	1,651m <sup>2</sup>	7.5
Gregory Hills Hotel	B5	IN1	No	Frontage bus stops	165	1,689m <sup>2</sup>	9.8
Average	-	-	-	-	-	-	8.7

Notes: (1) The public floor area includes pub areas, restaurants, terraces, dining areas and other areas which are primary patron generators. It excludes ancillary areas such as smoking rooms, gardens, kids areas, gaming rooms, kitchen and back of house which do not directly generate patron car parking demand.

(2) Floor plans for the Gregory Hills Hotel are unavailable. It is assumed to be 75% of the GFA

As shown, both existing sites have similar zonings and public transport accessibility compared to the subject site. The average parking provision between the sites is **8.7 spaces per 100m<sup>2</sup>** of public floor area.

#### 4 Development Changes

The proposed development in the previous iteration included 1,845m<sup>2</sup> public floor area and 127 car parking spaces, split out into the following areas:

- 235m<sup>2</sup> brewhouse (open to public)
- 665m<sup>2</sup> bistro
- 330.5m<sup>2</sup> sports bar
- 135m<sup>2</sup> sports terrace
- 348m<sup>2</sup> garden terrace
- 94.4m<sup>2</sup> outdoor dining terrace
- 37.4m<sup>2</sup> private dining room

The car parking provision for the previous iteration of the development was 6.9 spaces per 100m<sup>2</sup> public floor area. This is a shortfall in car parking compared to the averaged provision as per **Table 1** of 8.7 spaces per 100m<sup>2</sup> public floor area.

As a result, the development has been reconfigured to achieve the following:

- Exclude the brewhouse as a patron generating area. The brewhouse will be for brewing staff only and will not be an area for use by patrons.
- Reduce the area of the bistro from 665m<sup>2</sup> to 544m<sup>2</sup>
- Reduce the area of the sports bar from 330.5m<sup>2</sup> to 281m<sup>2</sup>
- Provision of a pick-up and drop-off courtesy bus to offset car parking demand

These changes to the development are summarised in **Table 2**.

**TABLE 2: ALTERATIONS TO FLOOR PLANS**

Layout	Public Floor Area (m <sup>2</sup> )								Parking Provision
	Brewhouse	Bistro	Sports Bar	Sports Terrace	Garden Terrace	Dining Terrace	Dining Room	Total	
Previous Scale	235	665	330.5	135	348	94.4	37.4	1,845m <sup>2</sup>	6.9 per 100m <sup>2</sup>
Proposed Scale	0	544	281	135	348	94.4	37.4	1,440m <sup>2</sup>	8.8 per 100m <sup>2</sup>

As shown, the proposed public floor area has been reduced from 1,845m<sup>2</sup> to 1,440m<sup>2</sup>. The recommended car parking requirement is therefore **125** spaces based upon the rate of 8.7 spaces per 100m<sup>2</sup> public floor area. The proposal provides **127** car parking spaces, which represents a **2**-space surplus from the recommended provision.

#### 5 Considerations for a Conservative Assessment

The above assessment is conservative based upon the below considerations.

##### 5.1 Courtesy Bus

It is noted that neither the Marsden Brewhouse nor the Gregory Hills Hotel offer a courtesy bus service. The applicant has agreed to offer a pick-up and drop-off courtesy bus service to help offset the car parking demand. Based upon MTE data, typically 50% of pub patrons in a non-town centre area drive their own vehicle to the site. The remaining 50% comprise of passengers, public transport

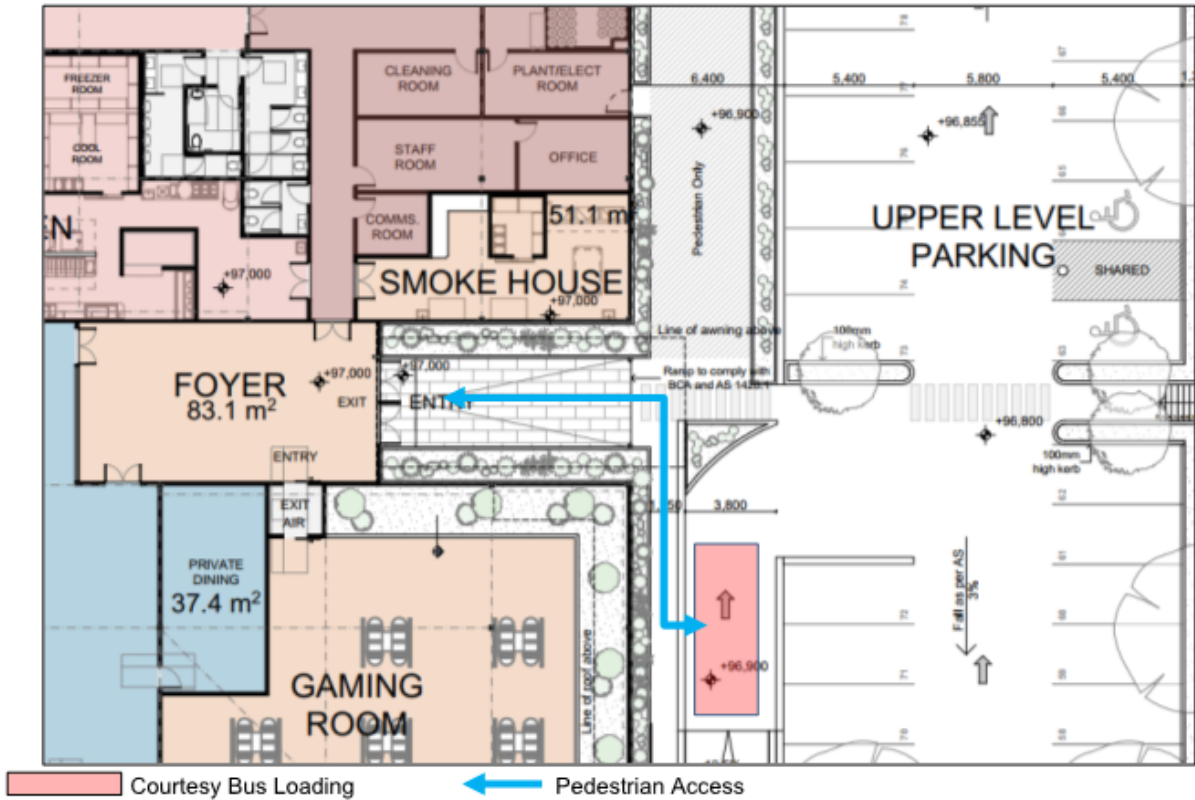


CLPP01

Attachment 6

users, bike riders, walkers, taxi passengers and other travel modes. Generally, the courtesy bus has the potential to reduce the car parking demand. To demonstrate this for a specific example a 15-seater van operating with six (6) passengers in a peak hour, three (3) of these patrons would have driven their own vehicle to the site and utilised a car parking space.

**Figure 4** demonstrates the location for the courtesy bus loading and unloading. The location is near the main pedestrian entrance for convenience.



**FIGURE 4: COURTESY BUS LOADING LOCATION**

**5.2 Proximity to Residential Zoning and Local Centres**

It is noted that the subject site is located within close proximity to a residential zone, just across Dick Johnson Drive. This is a similar scenario to the Marsden Brewhouse which is located across Richmond Road to residential zones. The proximity to residential zoning results in a higher likelihood that patrons from the residential area will walk to the subject site rather than drive and require the use of a car parking space. The Gregory Hills Hotel, by comparison, is located approximately 800m walking distance to residential zones.

Similarly, the Marsden Brewery is located 600m walking distance from a B2 Local Centre zone and the subject site is located 1km from a B2 Local Centre zone. The Gregory Hills Hotel is located 3.5km walking distance to the nearest B2 Local Centre zone. Proximity and reasonable walking distance to a Local Centre also increases the likelihood of walking trips and reduces the likely frequency of vehicle trips.

It is logical then that the Gregory Hills Hotel has a higher car parking provision than the Marsden Brewhouse. Given the Oran Park site's proximity to residential and local centre zoning, it is expected that its car parking demand will be more similar to the Marsden Brewhouse than the Gregory Hills Hotel.



CLPP01

Attachment 6

**6 Conclusion**

With respect to the above assessment, an appropriate parking provision for the subject development of 1,440m<sup>2</sup> public floor area is 125 spaces based upon a rate of 8.7 spaces per 100m<sup>2</sup> public floor area. The proposal has 127 car parking spaces, which satisfies the recommended car parking requirement. Additionally, a shuttle bus service will be implemented to reduce reliance on car parking spaces.

Please contact the undersigned on 9521 7199 should you require further information or assistance.

Yours faithfully,  
**McLaren Traffic Engineering**

**Craig McLaren**  
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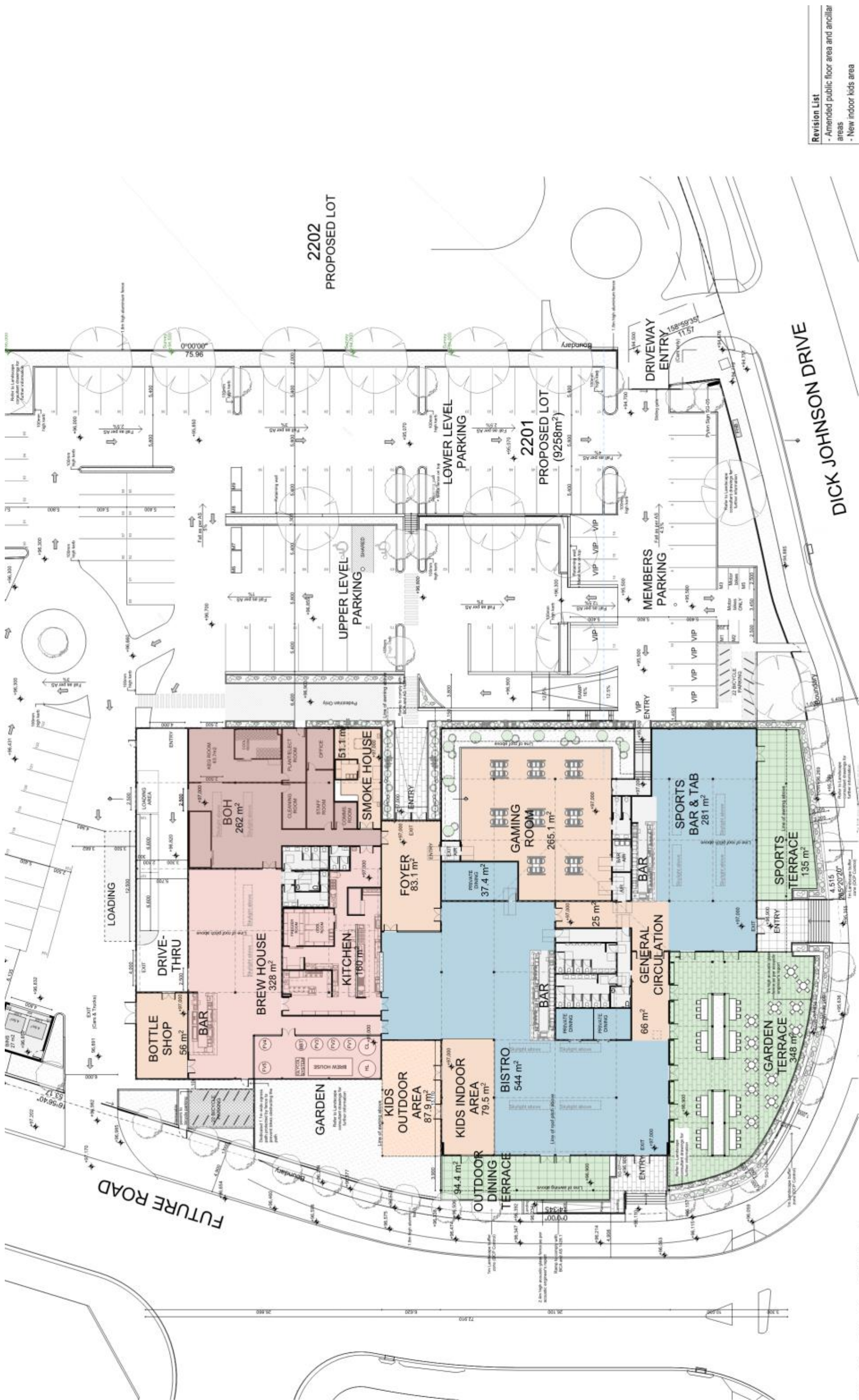
CLPP01

Attachment 6



**ANNEXURE A: PROPOSED PLANS  
(1 SHEET)**





**Revision List**

- Amended public floor area and ancillary areas
- New indoor kids area

Drawing No: **SK70**  
 Drawing content: **Ground Floor Plan - Public Floor Area**  
 Project: **Oran Park Tavern**  
 Project address: **Dick Johnson Drive**  
 Status: **Date**  
 Scale: **Scale**  
 Issue No.: **CLPP01**

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No	Description	Date
1	DA	30/05/2021
2	DA	01/04/2022
3	DA	21/07/2022
4	DA	02/08/2022

MC: Metal Cladding  
 GL: Glass  
 CL: Cladding  
 ST: Stone  
 BR: Brick

end:  
 --- Boundary Line  
 --- Trusses - Axis Line  
 --- Line of building above

1 Ground Floor Plan - Public Floor Area





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CLPP01

111 Steward Road, Oran Park  
**Peer Review of Development Application**  
**DA/2020/1220/1**



Prepared for

**CAMDEN COUNCIL**

By

**TPE Consulting Pty Ltd**

Attachment 7



### Report Documentation Control

<i>Title</i>	111 Steward Drive, Oran Park – Peer Review of Development Application DA/2020/1220/1
<i>Date</i>	16 August 2022
<i>Author(s)</i>	D.J. Drozd and D.J. Davidson
<i>Client</i>	CAMDEN COUNCIL
<i>Job No.</i>	202254
<i>Disclaimer</i>	<b>This report is believed to be true and correct at the time of writing. It is based on the information and data provided by the client and other relevant organisations during preparation. TPE Consulting Pty Ltd does not accept any contractual, tortious or other form of liability for any consequences arising from its use. People using the information in the report should apply and rely on their own skill and judgement to a particular issue they are considering.</b>

### EXECUTIVE SUMMARY

The purpose of this report is to provide a peer review assessment, as requested by Camden Council, of a development application (DA/2020/1220/1) for a Tavern ('Brew House') at the intersection of Steward Drive and Dick Johnson Drive, Oran Park.

The assessment covers development control plan (DCP) parking rates, for Camden as well as other Council's, and reviewing parking demand studies for existing sites. The assessment concludes that there is a shortfall in proposed parking provision and recommends that a reduction in floor space and /or increase to the parking supply to the development be sought or negotiated.

August 2022

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**1. INTRODUCTION**

**1.1 Background of the Report and Proposal**

TPE (Traffic, Planning and Environmental) Consulting Pty. Ltd. has been engaged by Camden Council to undertake a peer review to assess DA/2021/1220/1, being the proposed 'Brew House' at 111 Steward Drive, Oran Park.

The scope of the peer review is understood to be as follows:

- Assess documents received by Council regarding car parking provision for the site;
- Assess appropriateness of the comparison sites undertaken in the submission;
- Assessment of the proposed parking provisions and whether they are aligned to provisions within similar sites;
- Consideration of DCP rates for such use from comparable neighbouring Councils.

The location of the proposed development is shown in Figure 1 below.

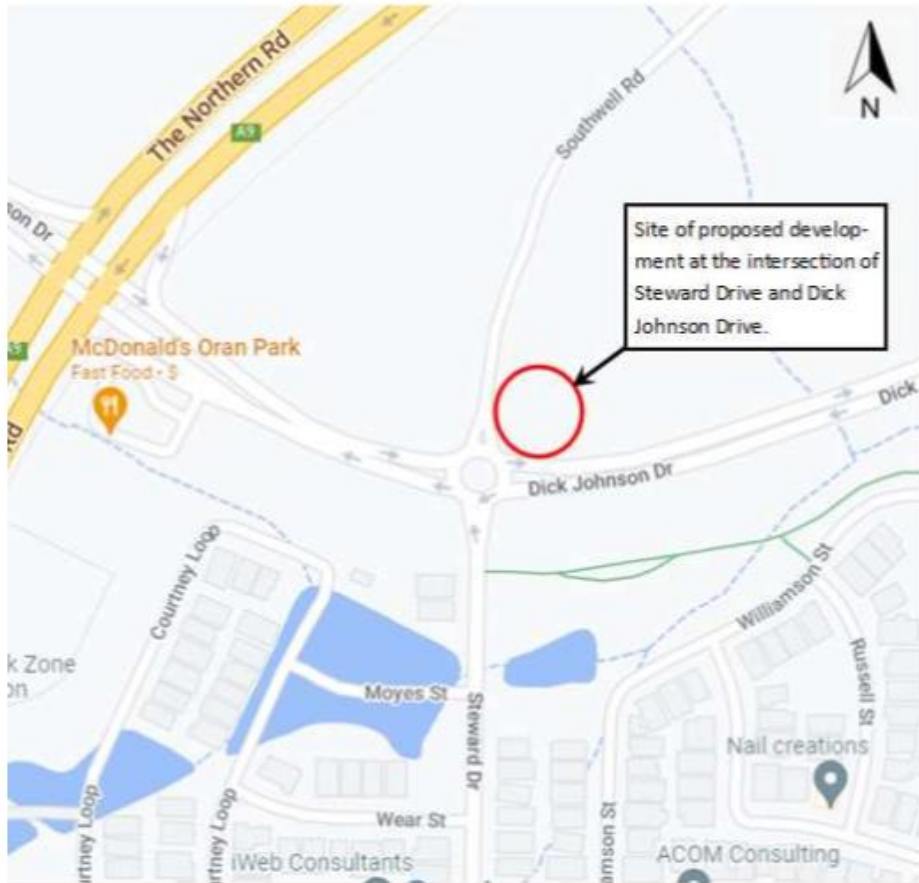


Figure 1: Proposed Development Location – Intersection of Steward Drive and Dick Johnson Drive, Oran Park.



In undertaking this peer review, TPE Consulting Pty. Ltd. has:

- Reviewed DA supporting materials in detail, including;
  - Traffic & Parking Assessment, prepared by John Cody Consulting, Ref. 17007, dated 21 June 2021.
  - Architectural Plans, prepared by ARCHEBIOSIS Architects, Issue 1, dated 30 April 2021.
  - Statement of Environmental Effects, prepared by Premise, Report No: 319187/SEE, Rev. Final, dated 30 June 2021.
  - Written Response to Council's RFI, prepared by Emily Lawson (Town Planner) for Premise, dated 29 June 2022.
  - Revised Traffic Impact Assessment, prepared by McLaren Traffic Engineering, Ref. 220585.01FA, dated 4 August 2022.
  - Revised Architectural Plans, prepared by ARCHEBIOSIS Architects, Issue 5, dated 8 August 2022.
- Assessed the proposed development against the DCP requirements of Camden Council;
- Made a comparative study of the DCP requirements of other Councils;
- Undertaken an independent parking demand study of an additional hotel site (Peachtree Hotel, Penrith);
- Made a comparative study of the parking demand studies undertaken by the applicant and TPE Consulting Pty. Ltd., and has drawn conclusions on the amount of parking required to facilitate the proposed development.

The application and supporting documents were submitted to Camden Council in July 2021, and the application is still under consideration for determination of approval or otherwise at the time of writing this report.

## 1.2 Summary of Parking Matters

In review of parking considerations for the proposed development we undertook to independently assess the proposal which is before Council. In doing so we have duly considered, without predisposition, whether the applicant's proposal would suffice to meet the parking needs of the development within its surrounding context, without causing undue parking pressure on frontage roads and side streets such that may adversely affect other road users and adjacent land use.

A concise outline of our findings is summarised in point form below, with details outlined in the report following:

1. The proposed parking provision falls significantly below Council's DCP requirement (shortfall in the vicinity of 298 spaces).
2. The proposed parking provision falls below the DCP requirements of other nearby Councils, both individually and when an average is applied, by a significant amount (average shortfall of approximately 165 spaces).
3. Variation of Council's DCP parking rate has already been considered by way of allowing parking calculations to be demonstrated via surveys of other sites, along with a supportive Traffic Impact Assessment.



- 4. However, even with a variation applied, there is still an apparent shortfall in proposed parking provision, particularly when demonstrable data adjustments are applied (shortfall in the vicinity of 22.5% - 35%).
- 5. Therefore we can say that on balance there is an overall shortfall in parking provision for this proposal.

The report concludes recommending further discussion with the proponent with regard to possible amendments to proposed floor plans suitable to the site.

**2. OVERALL ASSESSMENT IN BRIEF**

**2.1 Traffic Generation, Access & Circulation, and SEPP Infrastructure 2021 Requirements**

A brief review of the various traffic components, other than the main parking matters, was undertaken by TPE Consulting Pty. Ltd. for contextual purposes and is summarised below.

Access and Circulation

Access and circulation is precipitated by several one-way aisles (and ramp), which connect to the internal roundabout. The dimensions of internal aisles and car parking appears to be compliant with AS2890.1. It was also noted that may be conflicts in the swept paths of service vehicles with parking spaces in some locations.

The proposed access and circulation of a B85 vehicle is shown in Figure 2 below (Source: *Traffic & Parking Assessment*, prepared by John Cody Consulting, Ref. 17007, dated 21 June 2021, page 23).



Figure 1: Proposed Access and Circulation (B85 vehicle).





### Traffic Generation

Traffic generation has been assessed as having a traffic generation potential, during the weekday PM peak period, in the order of 65-125 vph, with the AM peak being generally accepted as minimal for pubs and clubs (Source: *Traffic & Parking Assessment*, prepared by John Cody Consulting, Ref. 17007, dated 21 June 2021).

### SEPP (Transport and Infrastructure) 2021

Referral to Transport for NSW, under Schedule 3 SEPP (Transport and Infrastructure) 2021, is not required for this proposal. However, it is noted, from review of documents associated with the application, the TNSW has been referred some elements of the proposal for concurrence.

## 3. DETAILED PARKING ASSESSMENT

The scope of this assessment and entire report is to review the car parking provision for the site, a detailed assessment of parking matters follows, addressing both the original and revised proposals and DCP rates as well as those derived from parking demand studies.

### 3.1 Overview of Original Proposal for Comparative Purposes

The original proposal for a Pub/Tavern/Brewhouse at 111 Steward Drive, Oran Park, was submitted (in July 2021) to Council under the same application as that being currently assessed by Council for the site. It is based on the Architectural Plans, prepared by ARCHEBIOSIS Architects, Issue 1, dated 30 April 2021 (see Appendix 2). It was proposed to provide 132 off-street car parking spaces. The following calculations for that proposal have been included in this report for comparative purposes.

It is noted that there is a disparity between the GFA stated in the applicant's supportive documents and that which we have identified, which is due to the inclusion below of outdoor and semi-outdoor areas of operation (such as outdoor terrace areas). It is appropriate that these areas be included in GFA as they comprise constructed operational zones attracting patrons and requiring parking facilitation along with the rest of the development.

We have calculated the original total GFA of the original proposal at 2,762.3m<sup>2</sup>, which comprises of 2,184.9m<sup>2</sup> of indoor operational areas (includes Foyer 83.1m<sup>2</sup>, Back of House 262m<sup>2</sup>, Brew House 330.7m<sup>2</sup>, Kitchen 160m<sup>2</sup>, Smoke House 51.1m<sup>2</sup>, Private Dining 37.4m<sup>2</sup>, Gaming Room 265.1m<sup>2</sup>, Sports Bar 330.5m<sup>2</sup>, and Bistro 665m<sup>2</sup>) and an additional 577.4m<sup>2</sup> of operational outdoor or semi-outdoor areas (Outdoor Dining Terrace 94.4m<sup>2</sup>, Sports Terrace 135m<sup>2</sup>, and Garden Terrace 348m<sup>2</sup>).

Using a composite breakdown for DCP assessment purposes, deriving from the total public floor area, the GFA of public bar area (i.e. the bar area component within each of the Brew House, Bistro and Sports Bar) has been estimated conservatively at 100m<sup>2</sup>.

Calculated separately to the above is the GFA of lounge/dining/beer garden areas (which comprised Brew House + Bistro + Private Dining + Gaming Room + Sports Bar + Outdoor Dining Terrace + Garden Terrace + Sports Terrace, less the bar area component already calculated), which is 2,106.1m<sup>2</sup>.

The bottle shop area of 50.8m<sup>2</sup> has been calculated separately as a retail component, which requires 1.7 parking spaces under the Camden DCP.

Based on DCP parking rates, the required parking provision has been calculated in **Table 1** below, comparing Camden Council's requirement with other nearby Council areas.



Table 1: Overview of Original Proposal Against DCP rates for Camden and Other LGAs.

<u>Council / Authority</u>	<u>Category</u>	<u>Rate***</u>	<u>Requirement if applied to this DA*</u>
RTA Guidelines	Categorised, but no rate given.	N/A	N/A
Camden Council	Pub / Registered Club / Restricted Premises	Provision for coaches to pick up and set down may also be required for large establishments. 1 car parking space per 2m <sup>2</sup> GFA of public bar area and 1 car parking space per 5m <sup>2</sup> GFA of lounge, beer garden, auditorium, billiard room, restaurant and 25 car parking spaces per 100m <sup>2</sup> of remaining public floor area. Developments must also accommodate: 1 bicycle space per 25 car parking spaces in excess of the first 25 car parking spaces; and 1 motorcycle space per 50 car parking spaces in excess of the first 50 car parking spaces.	50 + 421 + 25 + 1.7 (i.e. 25 and 1.7 being for Foyer area and Bottle shop respectively) = <u>498</u> parking spaces. (Shortfall = 366).
Penrith City Council	Pubs/Registered Clubs	1 space per 4m <sup>2</sup> of bar floor area plus 1 per 6m <sup>2</sup> lounge and dining room.	25 + 351 + 1.7*** = <u>378</u> parking spaces. (Shortfall = 246).
Blacktown City Council	Pub and registered club.	Land zoned B3 Commercial Core or B4 Mixed Use Assessed on merit. Elsewhere 1 space per 18.5sq.m GFA.	(2,762.3 ÷ 18.5) + 1.7*** = <u>151</u> parking spaces. (Shortfall = 19).
Campbelltown City Council	Registered Club	1.5 spaces per 10m <sup>2</sup> GFA	1.5 x (2,762.3 ÷ 10) + 1.7*** = <u>416</u> parking spaces. (Shortfall = 284).
Hills Shire Council	Pubs/ Registered Clubs	1 space per 1.85m <sup>2</sup> of service area in bar and lounge plus 1 space per 2 employees	359 + 15 + 1.7*** = <u>376</u> parking spaces ("service area" calculated for Bistro/ main bar service area only, then added to staff parking**). (Shortfall = 244).



Hawkesbury City Council	Hotel and Licensed Club	1 space per 10m <sup>2</sup> of service area/bar area, plus 1 space per 2 employees.	184 + 15 + 1.7*** = <u>201</u> parking spaces (calculation did not include gaming room in this instance*). (Shortfall = 69).
<b>Average shortfall of proposed parking for Oran Park Brew House</b>	-	-	<b>204.6 parking spaces</b> (average shortfall).

\*There are interpretive decisions to be made in undertaking any assessment based on floor space usage, with regards to DCP descriptions and their application. As such the above DCP parking calculations, derived through this assessment process, are indicative and may vary from one officer's assessment to the next.

\*\* (Noting that the SOE for the proposed development, on page 17, states that "The proposed Tavern is expected to employ a typical maximum workforce of approximately 15 persons on the site at any time, increasing up to 30 persons on Friday/Saturday night-time if live entertainment is provided"

\*\*\*Rather than apply a different retail parking rate for the bottle shop component in each instance, we elected to use the Camden DCP retail rate, being 1 space per 30m<sup>2</sup> for premises under 200m<sup>2</sup>, and is factored into each calculation.

The analysis that follows, over the proceeding sections of the report, indicates that the revisions proposed do not represent a significant improvement in parking shortfall with regard to either the DCP requirements (as above) or variations based on survey results.

### 3.2 Revised Proposal

The revised proposal for a Pub/Tavern/Brewhouse at 111 Steward Drive, Oran Park, is based on the Revised Architectural Plans, prepared by ARCHEBIOSIS Architects, Issue 5, dated 8 August 2022 (see Appendix 3). It is proposed to provide 127 off-street car parking spaces.

The revision also proposes a reduction in the Sports Bar from 330.5m<sup>2</sup> to 281m<sup>2</sup> and a reduction in the Bistro from 665m<sup>2</sup> to 544m<sup>2</sup>, although it is noted that neither the floor plan nor the dimensions on the revised architectural plans has changed for the Sports Bar and Bistro areas, but rather the notation has been altered to denote the smaller figures. However, it is understood that architectural plans may be indicative prior to finalisation for the construction stage.

In the case of the Bistro area, there is now the inclusion of a newly proposed 79.5m<sup>2</sup> "Kids Indoor Area", which is factored into GFA calculations. Our position is that the area can readily be repurposed for another use (particularly given its proximity to the main bar and associated seating and tables), and it is therefore considered appropriate to include this area in the GFA for parking calculation purposes.

As outlined separately, in **Section 3.4** of this report, the revised architectural plans and accompanying traffic statement indicate that the Brew House is no longer proposed to be made available for public use, though it has evidently been designed for that purpose. Nevertheless, even when the Brew House is excluded from the calculations below, a shortfall in parking provision is still evident when reviewing DCP rates for Camden, as well as DCP rates for other LGAs.

We have calculated the revised total GFA of the proposal at 2,668.6m<sup>2</sup>, which comprises of 2,091.2m<sup>2</sup> of indoor operational areas (includes Foyer 83.1m<sup>2</sup>, Back of House 262m<sup>2</sup>, Brew House 328m<sup>2</sup>, Kitchen 160m<sup>2</sup>,



Smoke House 51.1m<sup>2</sup>, Private Dining 37.4m<sup>2</sup>, Gaming Room 265.1m<sup>2</sup>, Sports Bar revised to 281m<sup>2</sup>, additional Kids Indoor Area 79.5m<sup>2</sup> and Bistro revised to 544m<sup>2</sup>, and an additional 577.4m<sup>2</sup> of operational outdoor or semi-outdoor areas (Outdoor Dining Terrace 94.4m<sup>2</sup>, Sports Terrace 135m<sup>2</sup>, and Garden Terrace 348m<sup>2</sup>). The bottle shop area of 50.8m<sup>2</sup> has been calculated separately as a retail component, which requires 1.7 parking spaces under the Camden DCP.

### 3.3 Proposed Gaming Room Exclusion

The Revised Traffic Impact Assessment, (prepared by McLaren Traffic Engineering, Ref. 220585.01FA, dated 4 August 2022, on page 3), states that:

*“(1) The public floor area includes pub areas, restaurants, terraces, dining areas and other areas which are primary patron generators. It excludes ancillary areas such as smoking rooms, gardens, kids areas, gaming rooms, kitchen and back of house which do not directly generate patron car parking demand.”*

However, for the purposes of our assessment the gaming room has been included in calculations as an attractor for patrons and, although ancillary in some respects relative to other operations, it is considered similar in nature to lounge areas which are routinely included in parking calculations for pubs and clubs.

### 3.4 Proposed ‘Brew House’ Area Exclusion

Although it is proposed by the applicant that the ‘Brew House’ area is intended for use by staff only, it is noted that this area can easily be made available for public use at any time without modification, and noting also that revised architectural plans indicate large exterior rooftop advertising signage which states “Brew House” (Archebiosis Architects, Drawing No: SK505, Issue no: 1, dated 7 March 2022).

The original proposal, which is still represented in the revised architectural plans, was for the Brew House to be a publicly accessible area. If an operational decision has been made that the Brew House bar, and adjacent area (not including the brewing area itself) is no longer to be accessed by the public, then there exists the opportunity for a review of the floor plans such that may allow for additional parking.

This is a matter where further discussion between Council and the applicant may be fruitful. Nevertheless, in its current form, the floor plans have been designed with the Brew House area as a public area (including a bar and large open space for seating/tables/etc.) and there may be a proclivity to revert this area back to public use. It is noted that an approval could be conditioned by Council to ensure that this area is not for public use, but again that begs the question as to whether that might be able to be accommodated into a revised floor plan which facilitates additional parking onsite.

### 3.5 Assessment Against Camden’s DCP Requirements

Camden Council’s ‘Camden Development Control Plan 2019’ contains the following parking requirements for developments categorised as a ‘Pub/Registered Club/or Restricted Premises’:

*“Provision for coaches to pick up and set down may also be required for large establishments. 1 car parking space per 2m<sup>2</sup> GFA of public bar area and 1 car parking space per 5m<sup>2</sup> GFA of lounge, beer garden, auditorium, billiard room, restaurant and 25 car parking spaces per 100m<sup>2</sup> of remaining public floor area. Developments must also accommodate: 1 bicycle space per 25 car parking spaces in excess of the first 25 car parking spaces; and 1 motorcycle space per 50 car parking spaces in excess of the first 50 car parking spaces”.*



For the first of the two floor area components of the above, we conservatively apportioned “public bar area” as the actual bars themselves and immediate standing/serving areas adjacent. In assessing DAs for pubs and clubs it is not uncommon to apply that categorisation to the bar, standing/serving area and all associated floor space for seating in the immediate vicinity or in the same room. Nevertheless a conservative approach has been deemed appropriate this assessment, especially since the DCP requirements turn out to be very high in either scenario.

Using a composite breakdown for DCP assessment purposes, deriving from the total public floor area, the GFA of public bar area (i.e. the bar area component within each of the Bistro and Sports Bar) has been estimated conservatively at 80m<sup>2</sup>, having removed the Brew House bar (shown on plans) from the calculations.

Calculated separately to the above is the GFA of lounge/dining/beer garden areas (which comprises Bistro + Private Dining + Gaming Room + Sports Bar + Outdoor Dining Terrace + Garden Terrace + Sports Terrace), which is 1,704.9m<sup>2</sup>.

The breakdown is as follows:

- 1 car space per 2m<sup>2</sup> of public bar area equates to 80m<sup>2</sup> divided by 2, which is 40 car spaces.
- 1 space per 5m<sup>2</sup> of lounge, beer garden, auditorium, billiard room, and restaurant equates to 1,704.9m<sup>2</sup> divided by 5, which is 340 parking spaces.
- An additional 43.5 parking spaces is applied in this instance as the remaining public floor area is calculated as being 174.1m<sup>2</sup> (Foyer at 83.1m<sup>2</sup> and “general circulation” areas of 66m<sup>2</sup> and 25m<sup>2</sup>, respectively as shown on the revised plans), plus 1.7 spaces for the bottle shop/retail component.
- The resulting calculation is a combination of the above, being 40 + 340 + 45 = 425 spaces.

As such the DCP requirement for a development of this type, within the Camden LGA, is in this instance 425 car parking spaces. The proposed development intends to provide 127 spaces, which represents a shortfall of 298 parking spaces.

### 3.6 Comparison with Parking Requirements of Other Councils

The following comparison (as it relates to the revised proposal) presents the DCP requirements of five nearby Council's, with regard to pub and club developments, compared to Camden (as well as mentioning the ‘RTA Guide to Traffic Generating Development’). Similarly to Table 1, which presents the original proposal and GFA, the information that follows indicates that many Councils have a higher expectation of parking provision than what is borne out in the site surveys of similar venues (which are approved and operating) in Sections 4.1 and 4.2 of this report.

Based on DCP parking rates, the required parking provision has been calculated in **Table 2** below, comparing Camden Council's requirement with other nearby Council areas.

**Table 2: Councils Parking Rates Comparison**

<b>Council / Authority</b>	<b>Category</b>	<b>Rate***</b>	<b>Requirement if applied to this DA*</b>
RTA Guidelines	Categorised, but no rate given.	N/A	N/A
Camden Council	Pub / Registered Club / Restricted Premises	Provision for coaches to pick up and set down may also be required for large establishments. 1 car parking	<u>425</u> parking spaces. (Shortfall = 298).

August 2022

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		space per 2m <sup>2</sup> GFA of public bar area and 1 car parking space per 5m <sup>2</sup> GFA of lounge, beer garden, auditorium, billiard room, restaurant and 25 car parking spaces per 100m <sup>2</sup> of remaining public floor area. Developments must also accommodate: 1 bicycle space per 25 car parking spaces in excess of the first 25 car parking spaces; and 1 motorcycle space per 50 car parking spaces in excess of the first 50 car parking spaces.	
Penrith City Council	Pubs/Registered Clubs	1 space per 4m <sup>2</sup> of bar floor area plus 1 per 6m <sup>2</sup> lounge and dining room.	<u>304</u> + 1.7 <sup>***</sup> parking spaces. (Shortfall = 179).
Blacktown City Council	Pub and registered club.	Land zoned B3 Commercial Core or B4 Mixed Use Assessed on merit. Elsewhere 1 space per 18.5sq.m GFA.	<u>144.3</u> + 1.7 <sup>***</sup> parking spaces. (Shortfall = 19).
Campbelltown City Council	Registered Club	1.5 spaces per 10m <sup>2</sup> GFA	<u>400.3</u> + 1.7 <sup>***</sup> parking spaces. (Shortfall = 275).
Hills Shire Council	Pubs/Registered Clubs	1 space per 1.85m <sup>2</sup> of service area in bar and lounge plus 1 space per 2 employees	<u>309.1</u> + 1.7 <sup>***</sup> parking spaces. (Shortfall = 184)**.
Hawkesbury City Council	Hotel and Licensed Club	1 space per 10m <sup>2</sup> of service area/bar area, plus 1 space per 2 employees.	<u>159</u> + 1.7 <sup>***</sup> parking spaces (excluding gaming room)* (Shortfall = 34).
<b>Average shortfall of proposed parking for Oran Park Brew House</b>	-	-	<b>164.8 parking spaces (average shortfall).</b>

\*There are interpretive decisions to be made in undertaking any assessment based on floor space usage, with regards to DCP descriptions and their exact application. As such the above DCP parking calculations, derived through this assessment process, are indicative and may vary from one officer's assessment to the next.

\*\* (Noting that the SOE for the proposed development, on page 17, states that "The proposed Tavern is expected to employ a typical maximum workforce of approximately 15 persons on the site at any time, increasing up to 30 persons on Friday/Saturday night-time if live entertainment is provided".

\*\*\*Rather than apply a different retail parking rate for the bottle shop component in each instance, we elected to use the Camden DCP retail rate, being 1 space per 30m<sup>2</sup> for premises under 200m<sup>2</sup>, and is factored into each calculation.



It is evident, from the above Table, that the proposed revisions to the development do not have any significant parking gains (with regard to DCP requirements) when compared with the original proposal. In both cases the parking provision falls well below what is required.

#### 4. COMPARATIVE STUDY OF SIMILAR SITES

This assessment has considered parking demand studies, both from the applicant and that which TPE Consulting Pty. Ltd. has undertaken itself, as a means to determining the potential parking needs of the proposed development more accurately.

##### 4.1 Parking Demand Study – Peachtree Hotel

For the purposes of this report, TPE Consulting Pty. Ltd. undertook a site inspection and parking survey at Peachtree Hotel, Penrith, on Sunday 24 July 2022, between 11:45am and 12:45pm. At the time of inspection the onsite car parking was observed to be at more than 100% parking capacity. The adjacent street frontage and side-street kerbside parking was also 100% occupied at a time when the frontage and surrounding industrial businesses were closed, with a regular stream of patrons observed walking from on-street parked vehicles to/from the venue. It is noted that the Hotel dimensions and internal layout are of similar proportions to proposed development

Peachtree Hotel provides 142 spaces plus overflow parking for approximately 20 vehicles. The public floor area is derived accurately from aerial photography (to ascertain the GFA), and then applying the assumption that the public floorspace will be approximately 75% of GFA (using the same convention proposed and employed in the Revised Traffic Impact Assessment, prepared by McLaren Traffic Engineering, Ref. 220585.01FA, dated 4 August 2022, on page 3, note 2, in subscript to their Table 1).

As the GFA is measured at 1,858m<sup>2</sup>, the public floorspace (excluding outdoor play areas and bottle shop) is estimated to be approximately 1,394m<sup>2</sup>. The overall GFA estimate is shown in the photograph below.

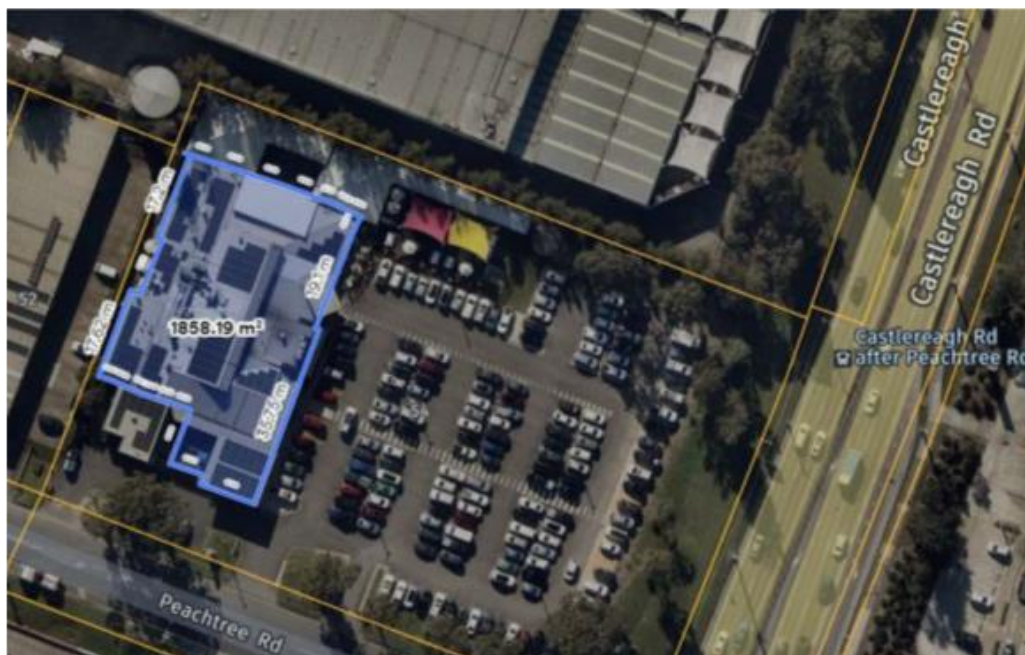


Figure 3: Peachtree Hotel, Penrith – Aerial photograph showing GFA estimate.

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Attachment 7

The Peachtree Hotel site has similar public transport access (fronted by a bus stop) and locational attributes to the proposed development, and other sites assessed for this DA. The application of the above yields a parking rate of approximately 11.6 spaces per 100m<sup>2</sup> of public floor area for the Peachtree Hotel, however it is evident from the site inspection that this rate does not suffice for this site during times of peak demand.

The images below were taken on site at the Peachtree Hotel on 24 July 2022.



The site inspection and parking survey showed this rate to be failing to meet the parking demands of the business. Numbers of patrons were observed accessing the premises from on-street parking locations as the available internal and overflow parking areas were at capacity. Many vehicles were also parked informally on verges and other areas as shown in the photographs (above) taken onsite at the time of inspection.

Whilst the parking rate observed for this hotel is similar to others studied for the purposes of this DA (undertaken by the applicant), the photos above clearly show that the rate does not facilitate times of peak parking demand (in the instance above, Sunday lunch time).





## 4.2 Review of Applicant Parking Demand Studies and Comparisons

The Revised Traffic Impact Assessment (prepared by McLaren Traffic Engineering, Ref. 220585.01FA, dated 4 August 2022) for Gregory Hills Hotel (which has 165 spaces) and Marsden Brewhouse (which has 124 spaces, assessed for the pub component of the development). It is noted, however, that the Marsden Brewhouse actually provides 266 car parking spaces as a result of ancillary uses (such as a function centre) included in the development (reference: <https://www.planningportal.nsw.gov.au/planning-panel/pub-and-brewery>).

Whilst this additional Marsden Brewhouse parking provision is correctly not applied to the pub component of the development, it does provide the benefit of overflow parking when the function centre and other ancillary areas are not in full use. This is not the case at Oran Park where any overflow parking will be forced into the surrounding streets and/or onto landscaped areas and verges (as is the case in the Peachtree Hotel example).

These adjustments are shown in **Table 3** below.

**Table 3: Oran Park Brew House Comparative Study in Review**

Site	Zoning	Surrounding Zoning	Courtesy Bus Operated	Public Transport	Parking Provision	Public Floor Area <sup>(1)(2)</sup>	Parking Provision (per 100m <sup>2</sup> )
Marsden Brewhouse	B7	IN2, B5	No	Bus Stops within 200m walk	124	1,651m <sup>2</sup>	7.5 per 100m <sup>2</sup>
Gregory Hills Hotel	B5	IN1	No	Frontage Bus Stops	165	1,689m <sup>2</sup>	9.8 per 100m <sup>2</sup>
Peachtree Hotel	IN1	IN1, RE1	Yes, Friday nights only.	Frontage Bus Stops	142 + 20 overflow spaces	1,394m <sup>2</sup>	11.6 per 100m <sup>2</sup>
Average	-	-	-	-	-	-	9.6 per 100m <sup>2</sup>

(1) Marsden Brewhouse and Gregory Hills Hotel data referenced from applicant's Revised Traffic Impact Assessment report, except where otherwise stated.

(2) Where floor plans are unavailable, public floor area is assumed to be 75% of GFA.

A parking shortfall is still evident when the corrected average parking provision of 9.6 spaces per 100m<sup>2</sup>, from **Table 3** above, is applied to the proposed Oran Park development. Even without taking into account the significant exclusions of the Brew House and Gaming Room areas, this would result in a parking requirement of 138 spaces. When the Gaming Room is included in the calculations (giving a total public area of 1,705m<sup>2</sup>), as is the recommendation of this report, the resulting parking requirement would be 164 spaces (which would leave a shortfall of 37 spaces, around 22.5%).

When both the Brew House and the Gaming Room are included in the calculations (giving a total public area of 2,033m<sup>2</sup>), the resulting parking requirement would be 195 spaces (which would leave a shortfall of 68 spaces, roughly 35%). Summarised below:

1. Parking required when Gaming Room & Brew House are excluded = 138 spaces (shortfall = 11).
2. Parking required when Gaming Room only is included = 164 spaces (shortfall = 37 spaces or 22.5%).
3. Parking required when both the Gaming Room & Brew House are included = 195 spaces (shortfall = 68 spaces or 35%).



The scenario outlined in point 1 above (where both the Gaming Room and Brew House are excluded from calculations) is not accepted as a reasonable position for the purposes of this assessment.

The inclusion of the Brew House in the calculations is still a matter for Council's consideration, due to the noted proclivity for this area to be made publicly accessible (with floor plans showing a bar area within it, and the Brew House itself being an advertised attraction for the site via proposed rooftop advertising), even though the applicant advises this will not be the case.

#### **4.3 Variations to DCP Requirements**

It is generally accepted that there are many circumstances where departures from DCP requirements can be accommodated, particularly if addressed via other mitigating measures or solutions, such as dual or complementary uses.

The Camden Council DCP 2019 states the following with regards to the variation of car parking rates from those provided: *"Council may consider variations to parking rates in certain circumstances that do not warrant demand and may be supported by a car parking and traffic impact assessment study submitted with a development application. Council will give consideration to other features of the development, such as proposed maximum staffing levels, expected customer levels etc. where warranted"*.

However, the proposed use of the parking calculation rate of 9.6 spaces per 100m<sup>2</sup> (derived from survey of other sites and detailed in Sections 4.1 and 4.2 of this report) is in itself a considerable variation as it represents a significant departure from the DCP rate, which may still lead to situations of parking demand exceeding capacity during peak use (as shown in the Peachtree Hotel study in Section 4.1 of this report). In that sense allowance has already been made for variation to the DCP requirements based on the specific features of the development.



## 5. CONCLUSION

This report documents a peer review assessment of the proposed Pub/Tavern/Brewhouse development at 111 Steward Drive, Oran Park. It is based on the Revised Architectural Plans, prepared by ARCHEBIOSIS Architects, Issue 5, dated 8 August 2022 (see Appendix 3), which proposes to provide 127 off-street car parking spaces.

Key findings include:

- The parking needs of the development, when assessed against the Camden DCP, is not met and the proposed parking provision falls short by approximately 298 spaces. This is high and suggests that a variation from the DCP rate may warrant consideration.
- The proposed parking provision falls short by an average of approximately 165 spaces when assessed against the DCPs of other neighbouring Councils. The five other Councils reviewed were Penrith, Blacktown, Hills, Hawkesbury and Campbelltown, with an average derived from the DCPs of all five.
- Lastly, as a result of reviewing similar pub/tavern/brewhouse sites, with assessment against parking rates derived from parking demand studies, the data generated indicates that the proposed parking for the development falls short by approximately 37 to 68 spaces (or approximately 22.5% - 35%).

Therefore when considering the key findings within this peer review we propose that on balance there is an overall shortfall in parking provision for this proposal. Further discussion is recommended with the proponents for amending floor space and /or increasing parking supply on the site.

### Recommendation

As the quantum of car parking is not sufficient to support the proposal in its current form, TPE Consulting Pty Ltd recommends Council negotiate a reduction in floor space and/or increase to the parking provision for this development.

## 6. ATTACHMENTS

Title	Pages
1. <i>APPENDIX 1 – PEACHTREE HOTEL PARKING DEMAND STUDY DATA</i>	1 Page
2. <i>APPENDIX 2 – EXCERPT FROM ORIGINAL ARCHITECTURAL PLANS (30/4/2021)</i>	2 Pages
3. <i>APPENDIX 2 - EXCERPT FROM REVISED ARCHITECTURAL PLANS (8/8/2022)</i>	2 Pages

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APPENDIX 1 – PEACHTREE HOTEL PARKING DEMAND STUDY DATA

	A	B	C	D	E	F	G
1	<b>Parking Demand Study Data - Peachtree Hotel</b>						
2	Date of Survey: Sunday 24 July 2022						
3							
4			<u>Capacity</u>	<u>Vehicles Parked</u>	<u>Percentage Occupied</u>	<u>Field Notes</u>	
5		Parking Aisle 1	26	26	100%		
6		Parking Aisle 2	28	28	100%		
7		Parking Aisle 3	40	40	100%		
8		Parking Aisle 4	20	20	100%		
9		Parking Aisle 5	12	12	100%		
10		Parking Aisle 6	13	13	100%	2 vehicles parked on landscaped area adjacent to overflow parking area	
11		Parking Aisle 7	3	3	100%	3 vehicles parked on landscaped area adjacent to overflow parking area	
12		Overflow Parking	20	20	100%	1 vehicle parked on landscaped area adjacent to overflow parking area	
13		On-Street Parking Within 100m Radius	Approximately 35	32	91%	Some groups of patrons observed to access the hotel from vehicles parked on nearby street frontage. Other businesses mostly closed. Not operating on Sundays	

August 2022

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**CLPP01**

**Attachment 7**

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APPENDIX 2 – EXCERPT FROM ORIGINAL ARCHITECTURAL PLANS (DATED APRIL 2021)

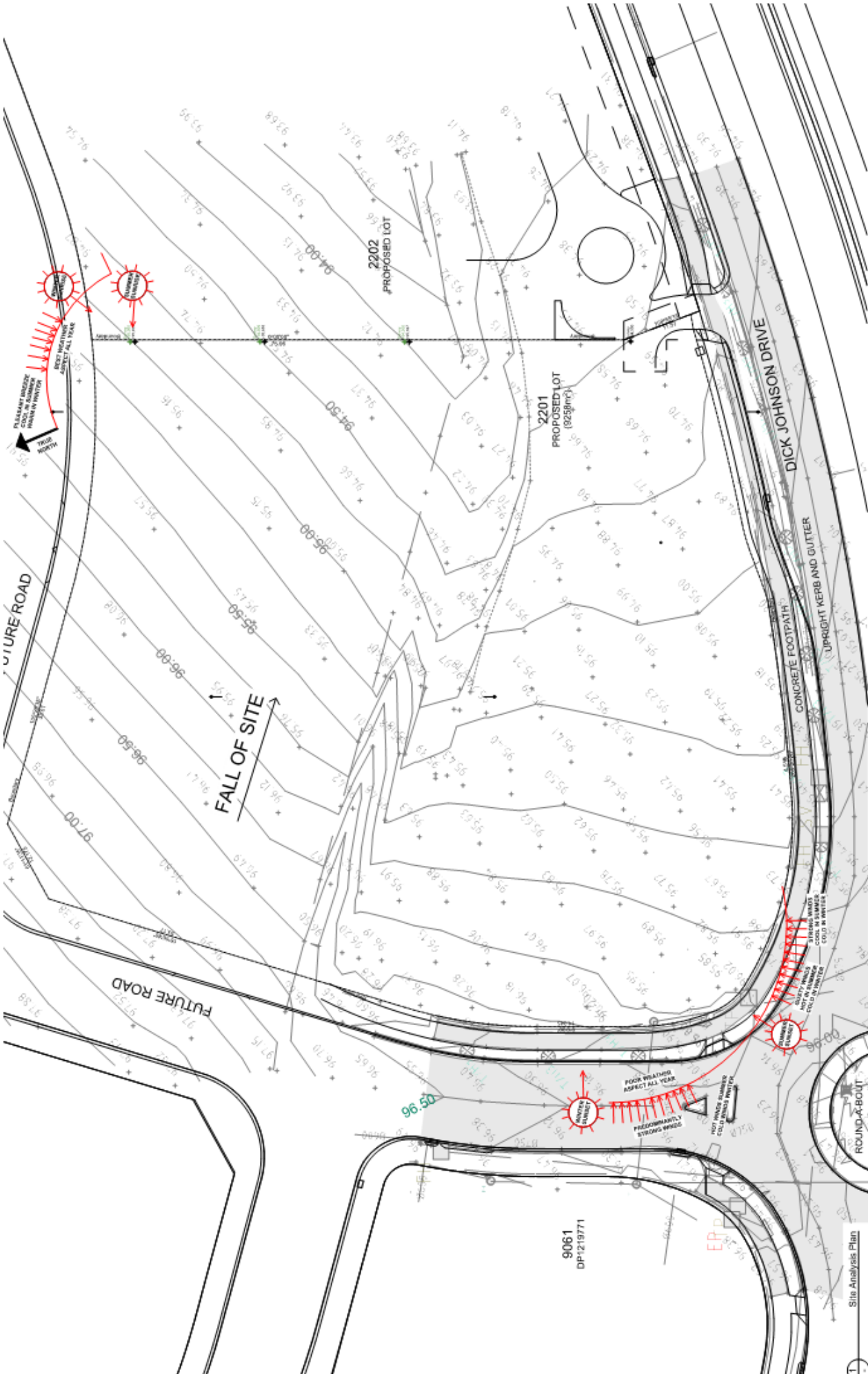




APPENDIX 3 – EXCERPT FROM REVISED ARCHITECTURAL PLANS (DATED AUGUST 2022)







Drawing | SK1  
 Drawing content | Site Analysis Plan  
 Project | Ocean Park Tavern  
 Project address | Dick Johnson Drive  
 Shop 38, 781 Burke St, Seaford NSW 3018  
 www.archebiosis.com  
 P 02 8392 2334  
 E archebiosis@archebiosis.com  
 Archebiosis Architects is the owner of the copyright of these ARCHITECTS

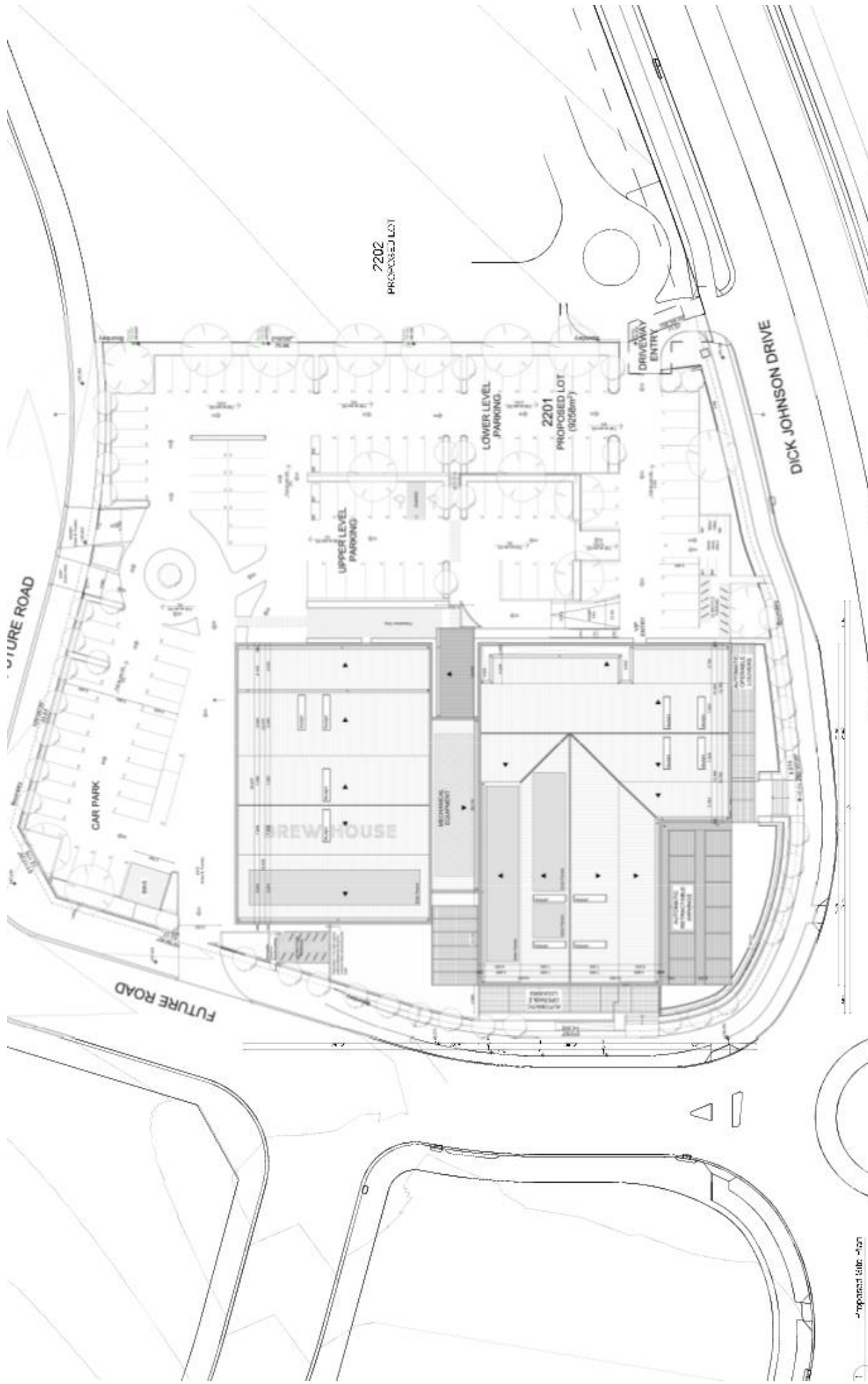
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2	SA	07/01/2022
3	SA	27/07/2022

MC: Metal Cladding  
 GL: Grass  
 CL: Cladding  
 ST: Stone

Boundary Line  
 Trusses - Axis Line  
 Line of building above

Attachment 8 CLPP01

Attachment 8 CLPP01



Proposed Sub-division

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 3. 0.00 0.00 0.00  
 4. 0.00 0.00 0.00

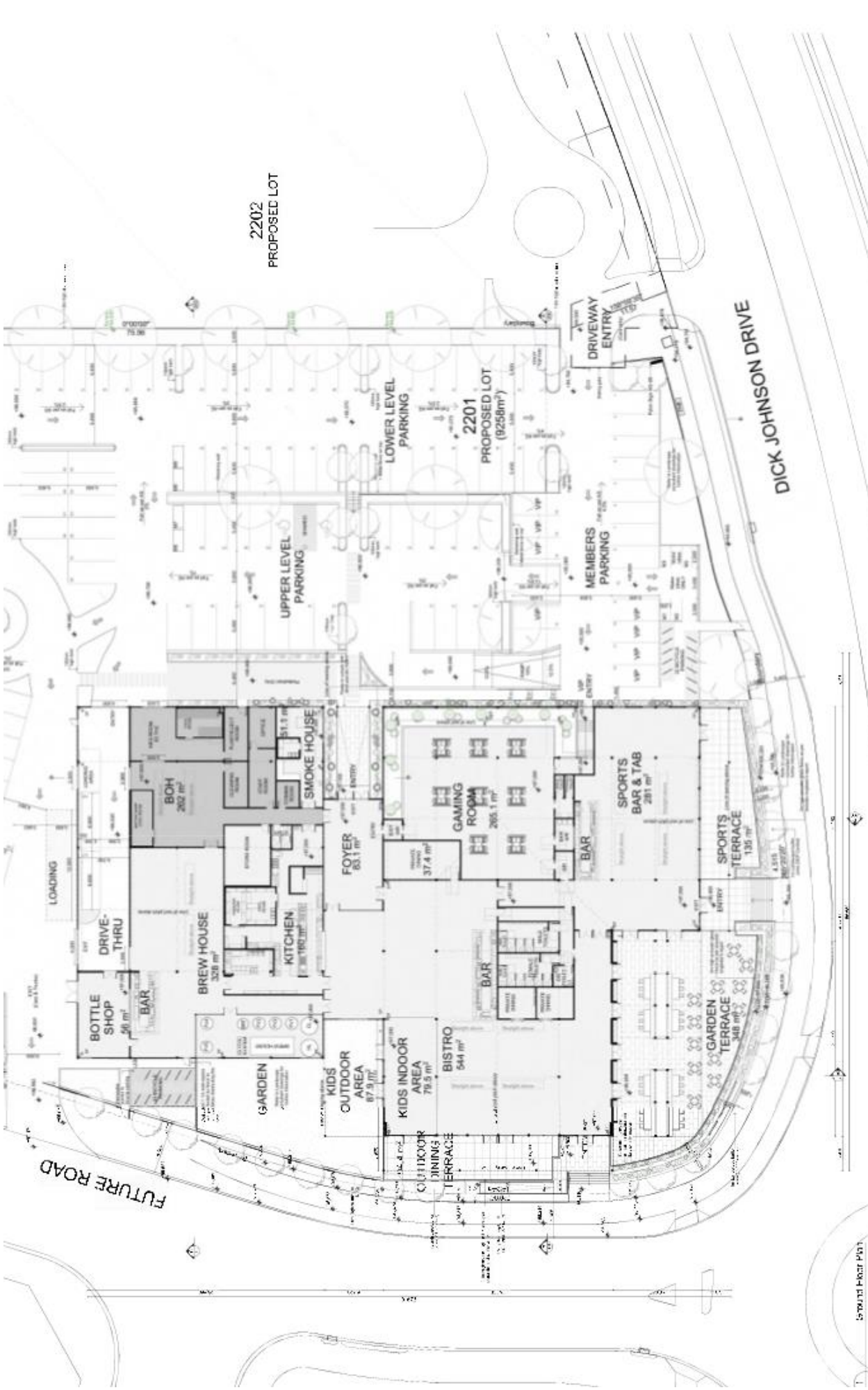
POC: Neil O'Sullivan  
 S/L: Gase  
 CL: O'Sullivan  
 ST: Stone

R. O'Sullivan, Inc.  
 10000 - Act. Dr.  
 10000 - Act. Dr.

514 382 722 E. - 4th St. - 10000 - Act. Dr.  
 WWW.ARCHIBIOSIS.COM  
 P: 410 332 2000  
 E: 201222@ARCHIBIOSIS.COM

ARCHIBIOSIS ARCHITECTS  
 Project: 2201 - 10000 - Act. Dr.  
 Period: 10/11/2022  
 Date: 10/11/2022

Drawing content:  
 Proposed Site Plan  
 SK11



Drawing content: Ground Floor Plan  
 Scale: 1/2  
 Project: 2201 & 2202 - W-16  
 Project address: 2201 Johnson Dr W

Site: 2201 & 2202 - W-16, 2201 & 2202 - W-16  
 Website: www.archebiosis.com  
 Phone: 011 330 2201  
 Email: 2201@archebiosis.com  
 Address: 2201 Johnson Dr W, 2201 & 2202 - W-16

Archibiosis Architects  
 2201 & 2202 - W-16

No. Quantity  
 1 04  
 2 04  
 3 04  
 4 04

Date: 17/03/2022  
 011-330-2201  
 2201-022

Brew House Legend:  
 H: Hilltop Park  
 L: Local Park  
 P: Pallet Park  
 B: Bottle Shop

MC: Misc. Cladding  
 GL: Glass  
 CL: Cladding  
 ST: Stone

R: Railing  
 T: Terrace  
 L: Lighting

Ground Floor Plan

Attachment 8

CLPP01

CLPP01

Attachment 8



Project: 2201 Proposed Lot (9258m²)  
 Project address: 2201 Future Rd  
 Drawing sheet: Ground Floor Plan 2/2  
 Drawing code: SK11

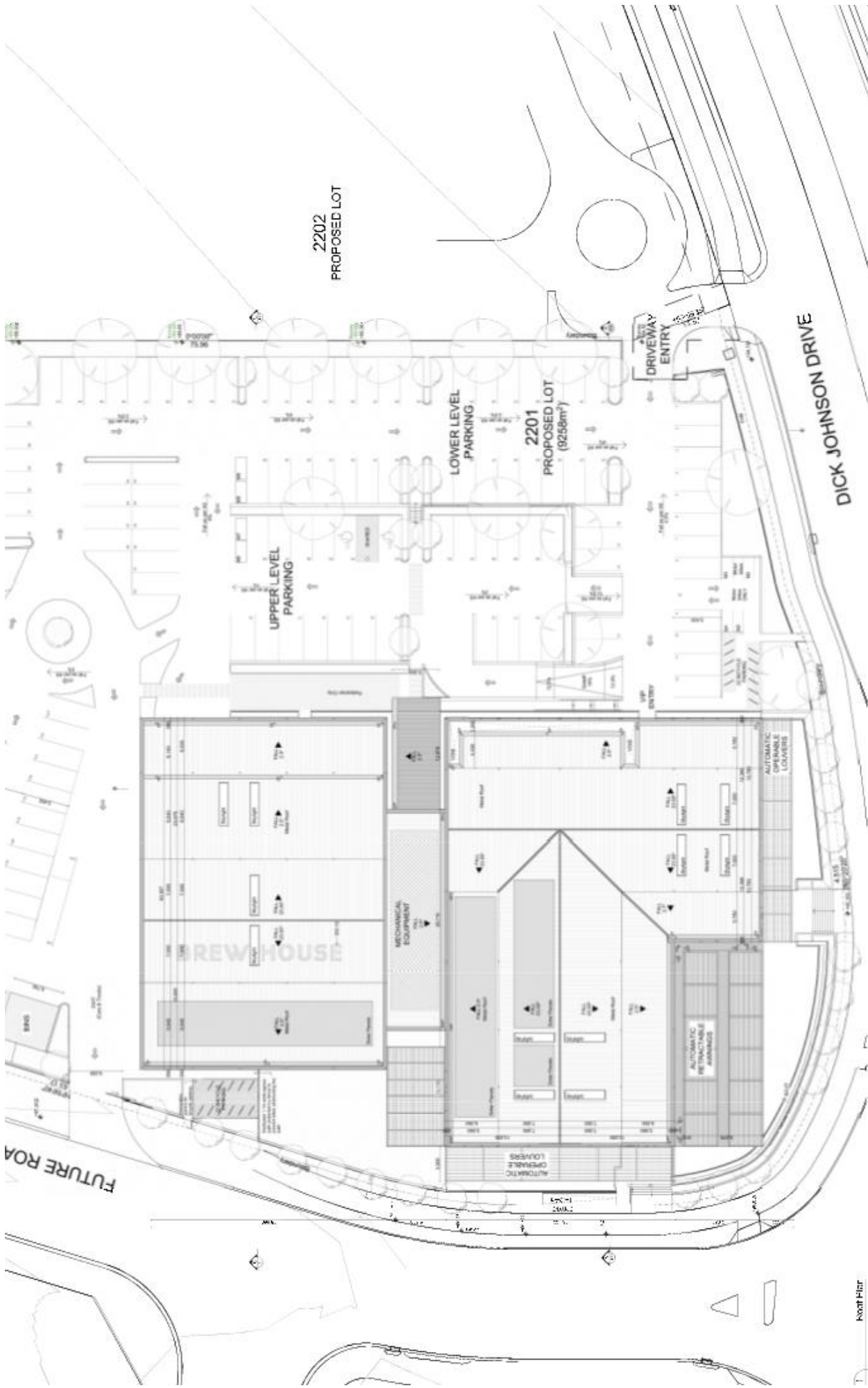
**ARCHEBIOSIS**  
 ARCHITECTS

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**Brew House Legend:**  
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 PL: Part-time park  
 PL: Full-time bar  
 PL: Part-time bar  
 PL: Full-time shop  
 PL: Part-time shop

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3	CA	411.0/3
4	CA	102.0/4

15/09/22 Hour Post



Roof Plan

Project: 01/20/2022 - 2022  
 Project address: 101 Johnson Drive

Drawing content: Roof Plan

Project: 01/20/2022 - 2022  
 Project address: 101 Johnson Drive

Architect: ARCHEBIOSIS ARCHITECTS

Scale: 1/8" = 1'-0"

North Arrow

Existing: SK11

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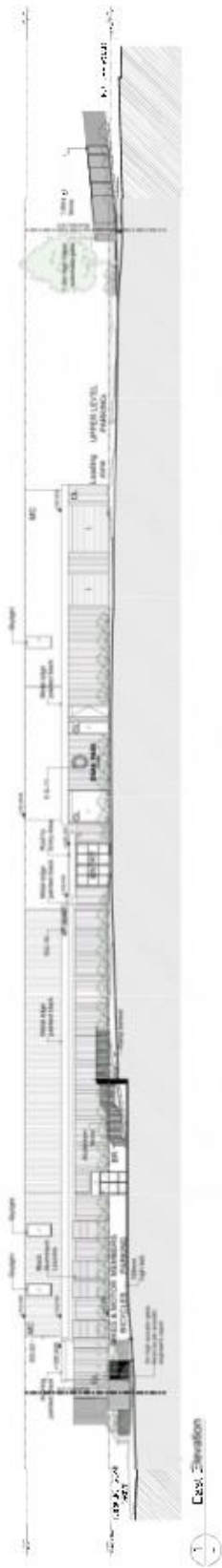
Architect: ARCHEBIOSIS ARCHITECTS  
 101 Johnson Drive, Suite 101, Camden, NJ 08102  
 Phone: 856-973-1111  
 Email: info@archebiosis.com

Architect: ARCHEBIOSIS ARCHITECTS  
 101 Johnson Drive, Suite 101, Camden, NJ 08102  
 Phone: 856-973-1111  
 Email: info@archebiosis.com

Architect: ARCHEBIOSIS ARCHITECTS  
 101 Johnson Drive, Suite 101, Camden, NJ 08102  
 Phone: 856-973-1111  
 Email: info@archebiosis.com

Attachment 8  
 CLPP01

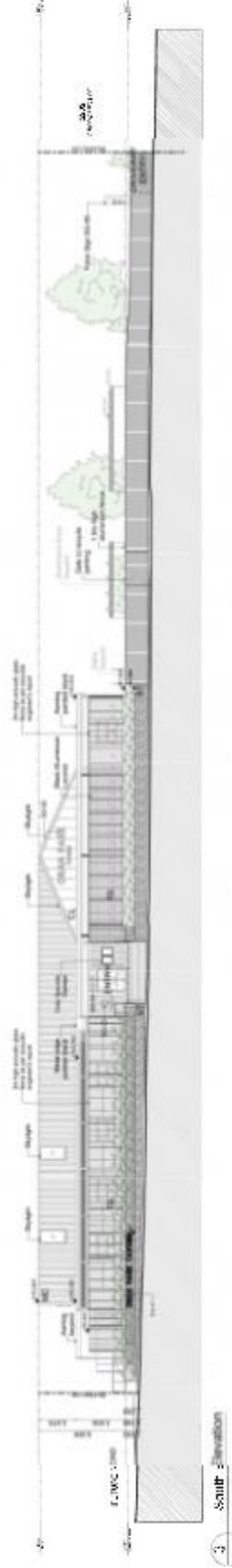
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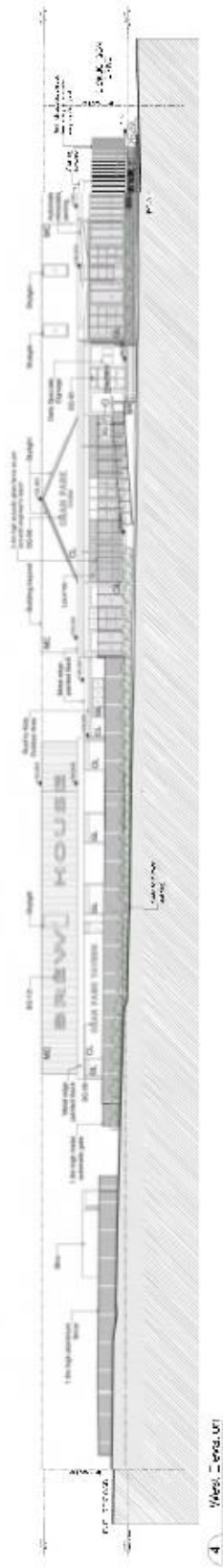
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2 North Elevation



3 South Elevation



4 West Elevation

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 --- Trusses - As. Dry  
 --- Unfinished  
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 SL: Glass  
 CL: Cladding  
 ST: Stone

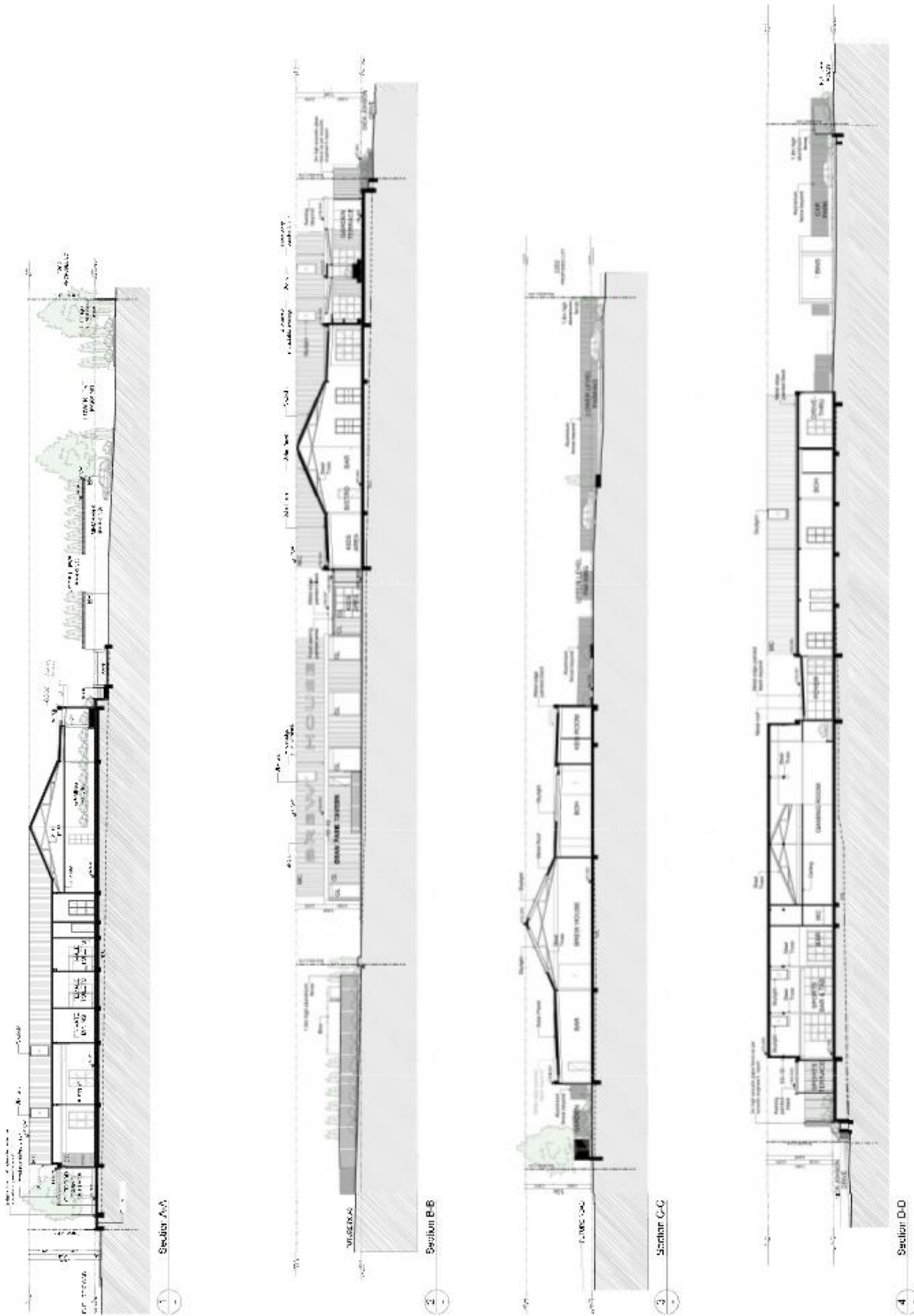
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4	CA	02/11/22

514 SE 25th Ave, Portland, OR 97115  
 WWW.ARCHIBIOSIS.COM  
 P: 503.224.2200  
 E: 2022@ARCHIBIOSIS.COM  
 ARCHIBIOSIS ARCHITECTS

Drawing content:  
 Elevations

Project:  
 CLPP01 - Main  
 Period: 01/12/22  
 Date: 1/12/22

Drawing:  
 SK2



Drawing content: Sections

Project: 012178 - "Wain"

Project address: 101 Juncus Ln

Architect: ARCHEBIOSIS ARCHITECTS

Site: 101 Juncus Ln, Raleigh, NC 27601

Phone: (919) 286-1000

Email: info@archebiosis.com

Website: www.archebiosis.com

Project No: 012178

Revision: 01/22/22

Date: 01/22/22

No.	Description	Date
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3	CA	01/22/22
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North Arrow: [Symbol]

Legend:

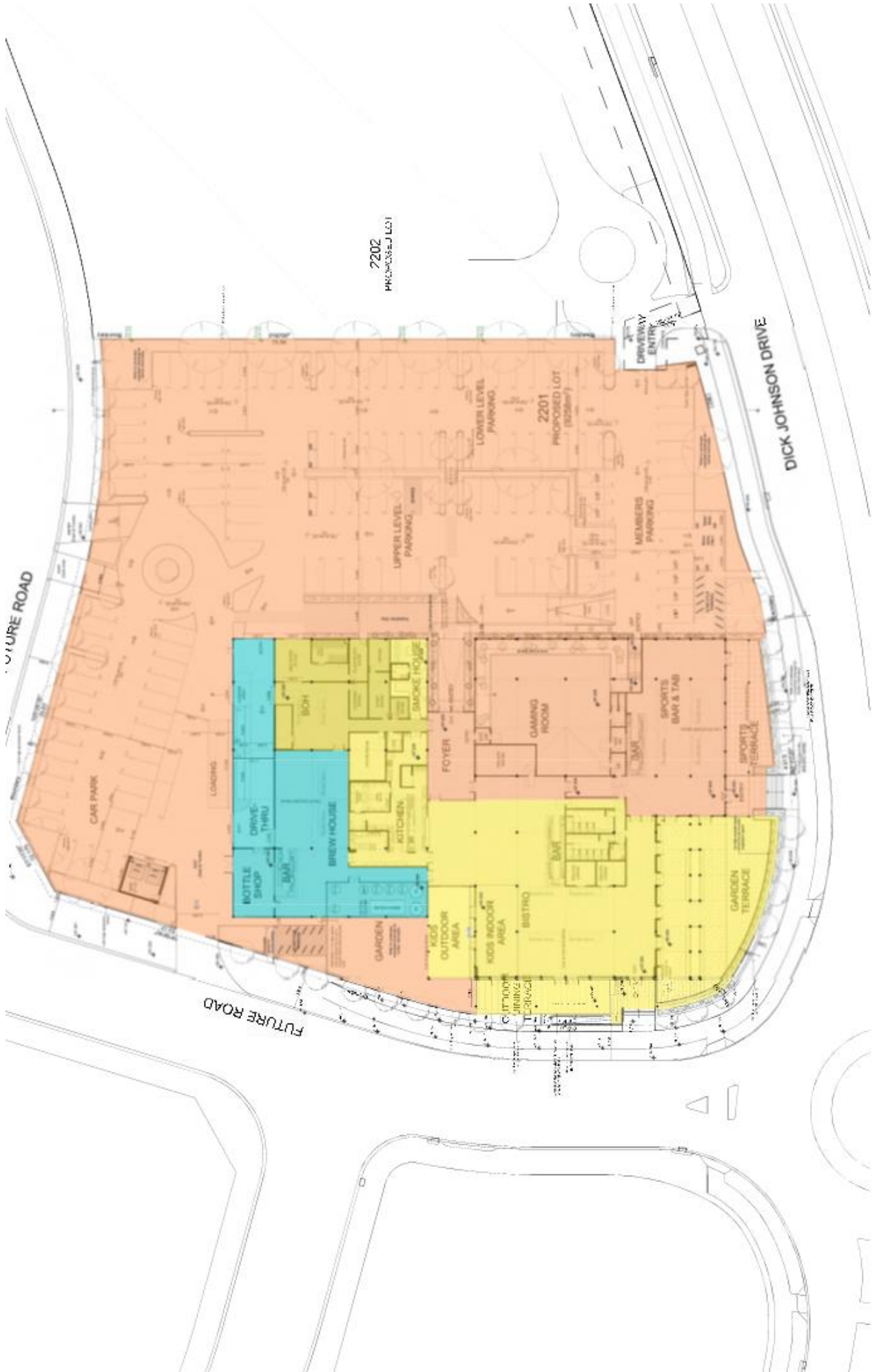
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- Proposed Structure
- Proposed Addition

Sheet Title: Sections

Sheet No: SK31

Attachment 8  
CLPP01

Attachment 8 CLPP01



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2	REV	04/14/22
3	REV	08/11/22

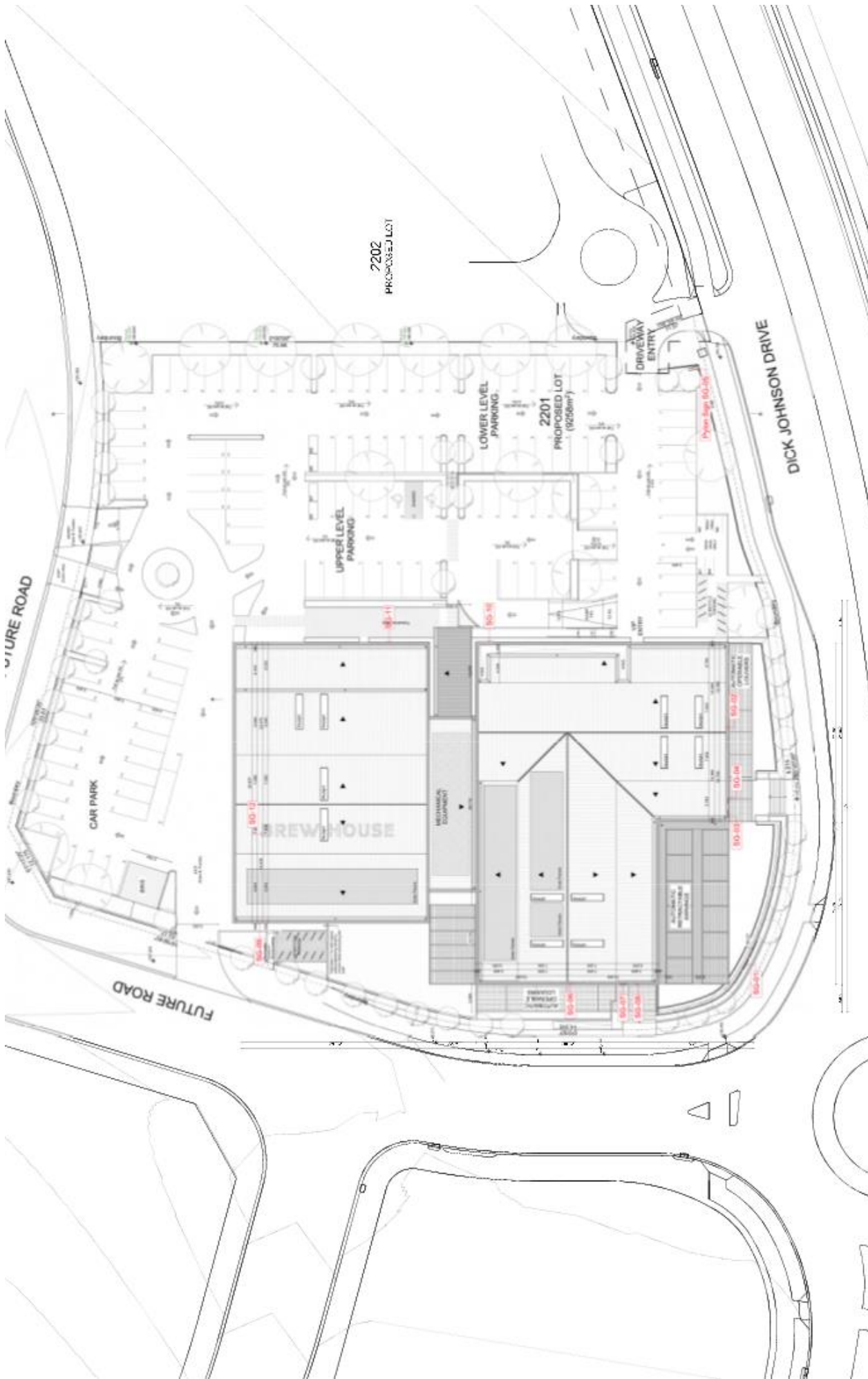
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 MW: Main Wall  
 GL: Glass  
 CL: Cladding  
 ST: Stone

**Scale Legend:**  
 0'-0" to 30'-0"

**Architectural Firm:** ARCHEBIOSIS ARCHITECTS  
 314 382 2212 | 1000 Rte. 108, Suite 1000, NJ 07030  
 WWW.ARCHBIOSIS.COM | P.O. BOX 2004  
 E: INFO@ARCHBIOSIS.COM | A: 382 2212

**Project Information:**  
 Project: 2201 & 7202 - Main  
 Project address: 7202 Johnson Dr  
 Drawing set: Ground Floor Plan - Construction Fit Out Stages Diagram  
 Drawing sheet: SK4





Enabling I  
SK51



Drawing content:  
Signage Location  
Plan

Project:  
01/26/2022 - 10/10/2022  
Project address:  
1011 Dick Johnson Dr

**ARCHEBIOSIS**  
ARCHITECTS

Site: 1011 Dick Johnson Dr, Suite 100, Camden, NJ 08102  
www.archebiosis.com  
P: 856.233.2200  
E: 201.222.5625 - 201.222.5626  
archebiosis.com | 1011 Dick Johnson Dr, Suite 100, Camden, NJ 08102

No.	Quantity	Unit
1	1	Sheet
2	1	Sheet
3	1	Sheet
4	1	Sheet

Architect: Archebiosis Architects  
Scale: As Shown  
Date: 10/10/2022  
Sheet: 01 of 04

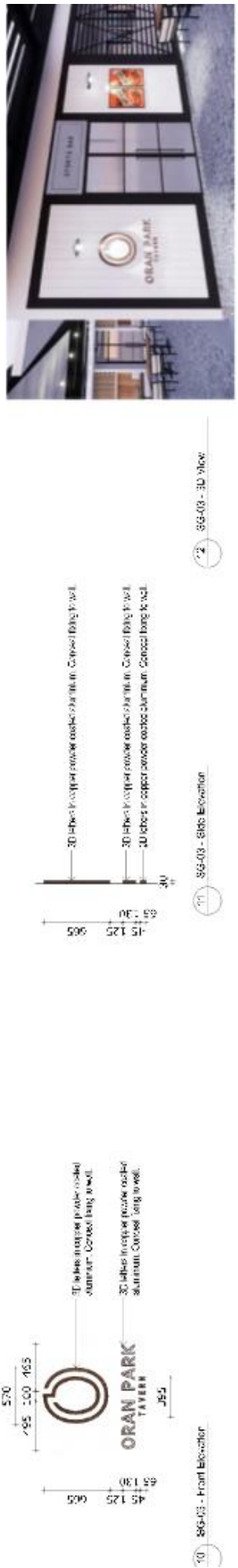
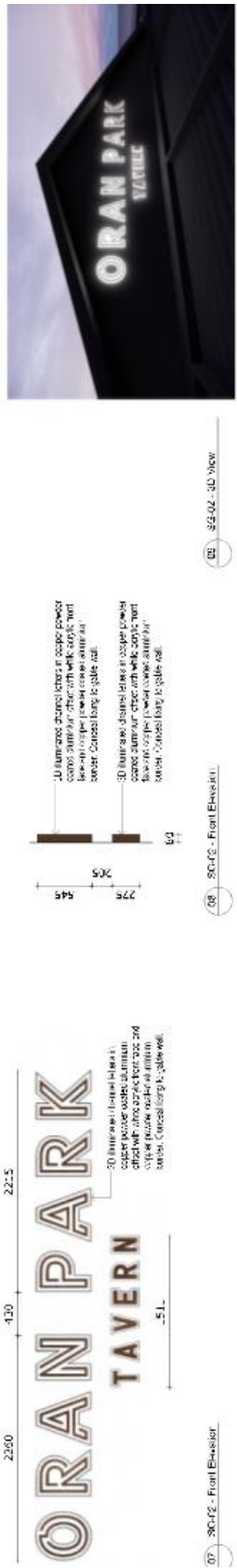
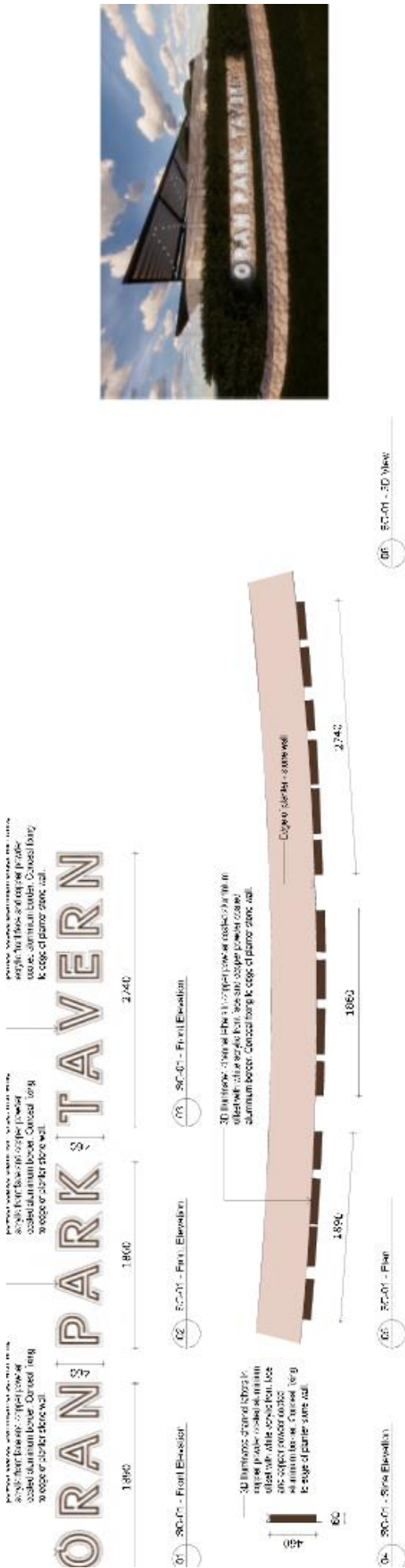
Project: 1011 Dick Johnson Dr  
1011 Dick Johnson Dr  
Camden, NJ 08102

CLPP01

Attachment 8

CLPP01

Attachment 8



Architectural drawing details including project information, drawing content, and contact details for Archebiosis Architects.

**Project:**  
 Client: Frank Tawell  
 Project address: Dick Johnson Drive  
 Location:

**Drawing content:**  
 SG-01 + SG-02 + SG-03  
 SK50

**ARCHEBIOSIS ARCHITECTS**

Step 20 - 20/09/2022, Section 53/2/2018  
 P.O. #28/2011  
 E: archebiosis@archebiosis.com

And attached to the following documents:  
 1. 20/09/2022, Section 53/2/2018

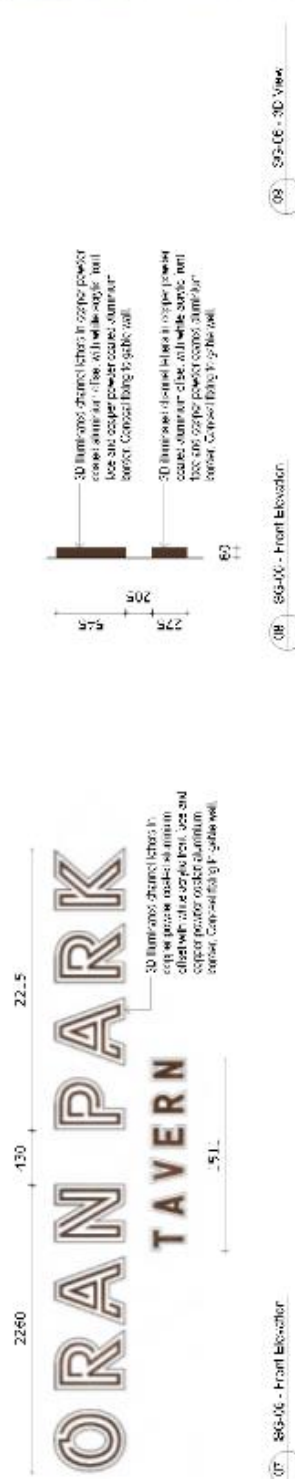
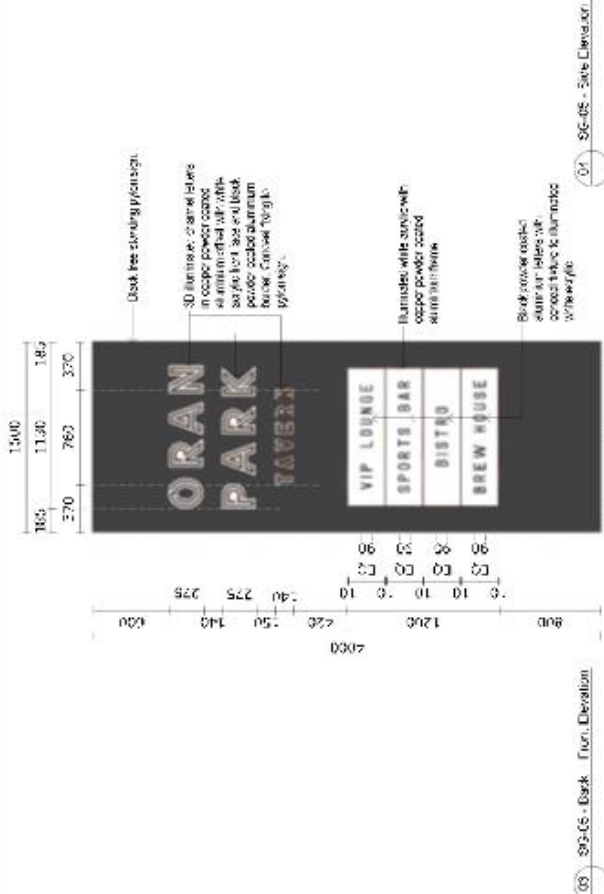
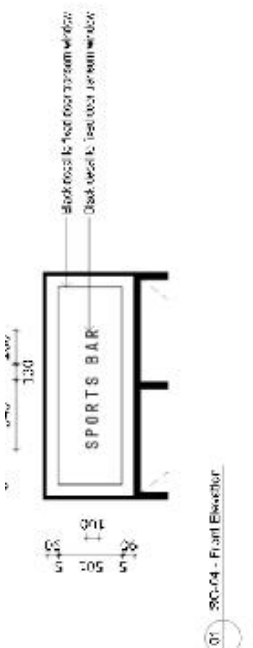
By	Reviewed	Date
	SA	20/09/2022



02 57-04 - 3D View



08 59-06 - 3D View



Architectural Plans

Project: Oran Park Tavern  
 Project address: Dick Johnson Drive  
 Client: Archebiosis Architects

Drawing content: SG-01 + SG-05 + SG-08  
 Drawing: SK50

Scale: 1/8" = 1'-0"

Architect: ARCHEBIOSIS ARCHITECTS

Site: 57-04 - 3D View

01 59-05 - Dark Front Elevation

02 57-04 - 3D View

03 58-05 - 3D View

04 59-05 - Site Elevation

05 59-06 - Front Elevation

06 59-06 - Front Elevation

07 59-06 - Front Elevation

08 59-06 - 3D View

Attachment 8

CLPP01

CLPP01

Attachment 8



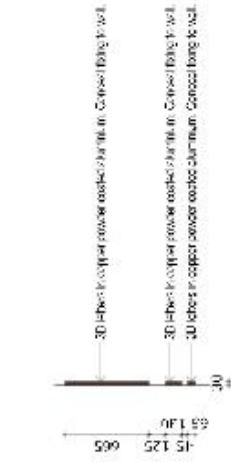
03 89C-07 - 3D View



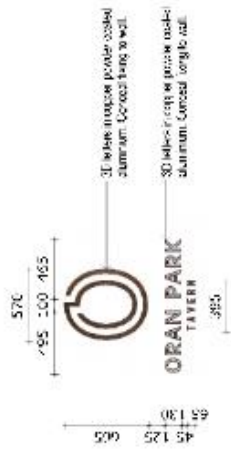
03 89C-08 - 3D View



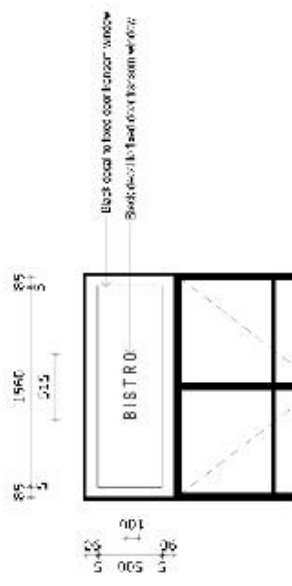
03 89C-09 - 3D View



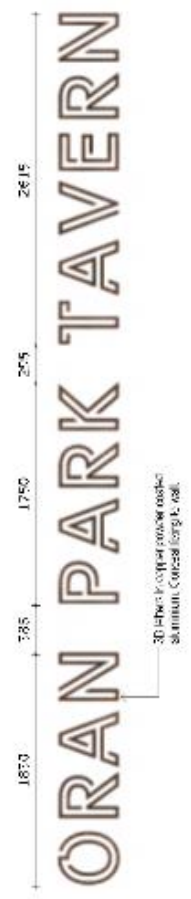
02 89C-07 - Side Elevation



01 89C-07 - Front Elevation



04 89C-08 - Front Elevation



06 89C-09 - Front Elevation

Architectural Plans

Project: Oran Park Tavern  
 Project address: 1111 Jackson Drive  
 Status: Pending

Drawing content: SG-07 + SG-08 + SG-09  
 Drawing: SK50

ARCHIBIOSIS ARCHITECTS

Site: 1111 Jackson Drive, Jackson, MS 39208  
 Phone: (601) 556-1111  
 Email: archibiosis@archibiosis.com

Archibiosis is a registered professional architectural firm in the state of Mississippi.

No.	Description	Date
1	01	08/20/22



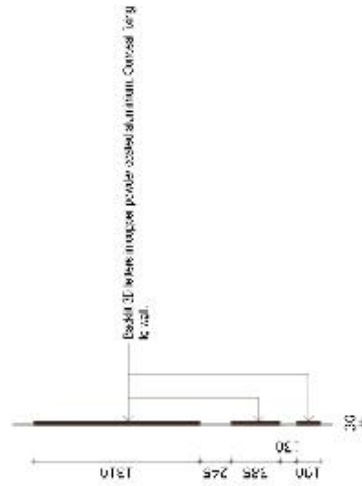
03 865-10 - 3D View



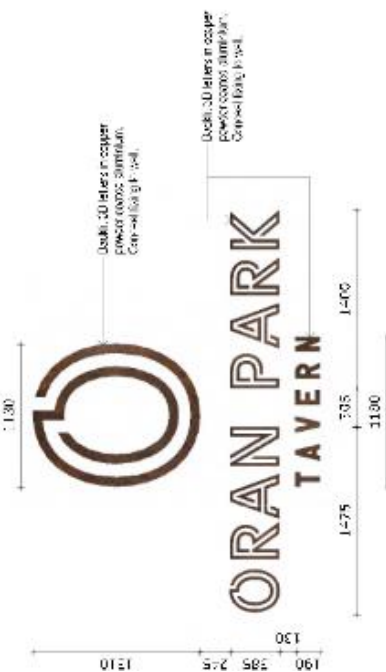
05 865-11 - 3D View



01 893-10 - Front Elevation



05 893-11 - Side Elevation



01 893-11 - Front Elevation

Architectural Plans

Project: Oran Park Tavern  
 Project address: Dick Johnson Drive  
 Site: Oran Park

Drawing content: SG-10 + SG-11

Scale: 1/8" = 1'-0"

Architect: ARCHEBIOSIS ARCHITECTS

Site: 2000 Highway 20, Camden NJ 07513  
 Phone: 908.238.2111  
 Email: info@archebiosis.com

Architectural rendering by: [Name] for [Company]

No.	Description	Date
01	01	08/20/22

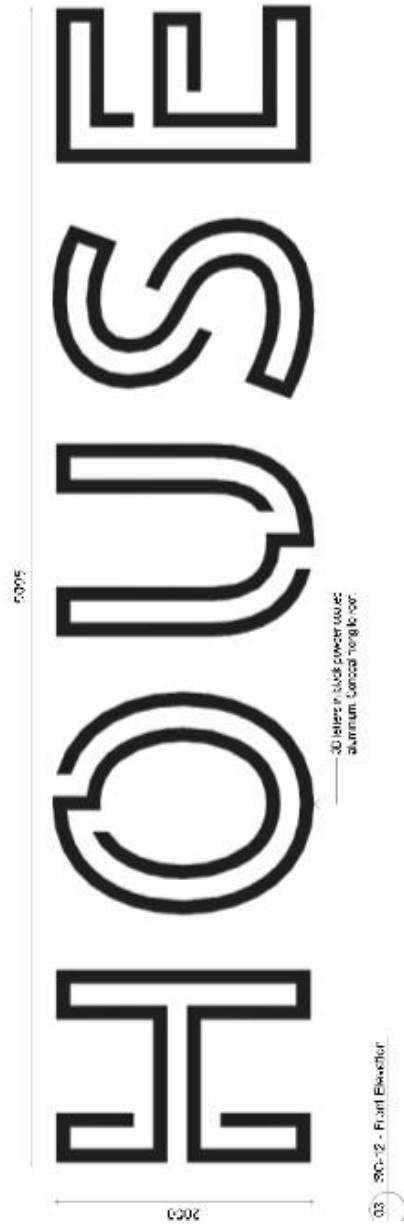
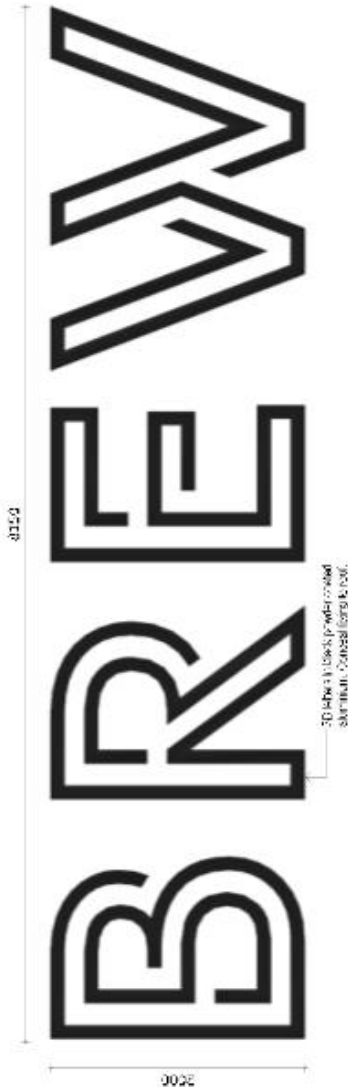
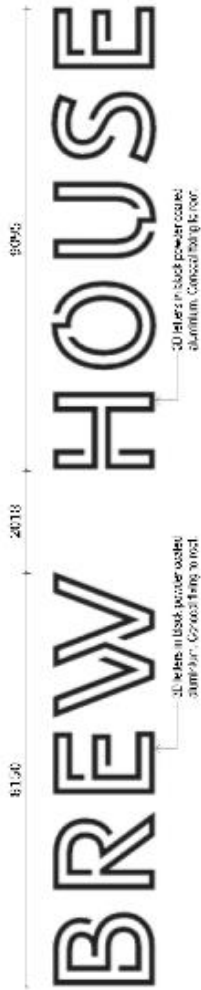
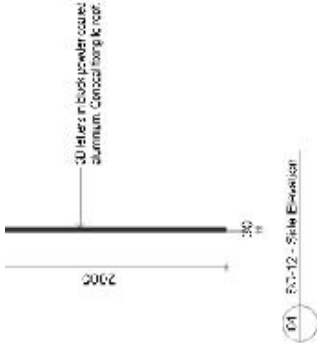
Drawing: SK50

CLPP01

Attachment 8

CLPP01

Attachment 8



05 50'-12" - 3D View

Architectural Plans

Project: Oost Frik Tavern  
Project address: Dirk Johnson Drive  
Studio

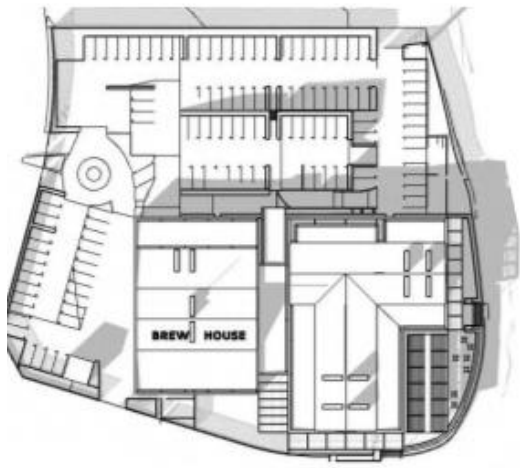
Drawing content: SG-12  
Scale: 1:50  
Date:

Drawing Number: SK50

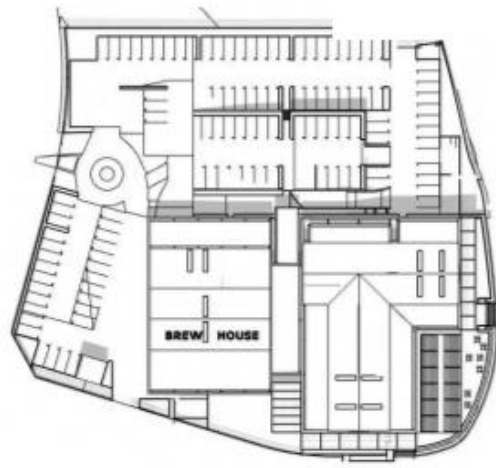
Architect: ARCHEBIOSIS ARCHITECTS

Step 06 - 06/06/2022 - Update 15.5V 2019  
Author: P.O. 175802111  
E: archebiosis@archebiosis.com  
Architectural Board of Professional Engineers, Inc. License No. 10000  
Architectural Board of Professional Engineers, Inc. License No. 10000

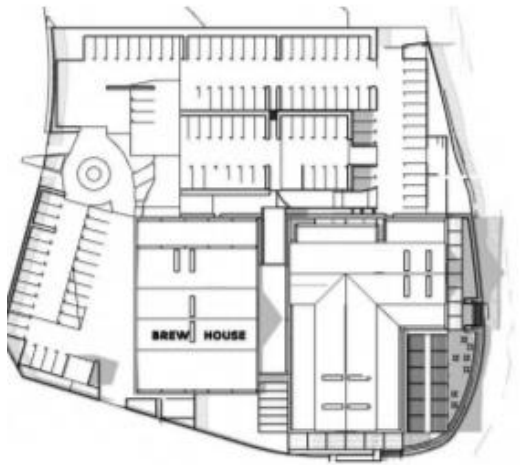
No.	Description	Date
1	01	05/20/22



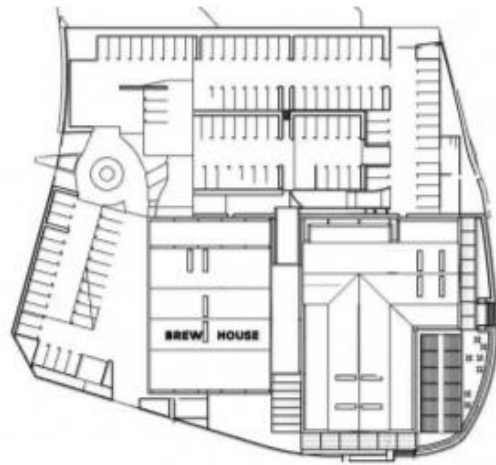
Shaded Exterior - 1st Floor



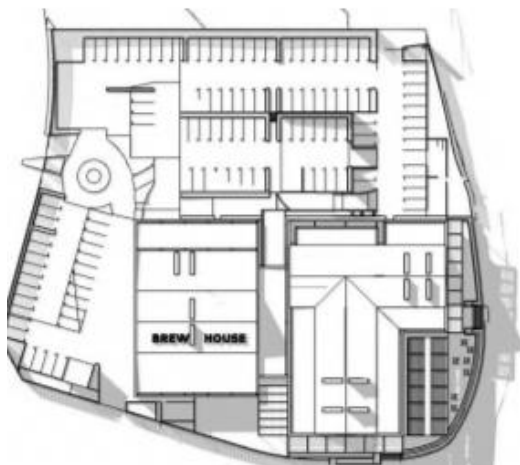
Shaded Exterior - 2nd Floor



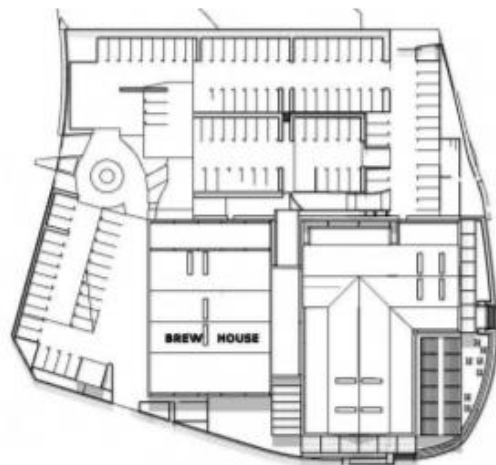
Shaded Exterior - 3rd Floor



Shaded Exterior - 4th Floor



Shaded Exterior - 5th Floor



Shaded Exterior - 6th Floor

Drawing | SK6



Drawing content  
Shadow Diagrams

Project  
Osh Park Tavern  
Project address  
Dick Johnson Drive

**ARCHEBIOSIS**  
ARCHITECTS

Shop 38, 780 Banksia St, Riverton NSW 2575  
www.archebiosis.com  
P 02 8392 2334  
E archebiosis@archebiosis.com

Archebiosis Architects is the owner of the copyright of these

No	Description	Date
1	SA	18/05/2021
2	SA	07/01/2022
3	SA	27/07/2022
4	SA	17/08/2022

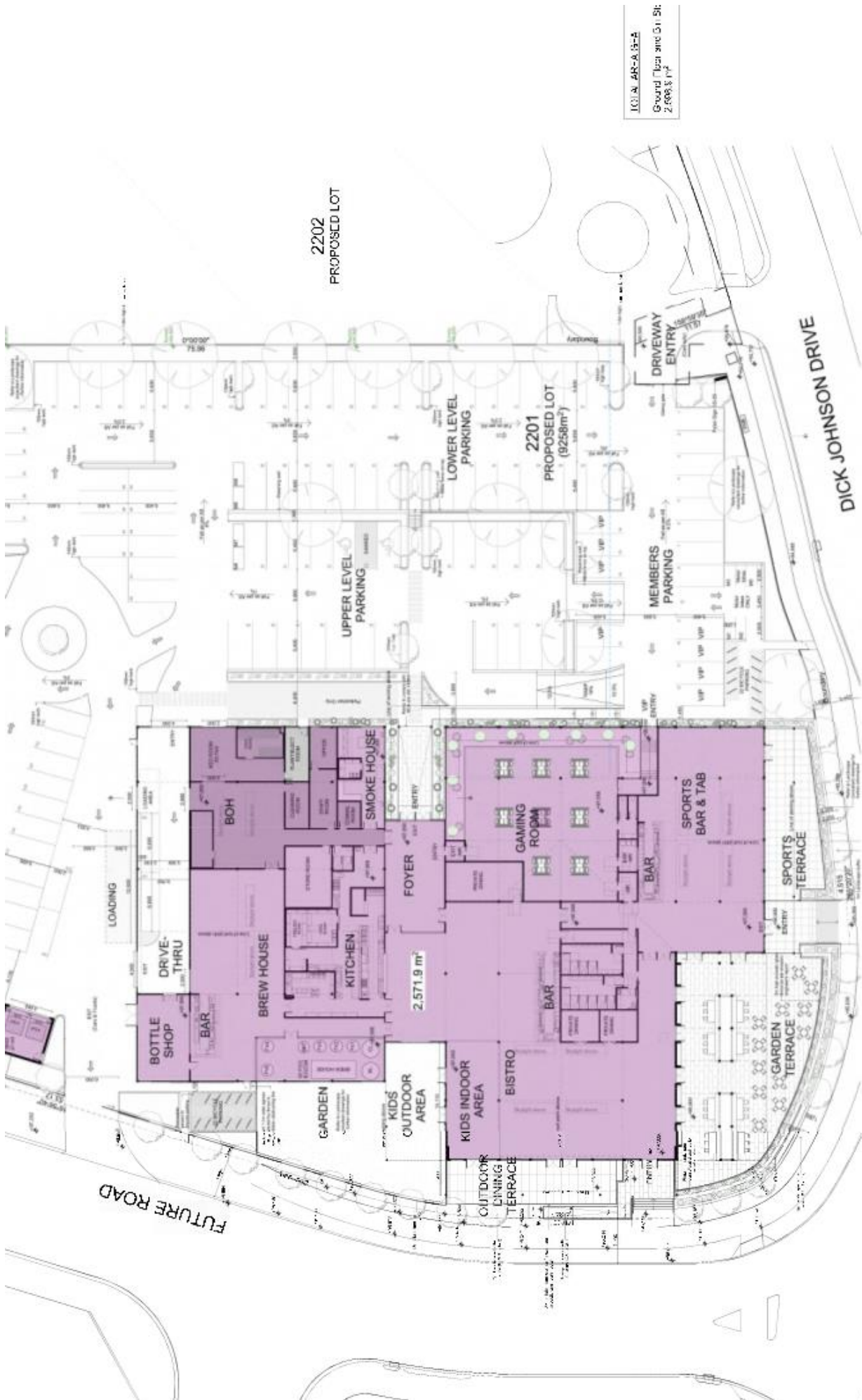
MC: Metal Cladding  
GL: Glass  
CL: Cladding  
ST: Stone

Boundary Line  
Trusses - Axis Line  
Line of building above

CLPP01

Attachment 8

Attachment 8  
CLPP01



KCLN AR-A 15-2  
Ground Floor and Site  
2.000 & 1/4"

Project: 2201/2  
Period: 10/2022  
Period: 10/2022

Drawing sheet:  
Ground Floor Plan -  
Total Area GFA

Enabling:  
SK71

Project: 2201/2  
Period: 10/2022  
Period: 10/2022

**ARCHEBIOSIS**  
ARCHITECTS

1511 2022 2022 2022 2022 2022  
www.archebiosis.com  
P: 416-291-2022  
E: info@archebiosis.com  
416-291-2022 2022 2022 2022 2022

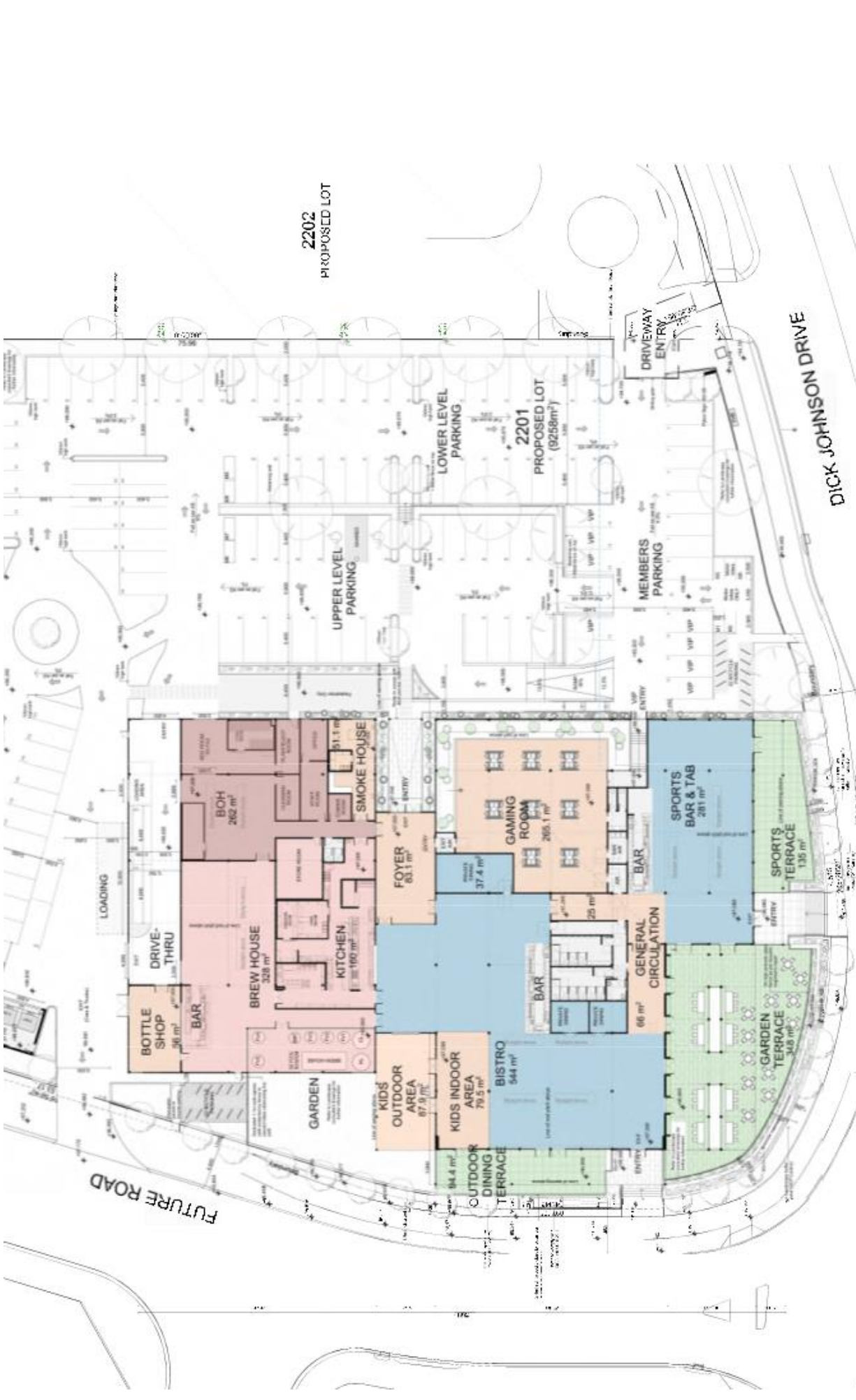
No.	Description	Unit
1	CA	1
2	CA	1
3	CA	1
4	CA	1

MS: Help Desk  
SL: Gas  
CL: Cladding  
ST: Stone

Ground Floor Plan - 10th Ave. 10/22

Legend:  
 - Room  
 - Terrace  
 - Cladding  
 - Stone





**Ground Floor Plan - Public Floor Area**

R-Walkway  
 Terrace - Acc. Upr.  
 Fire Fighting Area

No. Use/Class - Entry  
 2. No. Use - Outdoor  
 3. Entry - Bar  
 3. No. Use/Class

No.	Description	Area	Use
1	GA	135.00	GA
2	CA	449.00	CA
3	CA	449.00	CA

1. No. Use/Class - Entry  
 2. No. Use - Outdoor  
 3. Entry - Bar  
 3. No. Use/Class

Site No: 2201, 2202, 2203, 2204, 2205, 2206, 2207, 2208, 2209, 2210, 2211, 2212, 2213, 2214, 2215, 2216, 2217, 2218, 2219, 2220  
 www.archbiosis.com  
 P: 01351 2541  
 E: 20122553@archbiosis.com  
 Archbiosis Pty. Ltd. 5/180 South St. Suite 200, Fremantle

**ARCHEBIOSIS**  
**ARCHITECTS**

Drawing content:  
 Ground Floor Plan -  
 Public Floor Area

Project:  
 11/21/22 - 2022  
 Project address:  
 221 Dick Johnson Dr

Enabling I:  
 SK7

Attachment 8 CLPP01

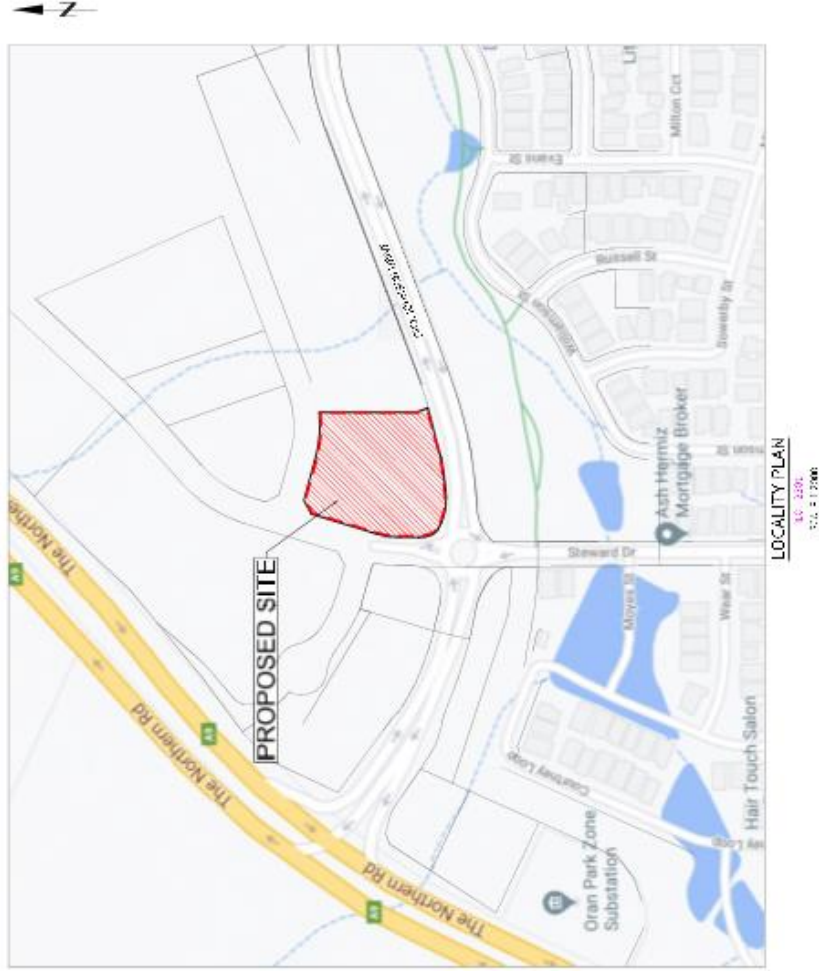


ranche 22/1 - DJD Slip Lane PPS  
DANI DADK

# ORAN PARK TAVERN ISSUED FOR DEVELOPMENT APPLICATION



SCHEDULE OF DRAWINGS	
DRAWING	TITLE
<b>GENERAL</b>	
001	COVER SHEET - SITE LOCALITY AND SCHEDULE OF DRAWINGS
002	GENERAL NOTES & LEGEND
101	WORK BILLS
200	EXISTING SITE LAYOUT AND SURVEY STATUSES R 40
<b>UTILITIES SERVICES</b>	
101	UTILITIES SERVICES PLAN
210	RAILWAY PROPOSED SITE SECTION 22, 23 AND R 2
310	RAILWAY PROPOSED SITE SECTION 23, 24 AND R 2
<b>ROADWORK</b>	
101	ROAD LAYOUT PLAN
200	LAND PARKING AND SIGN POSITION - VEHICLE & WALKWAY DESIGN
<b>STORMWATER CATCHMENT PLAN</b>	
101	STORMWATER CATCHMENT PLAN
200	DESIGN OF SITE PLAN
300	STORMWATER LAYOUT PLAN
400	STORMWATER DRAINAGE LAYOUT SECTIONS
500	STORMWATER FIT TO RECEIVE
600	STORMWATER CALCULATIONS (SITE) - APPROXIMATE
700	STORMWATER CALCULATIONS (OFF SITE) - APPROXIMATE
<b>BRUSH AND DEBRIS MANAGEMENT</b>	
101	BRUSH AND DEBRIS MANAGEMENT PLAN
200	BRUSH AND DEBRIS MANAGEMENT - CONES



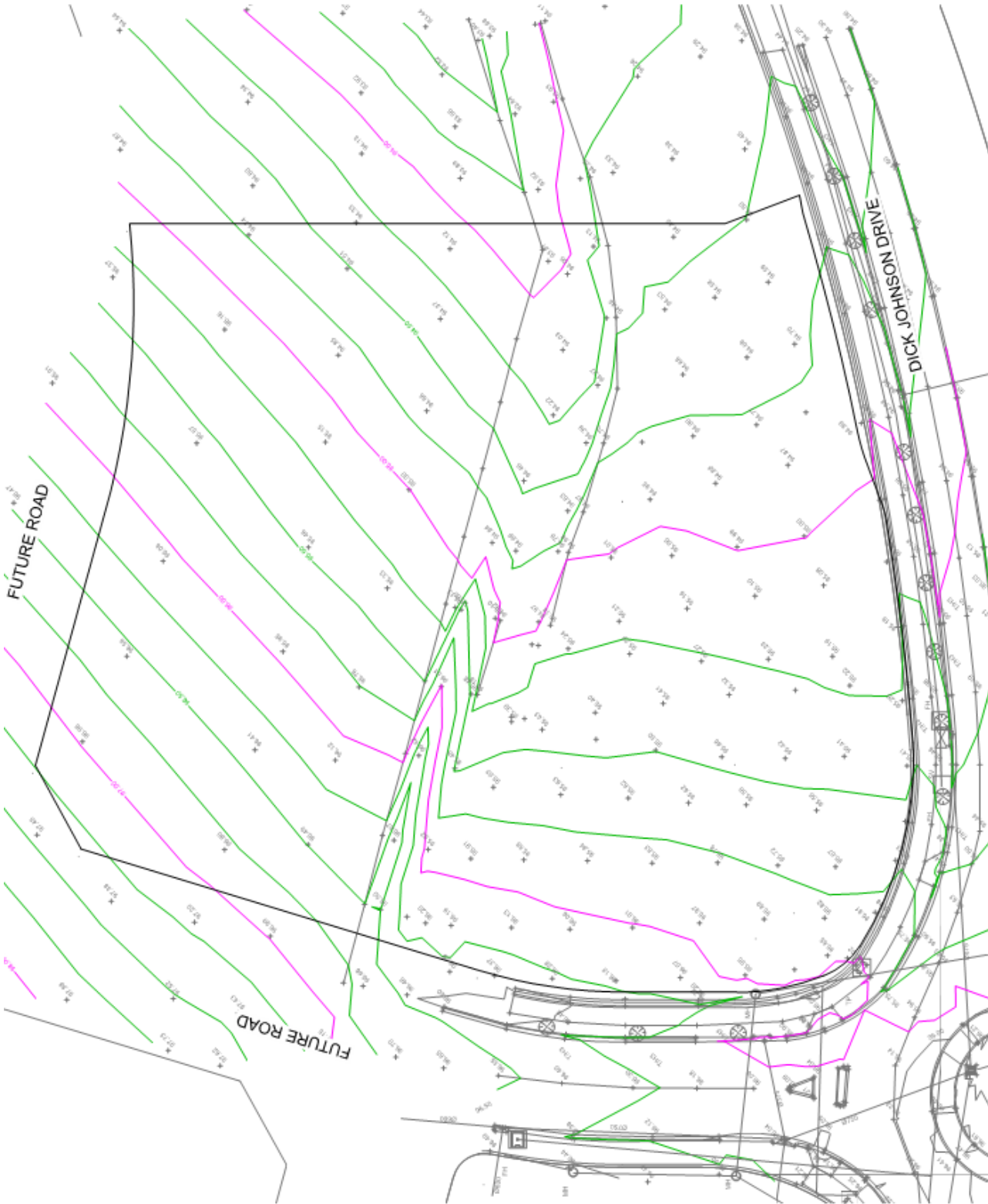
<b>CONCEPT DESIGN</b>	<p><b>ORAN PARK COUNCIL</b> STATE GOVERNMENT OF NEW SOUTH WALES CITY OF ORAN PARK ORAN PARK, NSW 2570</p>	<p>SCALE 1:1000 0 10 20 30 METRES</p>	<p><b>MOMENTO HOSPITALITY</b></p> <p><b>ORAN PARK TAVERN</b> DICK JOHNSON DRIVE, ORAN PARK</p>
		<p>DATE: 15/09/22 DRAWN BY: M. HUGHES CHECKED BY: M. HUGHES</p>	<p>PROJECT NO: 319187</p>





CLPP01

Attachment 8



NOTES:

1. ALL CO-ORDINATES ARE TO BE VERIFIED ON SITE BEFORE CONSTRUCTION.
2. ALL EXTERNAL SITE BOUNDARIES ARE TO BE MARKED BY A REGISTERED SURVEYOR PRIOR TO COMMENCING SITE WORKS.
3. WAC-EX SURVEY HAS NOT BEEN COMPLETED AND AS SUCH NOT ALL EXISTING UTILITIES HAVE BEEN LOCATED AND LEVELLED. CONTRACTOR PROCEED WITH CAUTION.
4. CONTOURS SHOWN ARE NATURAL SURFACE CONTOURS.
5. MINOR CONTOUR INTERVAL IS 0.2m.
6. MAJOR CONTOUR INTERVAL IS 1.0m.

ORAN PARK OFFICE  
SUITE 301, LEVEL 3, ORAN PARK PODIUM  
551 ORAN PARK DRIVE  
ORAN PARK, NSW 2570

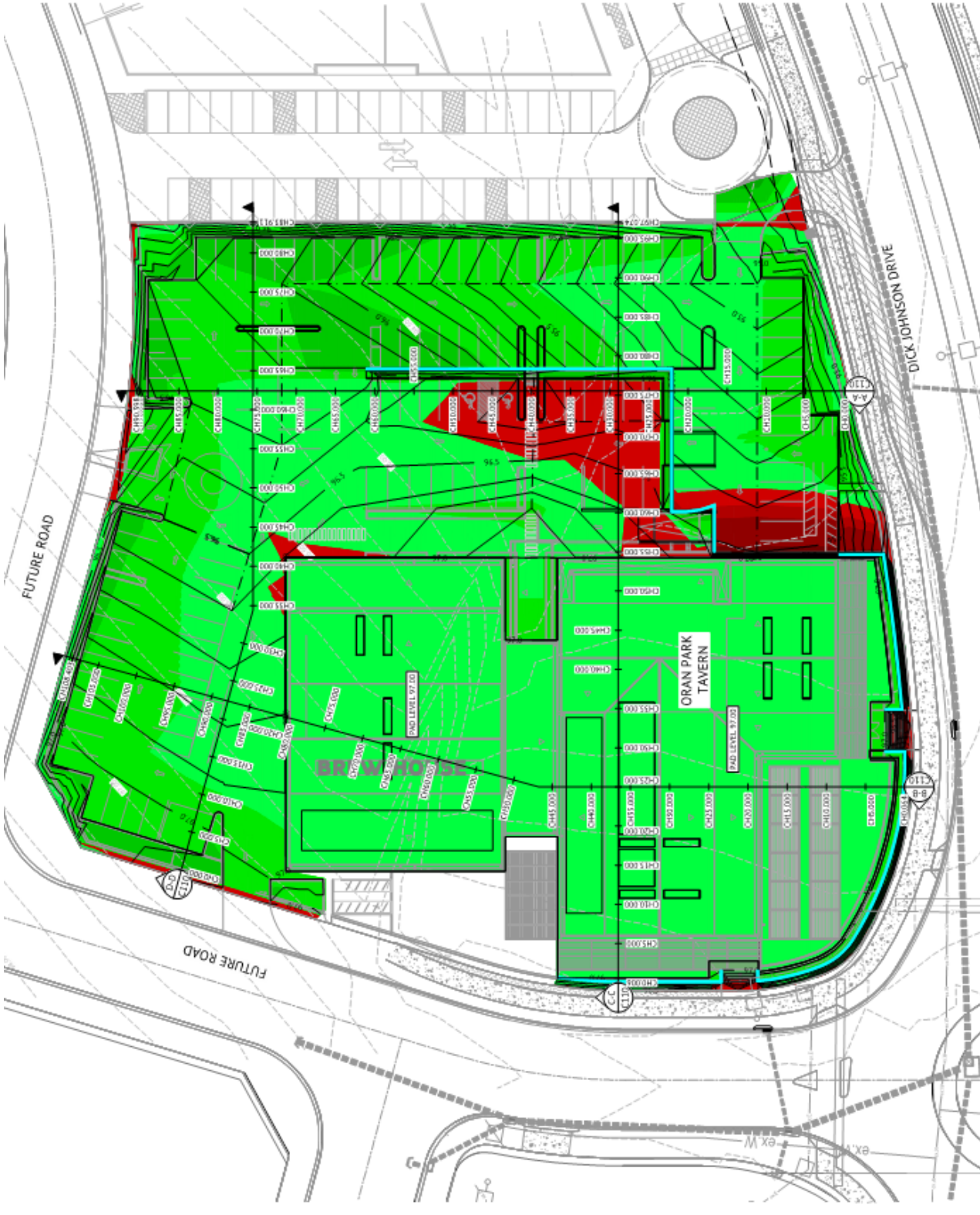
ORAN PARK TAVERN  
DICK JOHNSON DRIVE, ORAN PARK

MOMENTO HOSPITALITY

SCALE 1:150 (A3)

319187

CONCEPT DESIGN



**EARTHWORKS VOLUMES**  
 CUT = -148m<sup>3</sup>  
 FILL = 11,317m<sup>3</sup>  
 BALANCE = +11,169m<sup>3</sup>  
 (- EXPORT, + IMPORT)  
**NOTES**  
 1. EXCLUDES BULKING AND CONSTRUCTION FACTORIES.  
 2. ALLOWANCE FOR MINIMUM ROAD BOX INCLUDED IN VOLUMES.

CUT/FILL	
Colour	Depth Range (m)
Red	-5.000 to -2.000
Orange	-2.000 to -1.000
Yellow	-1.000 to -0.500
Light Green	-0.500 to 0.000
Medium Green	0.000 to 0.250
Dark Green	0.250 to 1.000
Lightest Green	1.000 to 2.000
Darkest Green	2.000 to 5.000
Darkest Green	5.000 to 10.000

**NOTES:**  
 1. CUT/FILL DEPTH IS REPRESENTATIVE LEVEL DIFFERENCE BETWEEN EXISTING TERRAIN AND PROPOSED EXISTING FINISHED GRADE.  
 2. ALL BOXES MUST BE COLOURED FOR THIS PLAN TO BE VISIBLE.  
 3. CONTOURS SHOWN ARE EXISTING AND DESIGN SURFACE.  
 4. CONTOUR INTERVAL EXISTING 0.5M.  
 NOTE: REFER DRAWING C FOR NOTES AND LEGEND.

**CONCEPT DESIGN**

ORAN PARK OFFICE  
 SUITE 301, LEVEL 3, ORAN PARK PODIUM  
 551 ORAN PARK DRIVE  
 ORAN PARK, NSW 2570

DATE: 11/11/2022  
 DRAWN BY: B. KELLY  
 PROJECT NUMBER: 2200000000  
 SHEET NUMBER: 01

SCALE: 1:150 (A4)

CLIENT: MOMENTO HOSPITALITY  
 PROJECT: ORAN PARK TAVERN  
 LOCATION: DICK JOHNSON DRIVE, ORAN PARK

319187

CLPP01

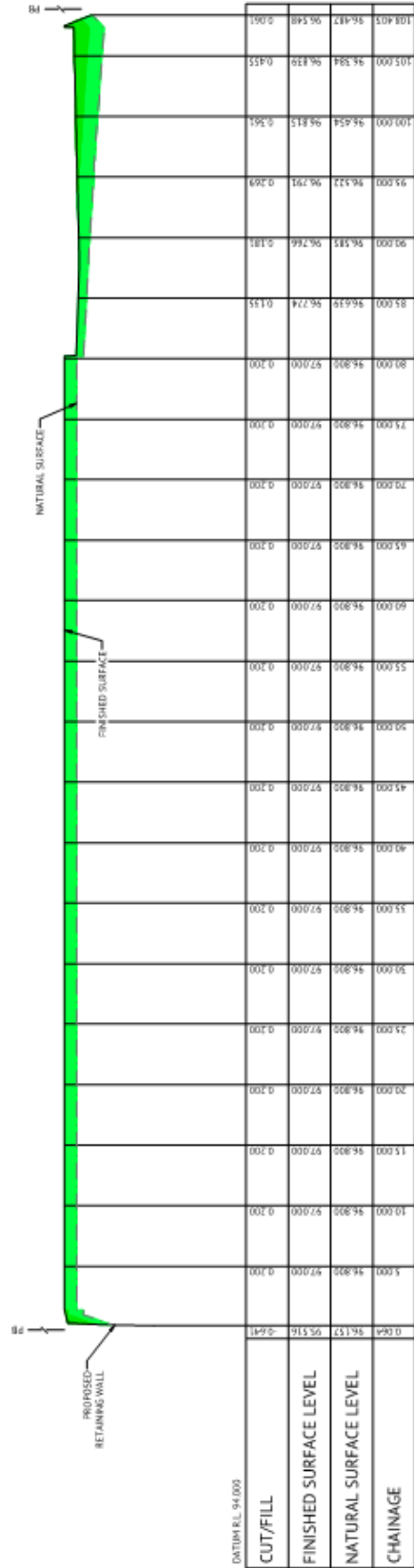
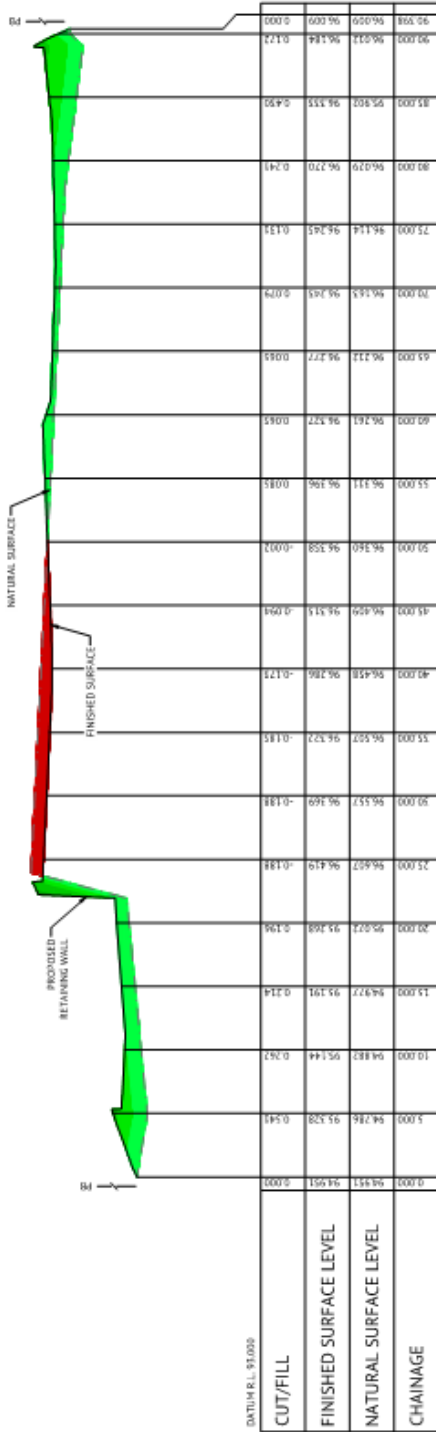
Attachment 8

Attachment 8 CLPP01

CUT/FILL	
Colour	Depth Range (m)
Dark Red	-8.000
Red	-2.000
Light Red	-1.000
Orange	-0.500
Yellow	-0.250
Light Green	0.000
Green	0.250
Dark Green	0.500
Lightest Green	1.000
Very Light Green	2.000
White	5.000
Lightest Green	10.000

NOTES:

L. ALL BOXES MUST BE COLOURED FOR THIS PLAN TO BE VALID



**CONCEPT DESIGN**

ORAN PARK OFFICE  
 SUITE 301, LEVEL 3 ORAN PARK PODIUM  
 351 ORAN PARK DRIVE  
 ORAN PARK, NSW 2270

ORAN PARK TAVERN  
 DICK JOHNSON DRIVE, ORAN PARK

MOMENTO HOSPITALITY

DATE: 15/09/22  
 PROJECT: 319187  
 SHEET NUMBER: 1

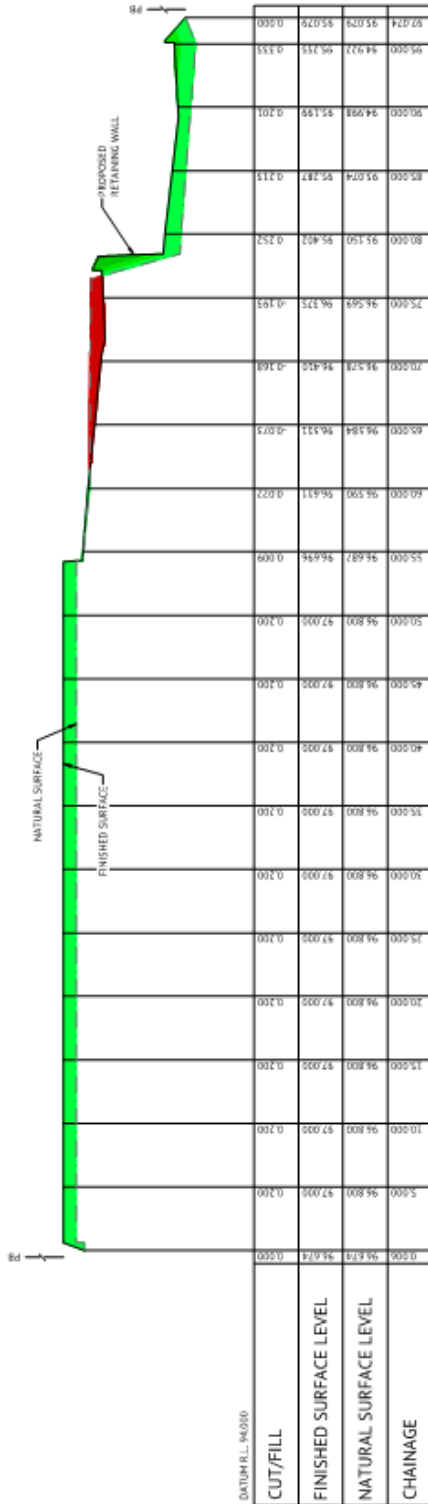
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 VERTICAL: 1:10 (A1)



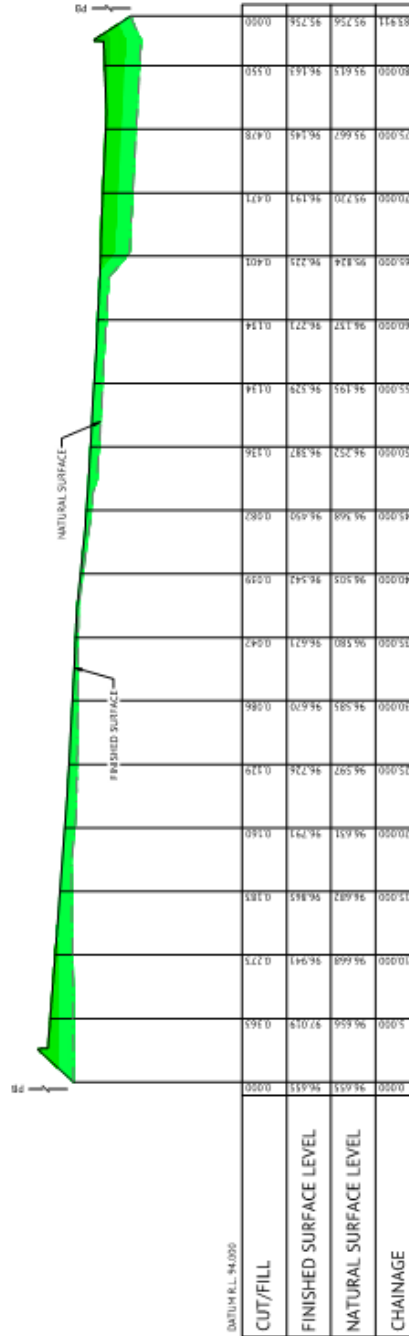
CUT/FILL	
Colour	Depth Range (m)
Dark Red	-3.000
Red	-2.000
Light Red	-1.000
Orange	-0.500
Yellow	0.000
Light Green	0.250
Green	0.500
Dark Green	1.000
Very Dark Green	2.000
Black	5.000
Black	10.000

**NOTES:**

1. ALL BOXES MUST BE COLOURED FOR THIS PLAN TO BE VALID.



**LONGITUDINAL SECTION - CC**  
 HORIZONTAL SCALE 1:800 (A1) 1:800 (A2)  
 VERTICAL SCALE 1:40 (A1) 1:80 (A2)



**LONGITUDINAL SECTION - DD**  
 HORIZONTAL SCALE 1:800 (A1) 1:800 (A2)  
 VERTICAL SCALE 1:40 (A1) 1:80 (A2)

DATUM R.L. 96.000

CUT/FILL	0.000
FINISHED SURFACE LEVEL	96.633
NATURAL SURFACE LEVEL	96.633
CHAINAGE	0.000

**CONCEPT DESIGN**

DATE: 11/09/2022

PROJECT: MOMENTO HOSPITALITY

LOCATION: ORAN PARK TAVERN, DICK JOHNSON DRIVE, ORAN PARK

SCALE: HORIZONTAL 1:300 (A1) 1:300 (A2) VERTICAL 1:40 (A1) 1:80 (A2)

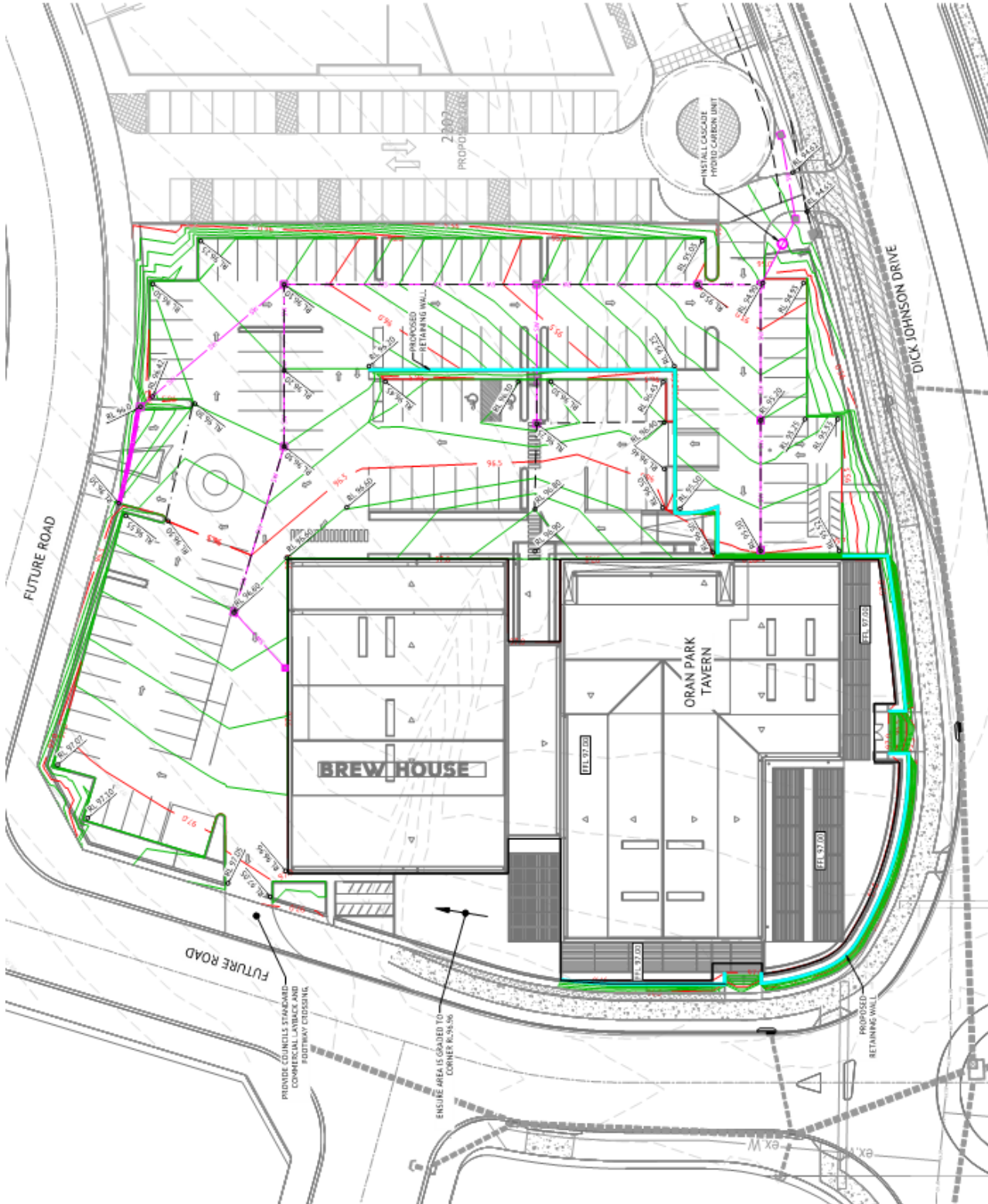
319187

**Attachment 8**

**CLPP01**

CLPP01

Attachment 8



NOTES:

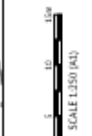
1. REFER TO HYDRAULIC CONSULTANT'S DRAWINGS FOR ALL PRIVATE LOT DRAINAGE INCLUDING BANHWATER TANK, DOWNPIPE & BUILDING GUTTERING DETAILING.
2. REFER TO LANDSCAPE PLANS FOR FURTHER DETAILS ON PROPOSED LANDSCAPE WALLS.
3. REFER TO STRUCTURAL PLANS FOR ALL WALL TYPICAL DETAILS & SLAB DROP EDGE BEAMS DETAILS. LOCATION OF EDGE BEAMS ARE THE STRUCTURAL ENGINEER'S ULTIMATE DISCRETION.
4. ALL BOXES MUST BE COLOURED FOR THIS PLAN TO BE VALID.
5. PROVIDE COMMERCIAL INDUSTRIAL LAMPBACK TO ALL PROPOSED LAMPWAY CROSSOVERS.
6. LIGHT POLE LOCATIONS TO BE CONFIRMED TO REQUIREMENTS OF ENDORSEMENT ENERGY.

NOTE: REFER DRAWING TO FOR NOTES AND LEGEND.

319187

MOMENTO HOSPITALITY

ORAN PARK TAVERN  
DICK JOHNSON DRIVE, ORAN PARK

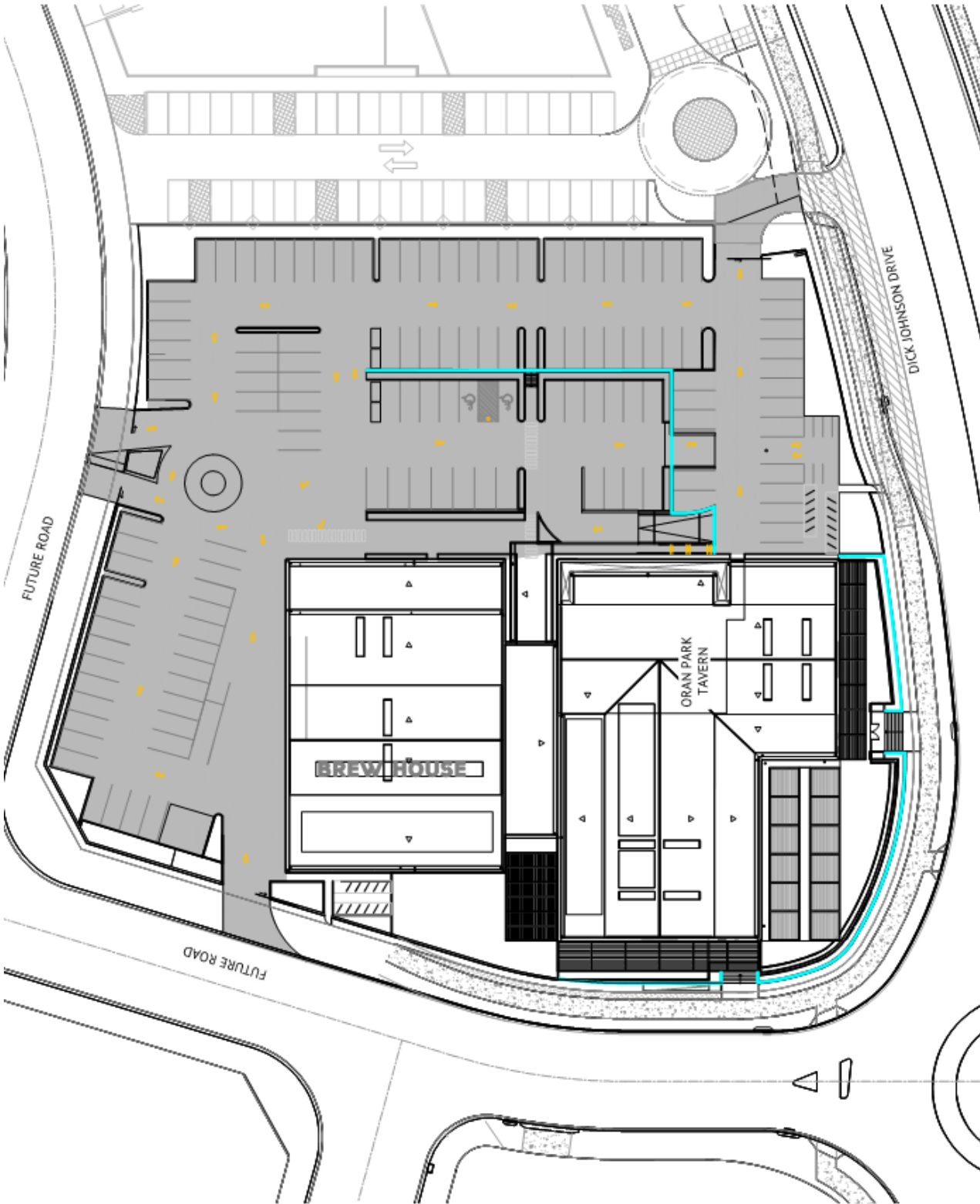


DATE: 11/11/22  
PROJECT: 319187  
DRAWING NUMBER: 319187-01

ORAN PARK OFFICE  
SUITE 301, LEVEL 3, ORAN PARK PODIUM  
351 ORAN PARK DRIVE  
ORAN PARK, NSW 2270



CONCEPT DESIGN



LEGEND

PROPOSED DRIVEWAY AND PARKING

NOTE: ALL SIGN POST FOOTINGS TO BE IN ACCORDANCE WITH CAMPBELL TOWN CITY COUNCIL STANDARD DRAWING SD-H18 (USING RIBBI GALVANISED M.S. C1300 HEAVY GAUGE 40mm).

NOTE: REFER DRAWING C FOR NOTES AND LEGEND.

**CONCEPT DESIGN**

ORAN PARK OFFICE  
SUITE 301, LEVEL 3, ORAN PARK PODIUM  
551 ORAN PARK DRIVE  
ORAN PARK, NSW 2570

PROJECT NUMBER: 319187

DATE: 15/09/2022

PROJECT: ORAN PARK TAVERN

LOCATION: DICK JOHNSON DRIVE, ORAN PARK

SCALE: 1:150 (A4)

SCALE BAR: 0m, 5m, 10m, 15m

REVISIONS:

NO.	DATE	DESCRIPTION
1	15/09/2022	ISSUED FOR PERMIT

**Attachment 8**

**CLPP01**

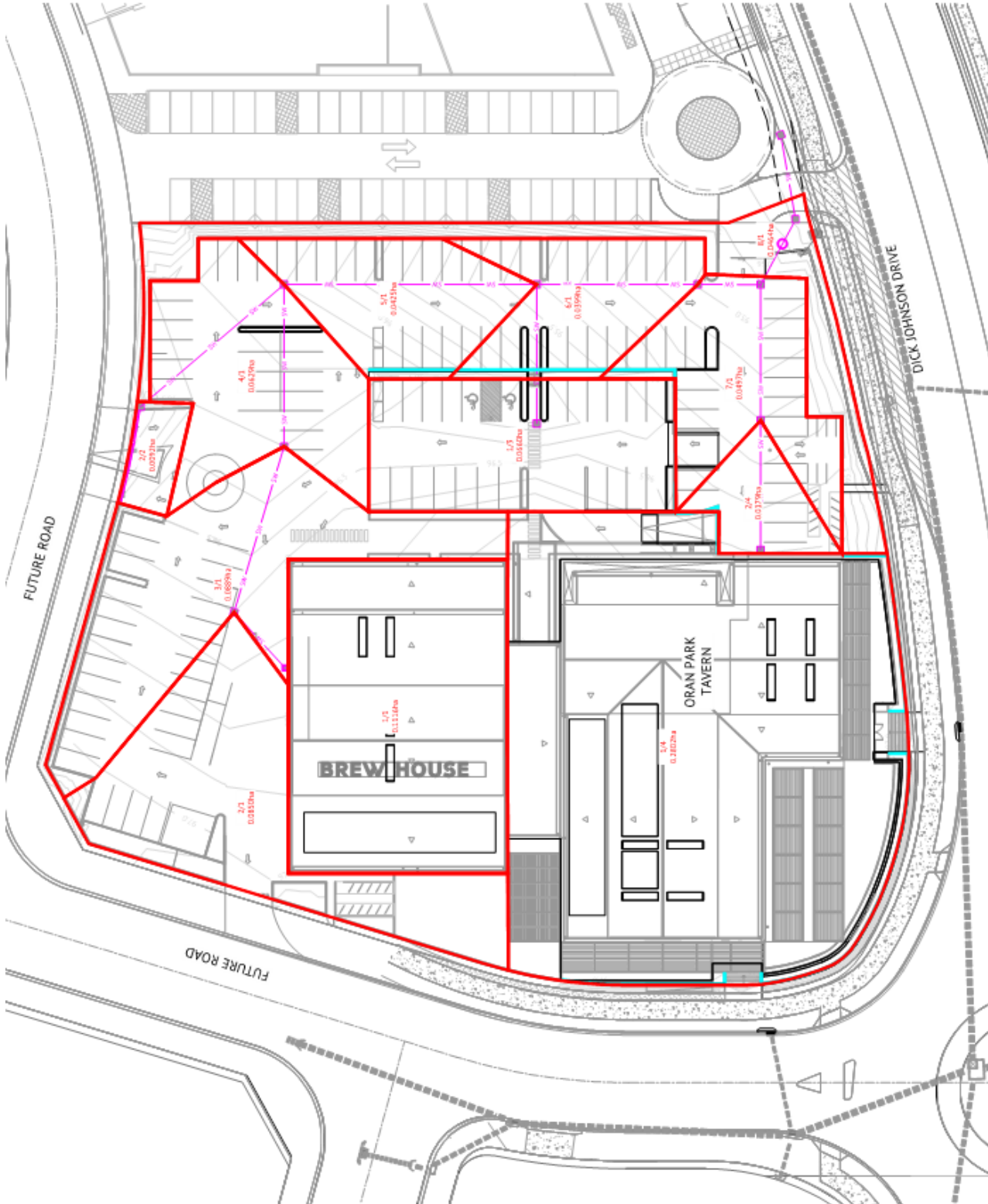
CLPP01

Attachment 8



LEGEND - PROPOSED

- STORMWATER CATCHMENT BOUNDARY
- STORMWATER CATCHMENT NUMBER AND AREA
- FINISHED CONTIGUOUS

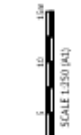


NOTE: REFER DRAWING C FOR NOTES AND LEGEND.

319187

MOMENTO HOSPITALITY

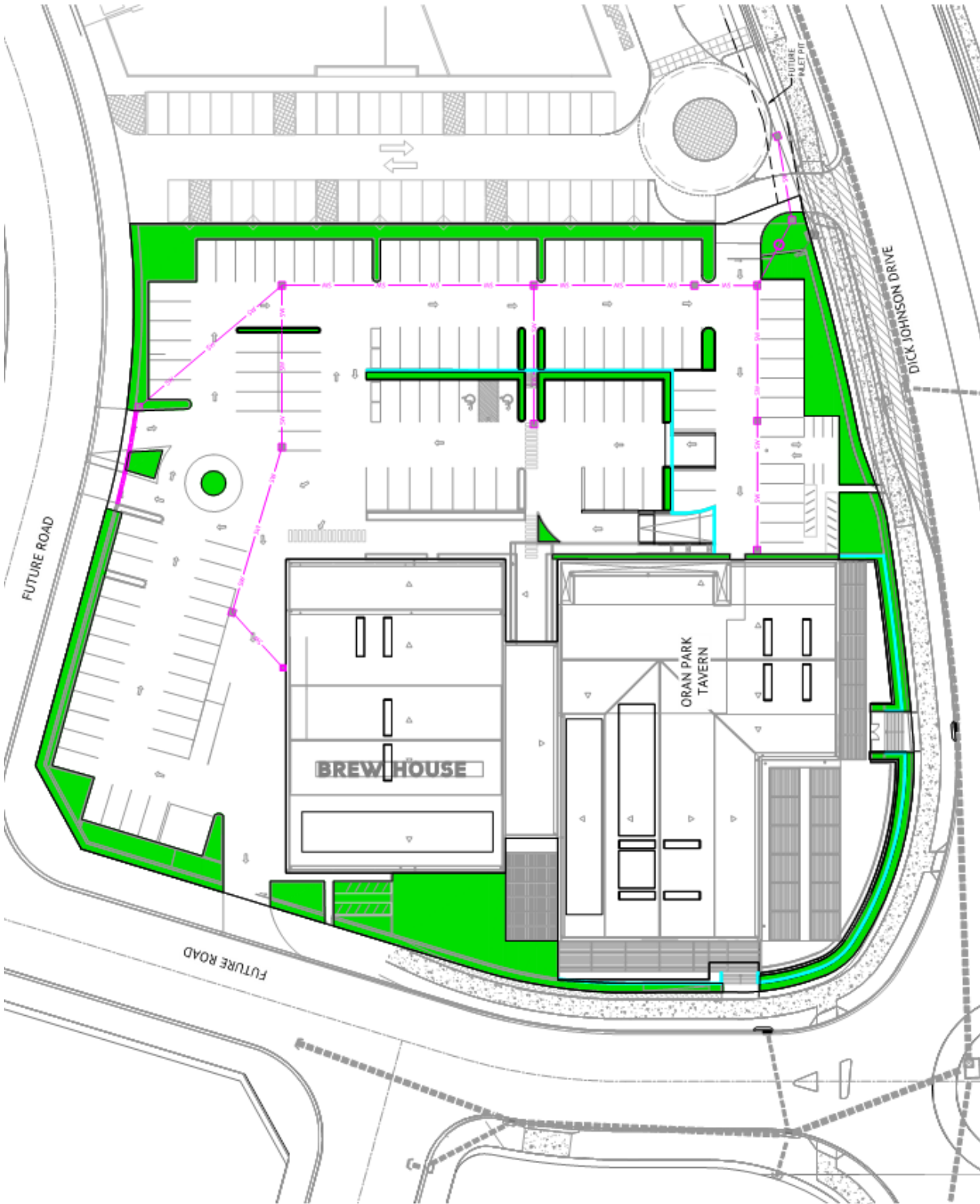
ORAN PARK TAVERN  
DICK JOHNSON DRIVE, ORAN PARK



ORAN PARK OFFICE  
SUITE 301, LEVEL 3, ORAN PARK PODIUM  
351 ORAN PARK DRIVE  
ORAN PARK, NSW 2570



CONCEPT DESIGN



**LEGEND:**  
 PREVIOUS AREA

**NOTE:**  
 TOTAL SITE AREA = 9,356sqm  
 TOTAL PREVIOUS AREA = 1,189sqm (12.8%)

NOTE: REFER DRAWING C FOR NOTES AND LEGEND.

319187  
 DRAWING NUMBER

MOMENTO HOSPITALITY

ORAN PARK TAVERN  
 DICK JOHNSON DRIVE, ORAN PARK

SCALE 1:150 (A4)

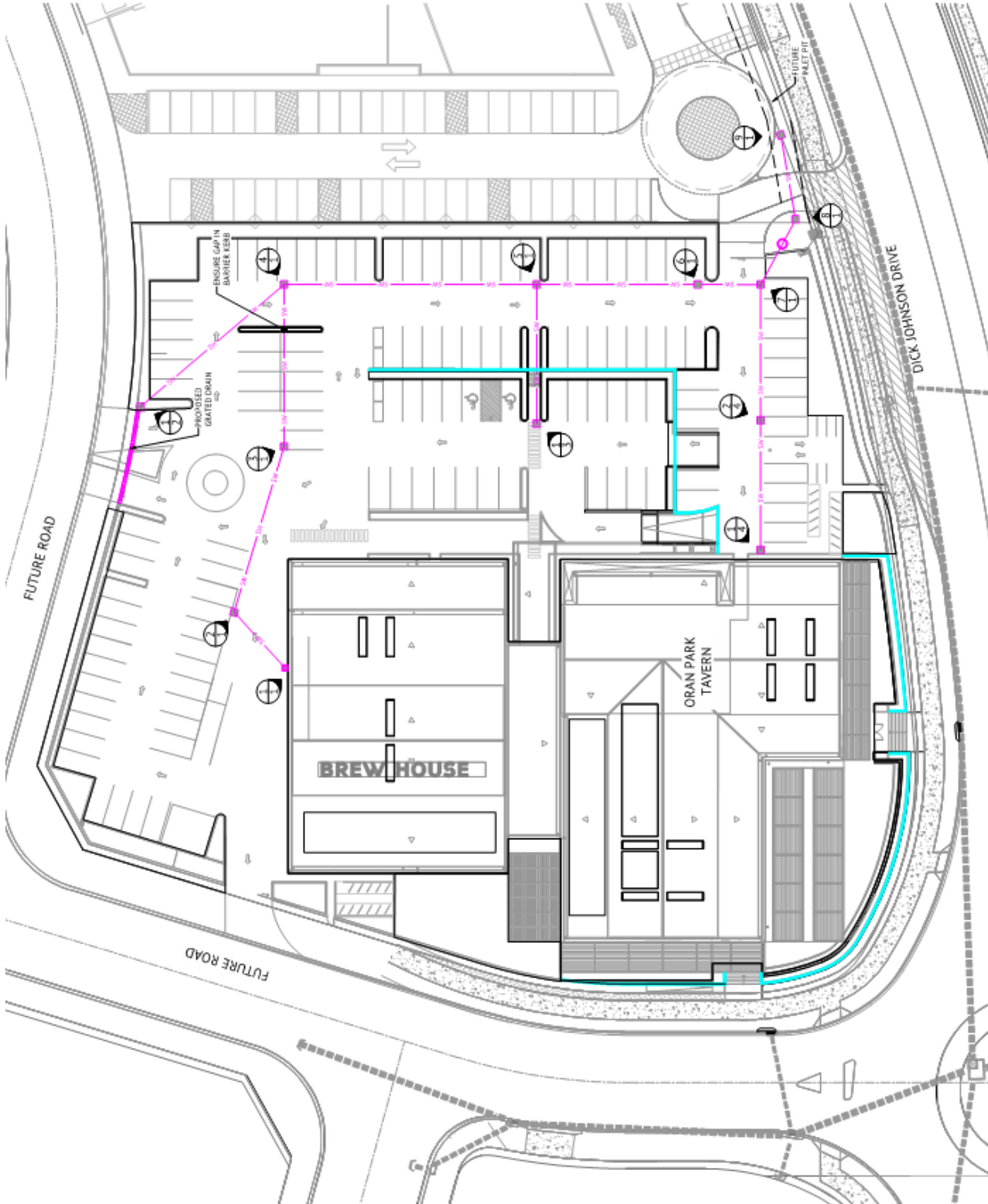
ORAN PARK OFFICE  
 SUITE 301, LEVEL 3, ORAN PARK PODIUM  
 351 ORAN PARK DRIVE  
 ORAN PARK, NSW 2570



CONCEPT DESIGN

Attachment 8 **CLPP01**

Attachment 8 CLPP01

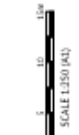


NOTE: REFER DRAWING C FOR NOTES AND LEGEND.

319187

MOMENTO HOSPITALITY

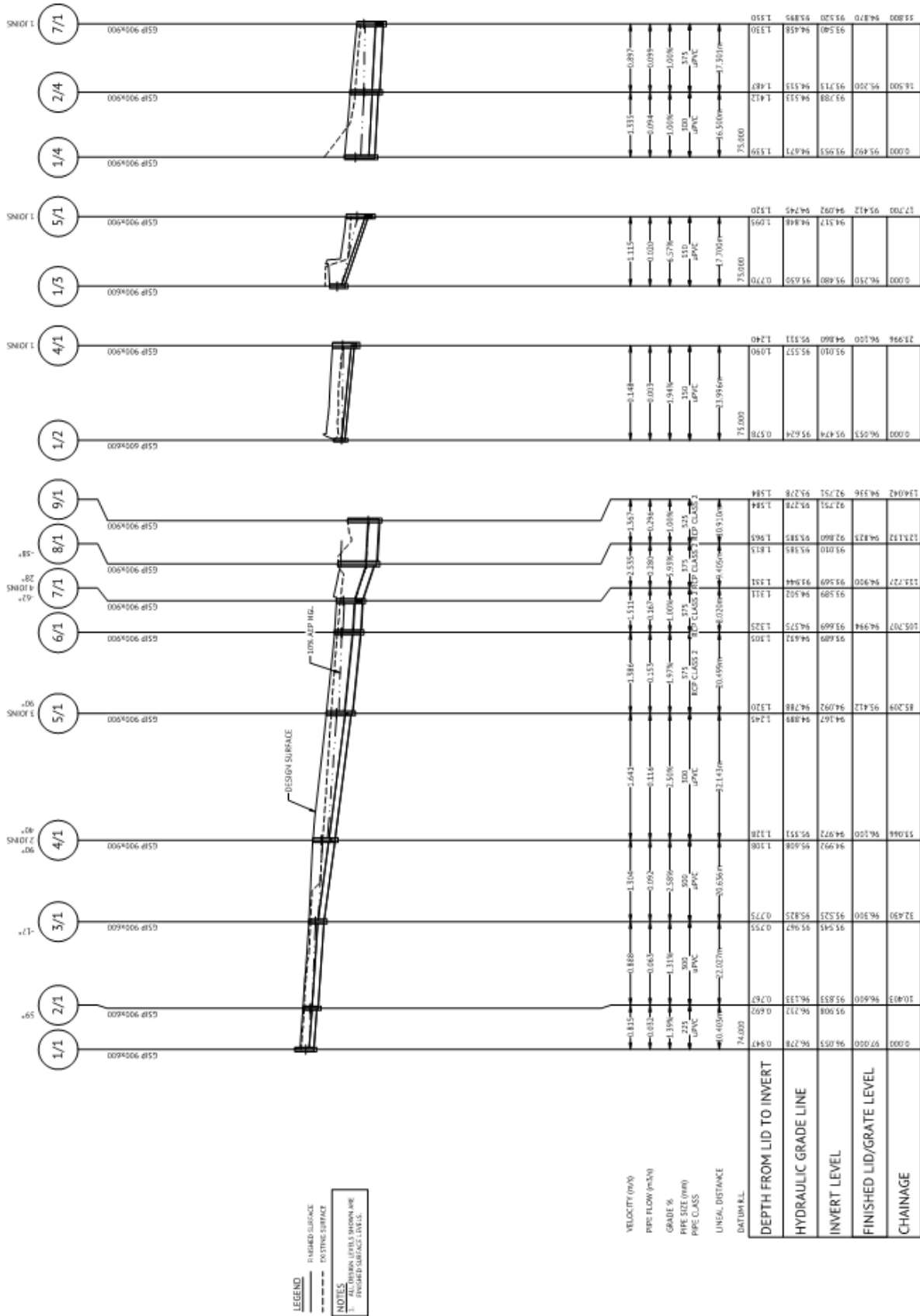
ORAN PARK TAVERN  
DICK JOHNSON DRIVE, ORAN PARK



ORAN PARK OFFICE  
SUITE 301, LEVEL 3, ORAN PARK PODIUM  
351 ORAN PARK DRIVE  
ORAN PARK, NSW 2570



CONCEPT DESIGN



**CONCEPT DESIGN**

DATE: 08/22/22  
 DRAWN BY: J. HARRIS  
 CHECKED BY: J. HARRIS  
 PROJECT NUMBER: 319187

**MOMENTO HOSPITALITY**  
**ORAN PARK TAVERN**  
 DICK JOHNSON DRIVE, ORAN PARK

SCALE: HORIZONTAL 1"=50' (A3) 2"=50'  
 VERTICAL 1"=10' (A3) 2"=10'

**ORAN PARK OFFICE**  
 SUITE 301, LEVEL 3, ORAN PARK PODIUM  
 551 ORAN PARK DRIVE  
 ORAN PARK, NSW 2270

**CLPP01**

Attachment 8 CLPP01

STORMWATER PIT SETOUT COORDINATES			
NAME	EASTING	NORTHING	TYPE
1/L	53.05483	94.329768	GSIP 900x900
2/L	62.70932	101.352205	GSIP 900x900
3/L	81.797709	94.989371	GSIP 900x900
4/L	102.433513	94.989105	GSIP 900x900
5/L	102.432896	62.895971	GSIP 900x900
6/L	102.433181	42.347336	GSIP 900x900
7/L	102.432896	34.327201	GSIP 900x900
8/L	110.732897	29.904222	GSIP 900x900
9/L			JUNCTION PIT 900 x 900
1/2	86.879051	113.341256	GSIP 400x600
1/5	84.733855	62.895605	GSIP 900x900
1/4	68.632896	54.347222	GSIP 900x900
2/4	85.131396	34.327201	GSIP 900x900
1/4	86.632896	54.347222	GSIP 900x900
3/4	85.131396	54.327201	GSIP 900x900

CONCEPT DESIGN



ORAN PARK OFFICE  
SUITE 301, LEVEL 3, ORAN PARK PODIUM  
351 ORAN PARK DRIVE  
ORAN PARK, NSW 2570

DATE: 11/09/22  
P.L.T.:  
R.S.:  
PROJECT NUMBER:  
S.E.:

SCALE

COURT  
PROJECT  
LOCATION

MOMENTO HOSPITALITY  
ORAN PARK TAVERN  
DICK JOHNSON DRIVE, ORAN PARK

319187  
SHEET NUMBER



STRUCTURE NUMBER	DOWNSTREAM STRUCTURE	CONTRIBUTING SUB-CATCHMENTS	TIME OF CONCENTRATION	RAINFALL INTENSITY	CO-EFFICIENT OF RUNOFF	SUB-CATCHMENT AREA	EQUVALENT AREA	SUB-CATCHMENT DISCHARGE (INC. BYPASS)	FLOW WIDTH	FLOW DEPTH	ROAD GRADE AT INLET	HALF ROAD CAPACITY	FLOW INTO INLET	BYPASS FLOW	BYPASS STRUCTURE NUMBER	CRITICAL TIME OF CONCENTRATION	RAINFALL INTENSITY	TOTAL (L x A)	SUM ADDITIONAL PIPE FLOW	PIPE FLOW	REACH LENGTH	PIPE GRADE	PIPE/BOX DIMENSIONS	CLASS	FULL PIPE VELOCITY	TIME OF FLOW IN REACH	CHARTS USED	Q <sub>90%</sub>	D <sub>90%</sub>	S/D <sub>90%</sub>	VELOCITY HEAD	UPSTREAM HEADLOSS CO-EFFICIENT	UPSTREAM HEADLOSS	WSE CO-EFFICIENT	CHANGE IN WSE	PIPE FRICTION SLOPE	PIPE FRICTION HEADLOSS (L x S)	NORMAL DEPTH	NORMAL DEPTH VELOCITY (MINOR STORM)	NORMAL DEPTH VELOCITY (1 YEAR STORM)	UPSTREAM OVERT LEVEL	DOWNSTREAM OVERT LEVEL	UPSTREAM H.G.L.	DOWNSTREAM H.G.L.	W.S.E.	SURFACE OR GRATE LEVEL	STRUCTURE NUMBER
2/1	2/1	1/1	4.00	136	0.89	0.120	0.024	40	10.14	0.025	8115	31	52	8	2/1	6.00	136	0.106	0	52	30.405	1.991	225	PPVC	0.81	0.15	92	1.00	1.83	0.024	5.54	0.187	0.387	0.066	0.137	0.066	0.137	1.40	0.80	0.80	96.212	96.465	97.000	1/1			
2/1	3/1	1/1, 2/1	4.00	136	0.89	0.090	0.080	38	10.14	0.025	8115	31	63	7	3/1	6.13	135	0.186	0	63	31.027	1.908	300	PPVC	0.89	0.23	37.42	0.49	0.75	0.090	1.95	0.079	1.99	0.080	0.75	0.166	0.162	1.65	0.80	0.80	95.967	96.211	96.699	2/1			
3/1	4/1	1/1, 2/1, 3/1	6.00	136	0.89	0.090	0.080	30	10.81	0.021	70	30	92	7	4/1	6.36	134	0.266	0	92	30.635	2.584	300	PPVC	1.30	0.16	33.34	0.33	1.03	0.087	1.64	0.143	0.342	1.05	0.217	0.186	2.29	0.80	0.80	95.608	95.967	96.500	3/1				
4/1	5/1	1/1, 2/1, 3/1, 4/1	6.00	136	0.89	0.065	0.056	21	9.82	0.021	2875	22	116	7	5/1	6.52	134	0.550	0	116	32.145	2.505	300	PPVC	1.64	0.25	44.46	0.19	1.00	0.137	1.87	0.237	2.20	0.305	1.44	0.462	0.195	2.58	0.80	0.80	94.889	95.654	96.100	4/1			
5/1	6/1	1/1, 2/1, 3/1, 4/1, 5/1	4.00	136	0.89	0.045	0.038	14	5.91	0.026	4205	19	153	5	6/1	6.77	133	0.651	0	153	30.999	1.966	375	RCPP	1.59	0.15	51.95	0.12	0.89	0.102	0.160	1.13	0.111	0.76	0.156	0.214	2.55	0.80	0.80	94.652	94.899	95.412	5/1				
6/1	7/1	1/1, 2/1, 3/1, 4/1, 5/1, 6/1	4.00	136	0.89	0.040	0.035	13	5.189	0.025	5582	15	147	4	7/1	6.92	132	0.966	0	147	8.020	1.000	375	CLAS	1.51	0.08	33	0.89	1.00	0.117	0.49	0.037	0.657	0.91	0.075	0.292	1.81	0.80	0.80	94.502	94.652	94.994	6/1				
7/1	8/1	1/1, 2/1, 3/1, 4/1, 5/1, 6/1, 7/1	6.00	136	0.89	0.050	0.044	17	6.424	0.026	5713	17	280	5	8/1	6.88	132	0.776	0	280	9.405	5.933	375	RCPP	2.53	0.04	36.44	0.16	1.00	0.328	1.70	0.559	1.84	0.604	5.93	0.558	0.223	4.12	0.80	0.80	93.385	94.547	94.990	7/1			
8/1	9/1	1/1, 2/1, 3/1, 4/1, 5/1, 6/1, 7/1, 8/1	6.00	136	0.89	0.050	0.044	17	21	0.026	17	17	296	4	1057	6.92	132	0.820	0	296	30.910	1.000	325	RCPP	1.37	0.09	34.37	0.86	0.71	0.095	0.00	0.000	0.000	0.000	0.107	0.320	2.14	0.80	0.80	93.278	93.385	94.823	8/1				
9/1	10/1		4.00	136	0.89	0.020	0.029	3	1.634	0.026	8	3	3	1	4/1	6.00	136	0.029	0	3	25.994	1.957	150	PPVC	0.15	0.53	32	1.00	1.05	0.001	7.00	0.028	0.608	0.54	0.129	0.056	0.82	0.80	0.80	93.608	93.744	94.182	9/1				
10/1	11/1		6.00	136	0.89	0.070	0.062	23	4.337	0.028	2916	20	20	4	5/1	6.03	136	0.062	0	20	17.700	6.571	150	PPVC	1.11	0.13	32	1.00	2.56	0.063	3.68	0.233	0.233	4.19	0.741	0.075	2.21	0.80	0.80	94.889	95.863	96.250	10/1				
11/1	12/1		6.00	136	0.89	0.282	0.220	94	94		94	94	94	0		6.00	136	0.220	0	94	16.500	1.000	300	PPVC	1.54	0.20	32	1.00	3.27	0.091	2.96	0.289	0.269	0.95	0.157	0.240	1.54	0.80	0.80	94.558	94.994	95.492	11/1				
12/1	13/1		6.00	136	0.89	0.000	0.018	7	2.738	0.025	1837	5	99	1	7/1	6.20	135	0.268	0	99	17.301	1.000	375	PPVC	0.90	0.18	33	0.85	0.80	0.041	0.00	0.000	0.000	0.32	0.055	0.202	1.64	0.80	0.80	94.502	94.558	95.200	12/1				
13/1	14/1		4.00	136	0.89	0.382	0.230	94	94		94	94	94	0		6.00	136	0.230	0	94	16.500	1.000	300	PPVC	1.54	0.20	32	1.00	3.27	0.091	2.96	0.289	0.269	0.95	0.157	0.240	1.54	0.80	0.80	94.558	94.994	95.492	13/1				
14/1	15/1		6.00	136	0.89	0.000	0.018	7	2.738	0.025	1837	5	99	1	7/1	6.20	135	0.268	0	99	17.301	1.000	375	PPVC	0.90	0.18	33	0.85	0.80	0.041	0.00	0.000	0.000	0.32	0.055	0.202	1.64	0.80	0.80	94.502	94.558	95.200	14/1				
15/1	16/1		6.00	136	0.89	0.000	0.044	17	4.424	0.026	5713	17	280	5	8/1	6.88	132	0.776	0	280	9.405	5.933	375	RCPP	2.53	0.04	36.44	0.16	1.00	0.328	1.70	0.559	1.84	0.604	5.93	0.558	0.223	4.12	0.80	0.80	93.385	93.944	94.990	15/1			
16/1	17/1		6.00	136	0.89	0.050	0.044	17	22	0.026	17	17	280	5	8/1	6.88	132	0.776	0	280	9.405	5.933	375	RCPP	2.53	0.04	36.44	0.16	1.00	0.328	1.70	0.559	1.84	0.604	5.93	0.558	0.223	4.12	0.80	0.80	93.385	93.944	94.990	16/1			

**CONCEPT DESIGN**

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**ORAN PARK OFFICE**  
SUITE 101, LEVEL 3 ORAN PARK POOLUM  
351 ORAN PARK DRIVE  
ORAN PARK, NSW 2570

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**ORAN PARK TAVERN**  
DICK JOHNSON DRIVE, ORAN PARK

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**MOMENTO HOSPITALITY**

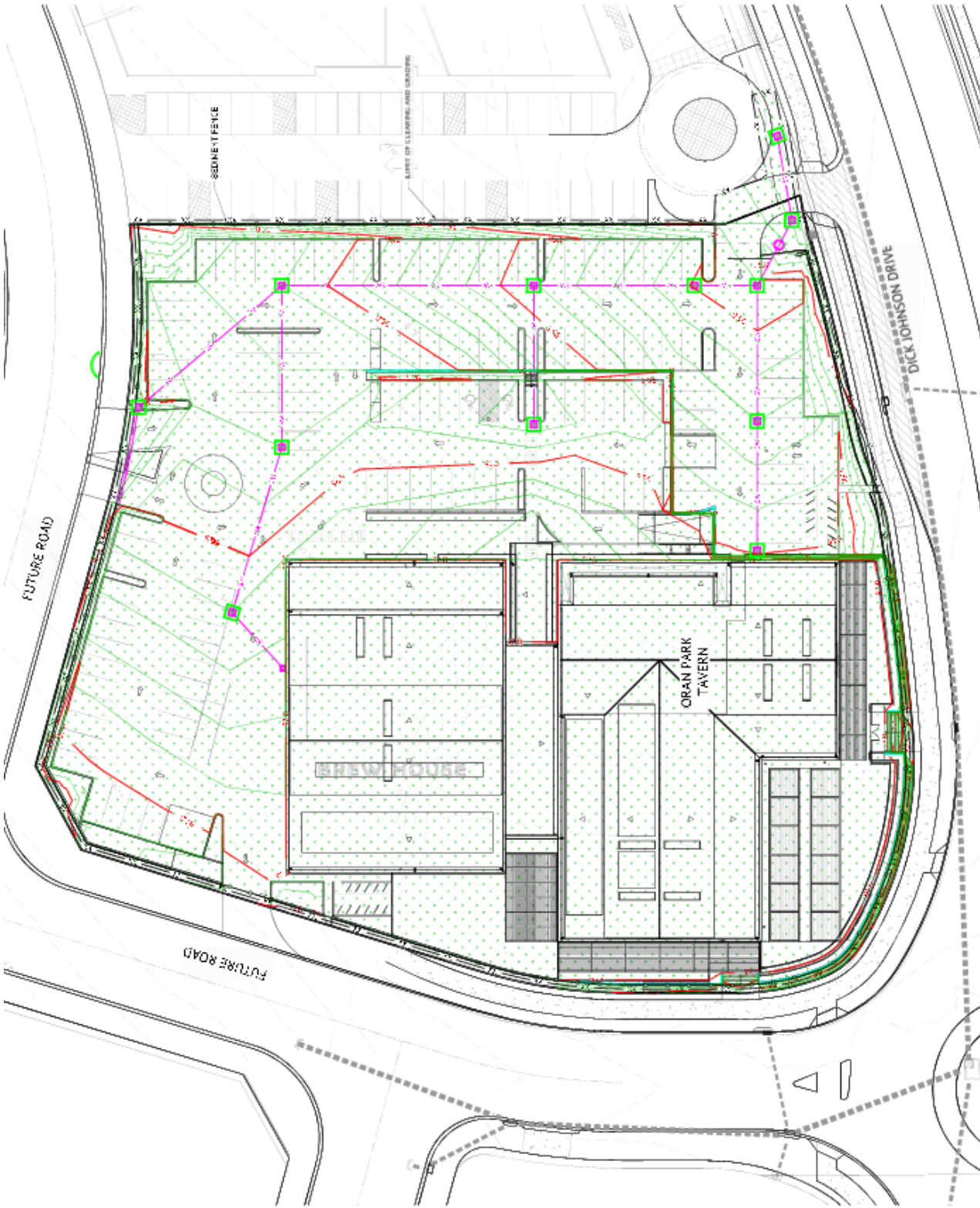
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**GEO-012**

**CLPP01**

**Attachment 8**





- NOTES:**  
 REFER TO THE SUBMITTAL COMMENTS FOR THE 10/14/2022 SET OF PLANS.
- LEGEND:**
- 10' SETBACK
  - 20' SETBACK
  - 30' SETBACK
  - 40' SETBACK
  - 50' SETBACK
  - 60' SETBACK
  - 70' SETBACK
  - 80' SETBACK
  - 90' SETBACK
  - 100' SETBACK
  - 110' SETBACK
  - 120' SETBACK
  - 130' SETBACK
  - 140' SETBACK
  - 150' SETBACK
  - 160' SETBACK
  - 170' SETBACK
  - 180' SETBACK
  - 190' SETBACK
  - 200' SETBACK

NOTE: REFER TO PLANS 0 FOR NOTES AND LEGEND

319187

MOMENTO HOSPITALITY

ORAN PARK TAVERN  
 DICK JOHNSON DRIVE, ORAN PARK

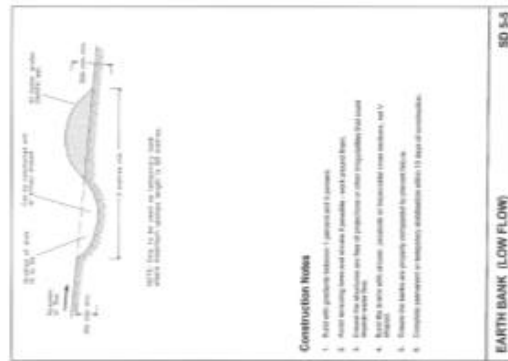
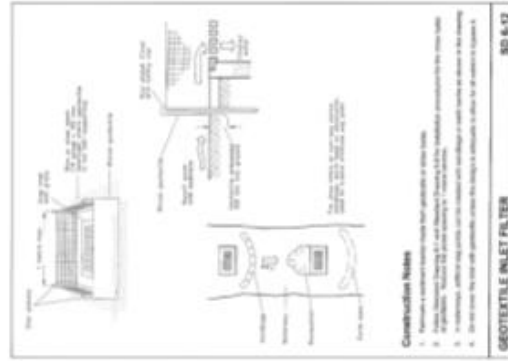
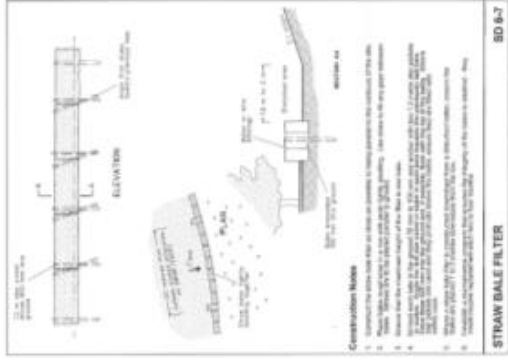
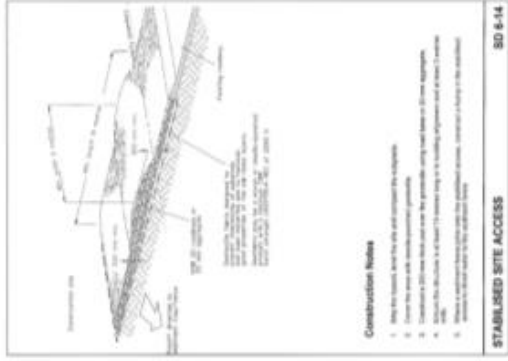
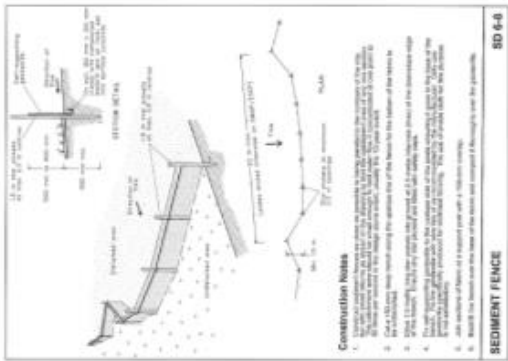
SCALE 1/8" = 1'-0"

ORAN PARK OFFICE  
 DATE: 10/14/2022  
 01 ORAN PARK - 10/14/2022  
 ORAN PARK, NSW 2070



CONCEPT DESIGN

Attachment 8 **CLPP01**



**CONCEPT DESIGN**

ORAN PARK OFFICE  
SUITE 301, LEVEL 3, ORAN PARK PROLOG  
351 ORAN PARK DRIVE  
ORAN PARK, NSW 2270

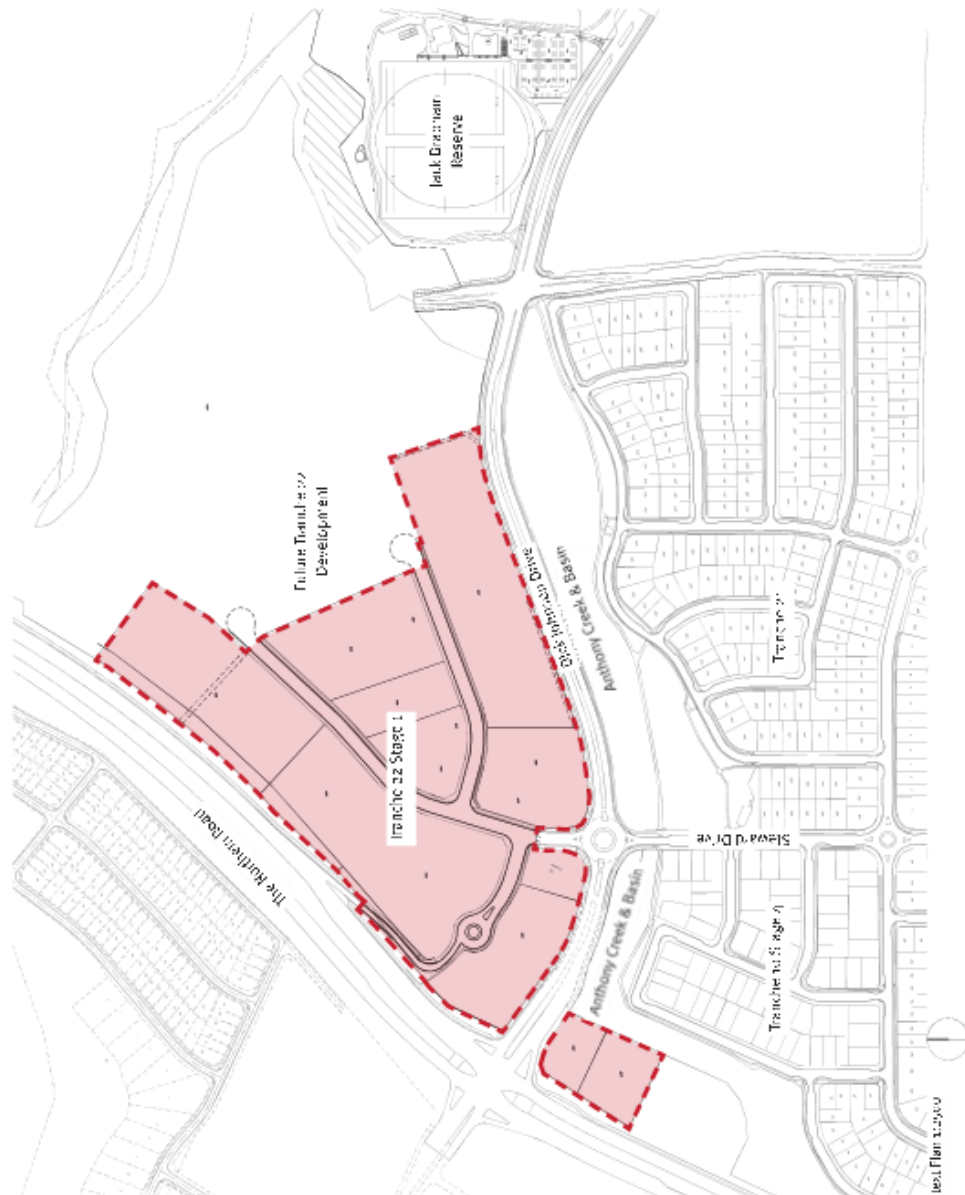
ORAN PARK TAVERN  
DICK JOHNSON DRIVE, ORAN PARK

MOMENTO HOSPITALITY

319187

Oran Park Town

Tranche 22 Stage 1



Drawing Register

Drawing No.	Drawing Title	Scale	Issue No.
L01	Title Sheet	1:2500	A
L02	Preliminary	1:2500	A
L03	Street Tree Plan	1:1000	A
L04	Site Solutions	1:100	A
L05	Street Tree Data Table	NA	A
L06	Indicative Planting & Materials Palette	NA	A

Revisions

No.	Date	Description	Drawn	Check
01	12/20	Issue for construction	CF	RTB

Consultant: **GREENFIELDS** **calibra**

15-1 James Street  
 Level 11 NSW 2008  
 Telephone: 02 9550 6444

Project: **OP1 Tranche 22 Stage 1**  
 Drawing Title:

Date: **12/2020**  
 Scale:

Drawing No. **1 01**

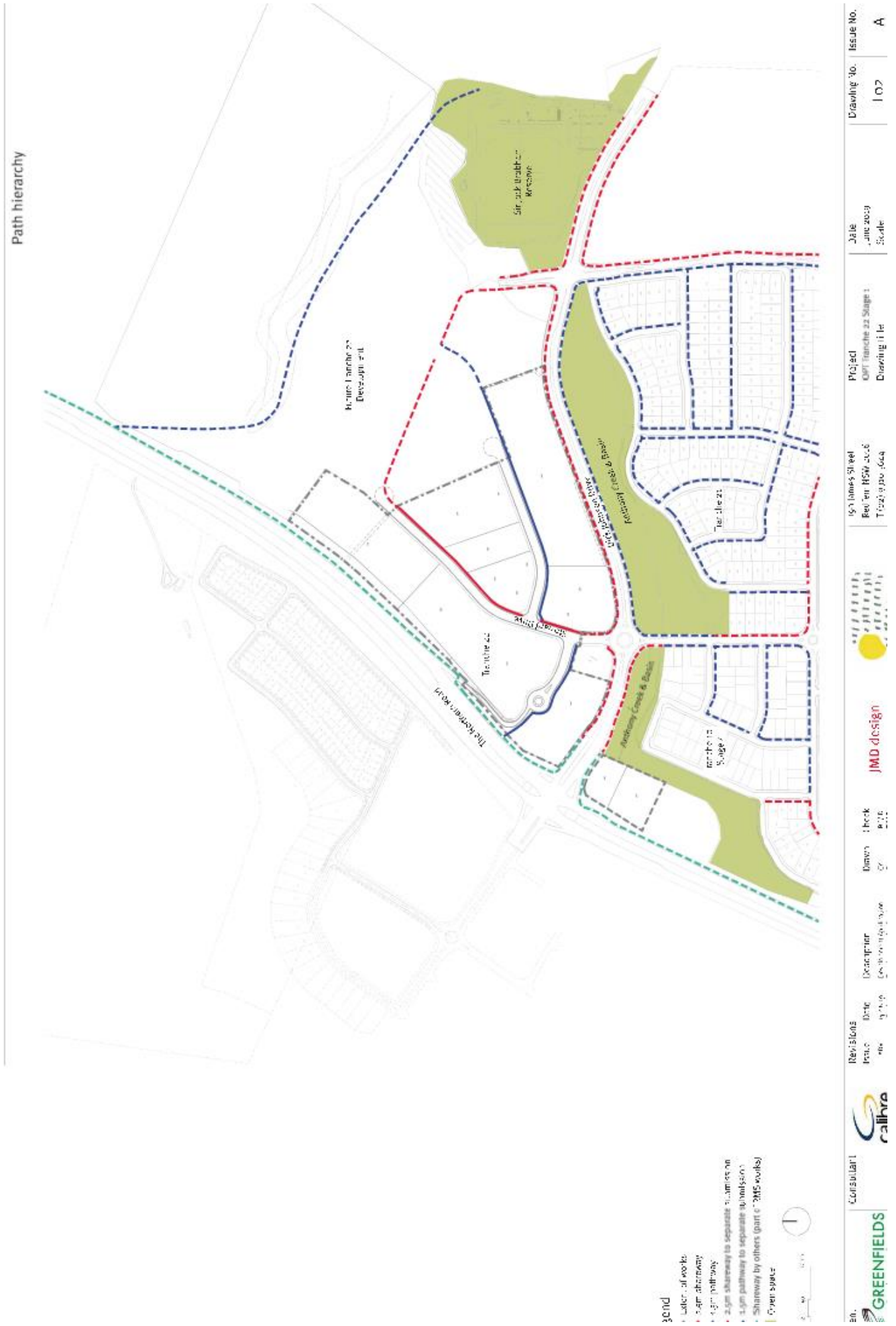
Issue No. **A**

Attachment 8

CLPP01

Attachment 8 CLPP01

Path hierarchy





- Extent of works
- Max. 2.5m high retaining wall refer engineer's documentation
- Max. 1.0m high retaining wall refer engineer's documentation
- Proposed 2.0m high spear security fence
- Proposed W-beam guard rail refer engineer's documentation
- Proposed contour lines refer engineer's documentation
- Electrical easement
- Property boundary
- 1.5m to 2.5m concrete footpath
- Reinforced concrete to engineer's documentation
- Mass planting to 4.5m high under electrical easement
- Turf
- Native grasses, groundcovers & shrubs mix (SM2 & SM3) Refer RMS's documentation
- Rural native grasses refer RMS's documentation
- Tree planting clusters (IC26) Refer RMS's documentation
- Stormwater lines and kerb inlet pits refer to engineer's documentation
- Existing street tree refer separate landscape package
- Restaurant
- Service station
- Fire station
- Tavern

**Decorative Plant Schedule**

Item	Botanical Name	Common Name	Height & Spread @ 12 Years	Install Pot Size
Native Trees				
1	<i>Ficus rubiginosa</i>	Rusty Fig	8.1 x 10m	400L
2	<i>Flindersia australis</i>	Crow's Ash	3.5 x 11m	75L
3	<i>Lophostemon confertus</i>	Bush Box	1.2 x 10m	75L
4	<i>Fraxinus pennsylvanica</i> 'Urbanite'	Green Ash	1.2 x 8m	75L
5	<i>Pyrus calleryana</i> 'Chanticleer'	Ornamental Pear	1.4 x 16m	75L

Consultant

Description: Development Application  
 Drawn: CY  
 Check: BC/00  
 JMD design

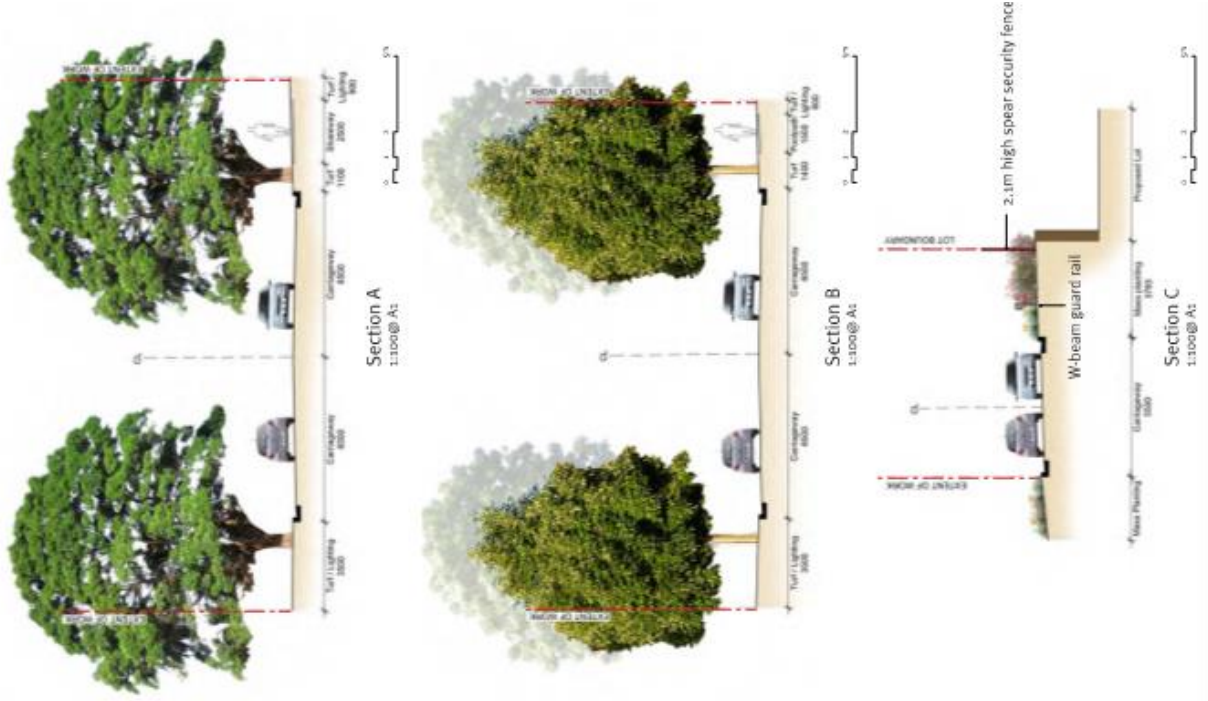
Project: OPF Tranche 22 Stage 1 Drawing Title  
 Date: June 2019 Scale  
 Drawing No.: LO3 Issue No.: A

190 James Street  
 Redfern NSW 2016  
 T (02) 9310 5644

**Attachment 8** **CLPP01**

# Attachment 8 CLPP01

Site Sections



Consultant	GREENFIELDS	Consultant	calibra	Revisions	Issue	ATW	Date	13.05.19	Description	Development Application	Drawn	CY	Check	BC/00	Project	OPF Tranche 22 Stage 1	Drawing title	Date	June 2019	Scale	LO4	Drawing No.	CLPP01	Issue No.	A



Street Tree Data Table

Botanical Name	Common Name	Photos	Mature Dimensions Height   Spread	Seasonal / Special Features	Preferred Soil Conditions	Drought / Frost Tolerance	Pest & Disease Susceptibility	Maintenance Requirement	Availability	Nuisance	Longevity/ Growth Rate	Root Damage Potential	Branch Shedding	Solar Acc
<i>Ficus rubiginosa</i>	Rusty Fig		15m   11m	Foliage: Dark Green top/Bronze under, Smooth Bark: grey-brown Clusters of yellow/red fruits in Spring and summer. Bird and small mammal attracting	Prefers moist fertile soils with good moisture levels in summer. Adaptable to a wide range of soil conditions.	Tolerates moderate exposure to salt-laden winds. Young trees are frost sensitive	No known susceptibility to any specific pests or diseases.	Regular watering during establishment. Not suitable for positions beneath powerlines.	Generally available from most commercial nurseries.	Small fleshy fruit 15-20mm in diameter.	Long-lived - 50 years plus in urban areas. Moderate.	Proposed location in roundabouts only.	Not known susceptibility to branch shedding	Evergreen not suitable where solar access in winter is required
<i>Flindersia australis</i>	Crow's Ash		15m   11m	Glossy green foliage	Prefers well drained fertile soils with good moisture levels in summer	Tolerates light frosts to minus 3°C	No known susceptibility to any specific pests or diseases.	Regular watering during establishment. Usually develops a single trunk.	Not readily available from commercial nurseries - may be available from specialist growers, propagated from seed.	Large woody fruits	Long-lived - 50 years plus. Moderate.	Root system is not considered to be extensive or overly vigorous.	Not known susceptibility to branch shedding	Evergreen not suitable where solar access in winter is required
<i>Fraxinus pennsylvanica</i> "Urbanite"	Green Ash		12m   8m	Architectural form. Bright yellow autumn foliage	Prefers deep alluvial soils with good sub-soil moisture levels in summer, but will tolerate low rainfall areas.	Tolerates poorly drained soils and mild drought.	No known pest or disease susceptibilities.	Regular watering during establishment.	Available from commercial nurseries in cooler climates such as Victoria. Several cultivars are clonally propagated.	Leaf fall (devoidous) but relatively small leaves and winged seeds.	Moderate to fast	Root system is not considered to be extensive or overly vigorous	No known susceptibility	Deciduous
<i>Pyrus calleryana</i> "Chariticeer"	Ornamental Pear		14m   6m	Dark sheds in small platelets leaving mottled irregular patterned trunk	Prefers moderately fertile soils with good moisture levels in summer	Drought sensitive.	Susceptible to aphids and thrips which may lead to sooty mould.	Require watering during establishment. Relatively regular periodic crown lifting to provide vehicular clearance	Generally available from commercial nurseries. Clonal material should be selected for propagation.	Leaf fall in autumn. Leaves are relatively small.	Moderate	Root system is not considered to be extensive or overly vigorous	No known susceptibility	Deciduous
<i>Lophostemon confertus</i>	Brush Box		15m   12m	Broad elliptical to rounded form with single trunk and upright branching habit	Adaptable to range of soil conditions, but perform best on deep fertile moist clay loams with moderate drainage	Prefers sheltered moist situation Tolerant of poor drainage & neglect Hardy to minus 3°C	No known pest or disease susceptibilities	Regular watering during establishment	Generally available in commercial nurseries	Consistent dense mats of dead leaves Some bark shedding in small strips	Long-lived - 50 years plus in urban areas Fast growth rate	Root system is not considered to be extensive or overly vigorous	No known susceptibility to branch shedding	Evergreen not suitable where solar access in winter is required

Consultant: caltra  
 Revisions: Issue 01, Date 14.08.19  
 Description: Development Application  
 Drawn: CY, Check: BC, 01  
 Project: OPF Tranche 22 Stage 1 Drawing (title)  
 Date: June 2019 Scale  
 Drawing No.: L05 Issue No.: A

Attachment 8 CLPP01

Indicative Planting & Materials Palette

Indicative Materials Palette

Botanical name	Common name	Height	Pot size
<b>Shrubs</b>			
<i>Callistemon citrinus</i> 'Little John'	Bottlebrush	0.4m	140mm
<i>Westringia fruticosa</i> 'Grey box'	Coastal Rosemary	0.45m	200mm
<i>Westringia fruticosa</i> 'Zena'	Coastal Rosemary	0.6m	200mm
<b>Grasses and Groundcovers</b>			
<i>Casuarina glauca</i> 'Cousin It'	Casuarina Cousin It	0.15m	140mm
<i>Gazania tomentosa</i>	Gazania	0.1m	140mm
<i>Grevillea juniperina</i>	Juniper-leaf Grevillea	0.3m	140mm
<i>Grevillea lanigera</i> 'Mt Tamboritha'	Wooly Grevillea	0.3m	140mm



Indicative Materials Palette

Consultant	Revisions	Check	Drawing No.	Issue No.
<b>GREENFIELDS</b>	Issue: ATW Date: 14.05.19	Drawn: CY Check: BC/00	LO6	A
Project	Date	Scale	Drawing title	
OPF Tranche 22 Stage 1	June 2019		OPF Tranche 22 Stage 1	
190 James Street Redfern NSW 2016 T (02) 9310 5644	JMD design		190 James Street Redfern NSW 2016 T (02) 9310 5644	

**CLPP02**

**CLPP02**

**SUBJECT: DA/2021/1246/1 - CONSTRUCTION OF A PEDESTRIAN BRIDGE OVER KOLOMBO CREEK WITH ASSOCIATED SHARED PATHWAYS, LANDSCAPING, TREE REMOVAL AND CIVIL WORKS - 297 SOUTH CIRCUIT, ORAN PARK**

**FROM:** Manager Statutory Planning

**EDMS #:** 22/359685

DA Number:	2021/1246/1.
Development:	Construction of pedestrian bridge over Kolombo Creek with associated shared pathways, landscaping, tree removal and civil works.
Estimated Cost of Development:	\$187,000
Site Address(es):	297 South Circuit, ORAN PARK PLt: 11 DP: 1213614; 107 O'Keefe Drive, ORAN PARK, LOT: 1353 DP: 1218056
Applicant:	Henry Britten-Jones (Greenfields Development Company No.2 Pty Ltd)
Owner(s):	Camden Council.
Number of Submissions:	No submissions.
Development Standard Contravention(s):	No contraventions.
Classification:	Nominated Integrated.
Recommendation:	Approve with conditions.
Panel Referral Criteria:	Subject site is owned by Camden Council.
Report Prepared By:	Nicholas Clarke – Senior Town Planner.

**PURPOSE OF REPORT**

The purpose of this report is to seek the Camden Local Planning Panel’s (the Panel’s) determination of a development application (DA) for the construction of a pedestrian bridge over Kolombo Creek with associated shared pathways, landscaping, tree removal and civil works at 297 South Circuit, Oran Park.

The Panel is to exercise Council’s consent authority functions for this DA as, pursuant to the Minister for Planning’s Section 9.1 Direction, the land is owned by Camden Council.

**SUMMARY OF RECOMMENDATION**

That the Panel determine DA/2021/1246/1 for construction of a pedestrian bridge over Kolombo Creek with associated shared pathways, landscaping, tree removal and civil works pursuant to Section 4.16 of the *Environmental Planning and Assessment Act 1979* by granting consent subject to the conditions attached to this report.

**EXECUTIVE SUMMARY**

Council is in receipt of a DA for construction of a pedestrian bridge over Kolombo Creek with associated shared pathways, landscaping, tree removal and civil works at 297 South Circuit and 107 O’Keefe Drive, Oran Park.

The DA has been assessed against the *Environmental Planning and Assessment Act 1979*, the *Environmental Planning and Assessment Regulation 2021*, relevant environmental planning instruments, development control plans and policies.

The DA was publicly exhibited for a period of 28 days in accordance with Camden Community Participation Plan 2021. The exhibition period was from 3 August 2021 to 30 August 2021 and no submissions were received.

The proposed development has been assessed against the State Environmental Planning Policy (Sydney Region Growth Centres) 2006, Camden Growth Centres Development Control Plan and Oran Park Precinct Development Control Plan 2007. The development is consistent with these planning policies and controls in that the bridge and path will protect and manage an area of high ecological and aesthetic value.

Based on the assessment, it is recommended that the DA be approved subject to the conditions attached to this report.

**AERIAL PHOTO**

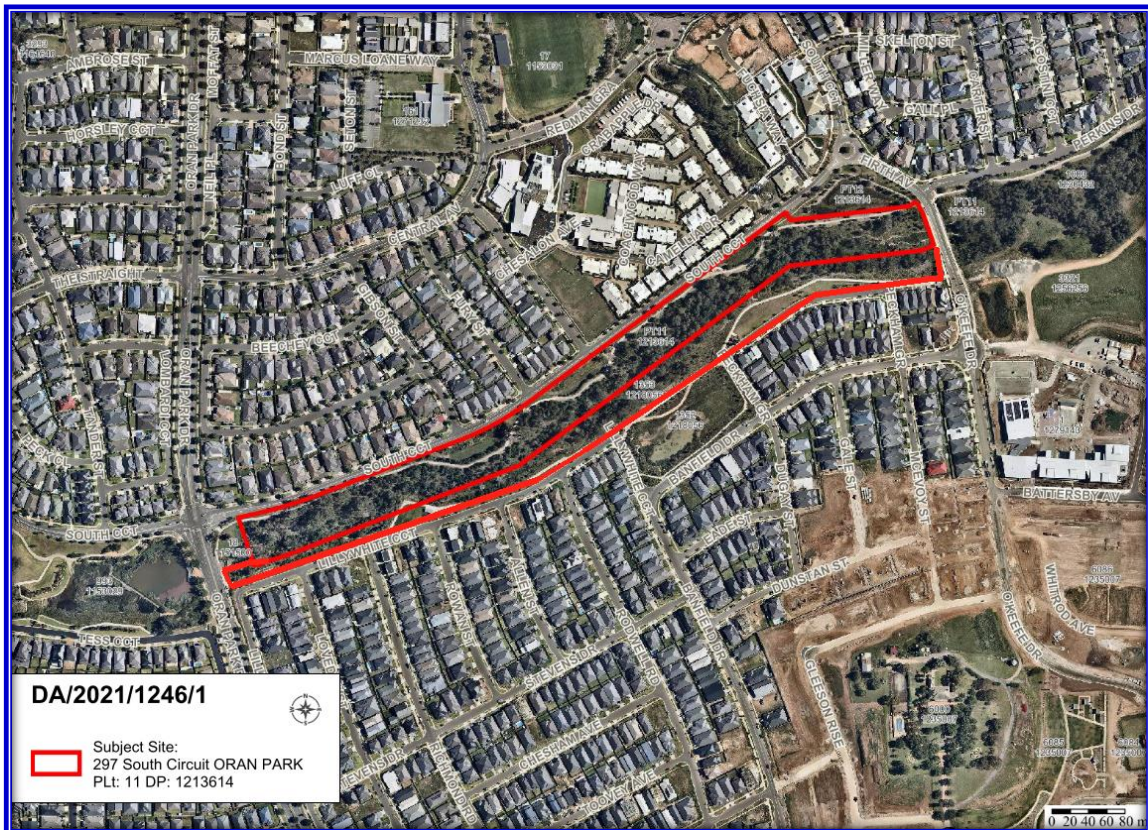


Figure 1: Aerial Photo

**THE SITE**

The subject site is two lots known as 297 South Circuit and 107 O'Keefe Drive, Oran Park and is legally described as PLt: 11 DP: 1213614 and LOT: 1353 DP: 1218056. The combined lots have an overall area of 6.6ha.

The (combined) site is generally rectangular in shape, with an approximate 850m frontage to South Circuit (north), 95m boundary to O'Keefe Drive (east), 870m boundary to Lillywhite Circuit/Peckham Grove (south) and 75m to Oran Park Drive (west).

The Kolombo Creek riparian corridor runs through the two lots in an east-west direction and acts as a natural boundary between the Oran Park and Catherine Field (Part) Precincts within the South West Growth Area.

Surrounding development consists of low density single and two storey residential dwellings and public open space.

The subject site straddles the boundary between the Oran Park and Catherine Field (Part) Precincts within the South West Growth Area. Lot PLt: 11 DP: 1213614 is located in the Oran Park Precinct and LOT: 1353 DP: 1218056 is located in the Catherine Field (Part) Precinct.

**ZONING PLAN**

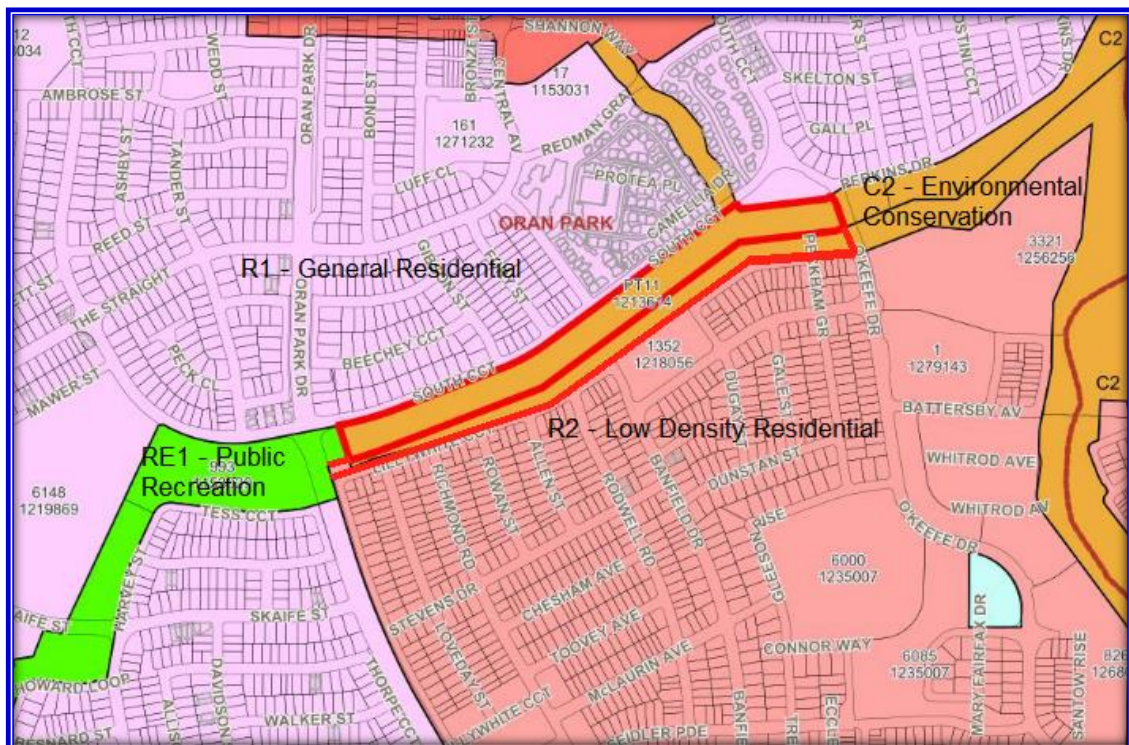


Figure 2: Zoning Map

**AREA MASTER PLAN**

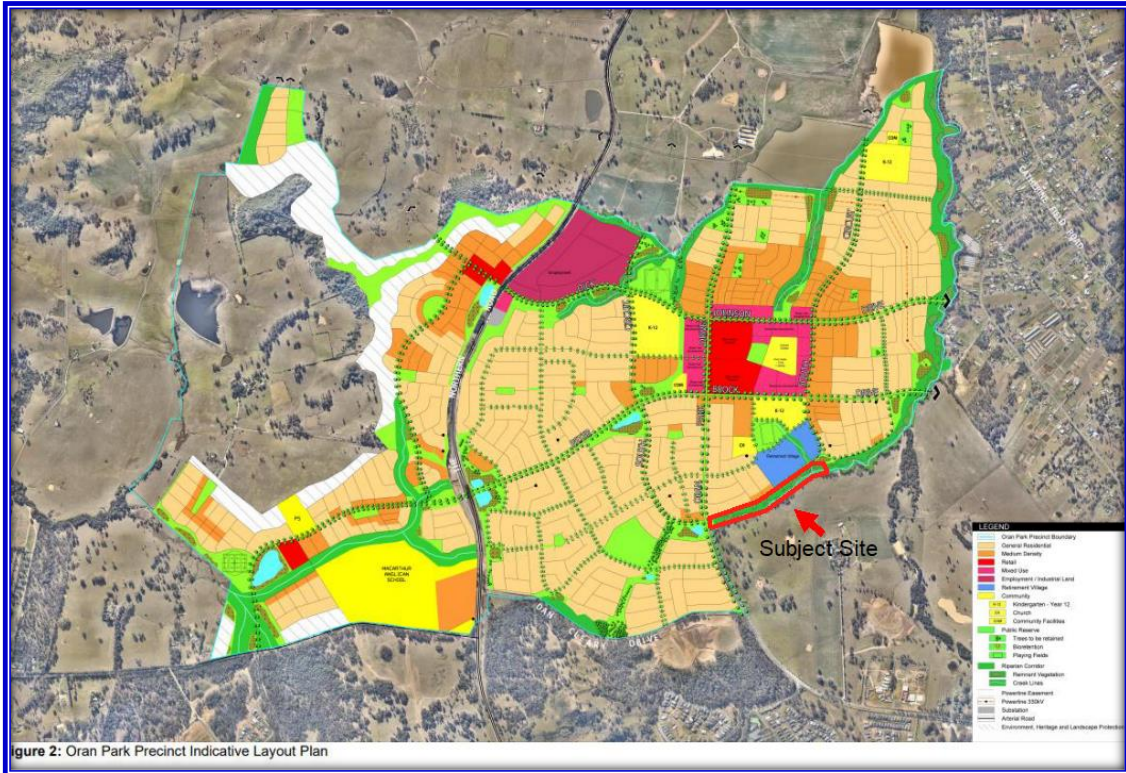


Figure 2: Oran Park Precinct Indicative Layout Plan

Figure 3: Oran Park Indicative Layout Plan



Figure 4: Catherine Field (Part) Precinct Indicative Layout Plan

**HISTORY**

There is no relevant development history for this site.

**THE PROPOSAL**

DA/2021/1246/1 seeks approval for the construction of a pedestrian bridge over Kolombo Creek with associated shared pathways, landscaping, tree removal and civil works.

Specifically, the development involves:

- Construction of a 3m wide x 9m long pedestrian bridge;
- Construction of 2.5m wide shareways to connect the bridge to the existing pedestrian/cycle network; and
- Removal of four trees, cut and fill earthworks, construction of rock walls and landscaping works.

The pedestrian bridge will connect the sharepath network either side of the Kolombo Creek riparian corridor located adjacent South Circuit in Oran Park Town and Peckham Grange in Oran Park South.

The majority of the proposed work is to occur on Lot PLt: 11 DP: 1213614, while only the sharepath extension is to be constructed on LOT: 1353 DP: 1218056.

The estimated cost of the development is \$187,000.



Figure 5: Locality Map



Figure 6: Site Plan  
**ASSESSMENT**

**Environmental Planning and Assessment Act 1979 - Section 4.15(1)**

In determining a DA, the consent authority is to take into consideration such of the following matters as are of relevance to the development the subject of the DA:

**(a)(i) the provisions of any environmental planning instrument**

The environmental planning instruments that apply to the development are:

- State Environmental Planning Policy (Sydney Region Growth Centres) 2006; and
- State Environmental Planning Policy (Biodiversity and Conservation) 2021

State Environmental Planning Policy (Sydney Region Growth Centres) 2006 (Growth SEPP)

The Growth SEPP aims to co-ordinate the release of land for residential, employment and other urban development in the North West Growth Centre, the South West Growth Centre and the Wilton Growth Area.

The subject site straddles the boundary between the Oran Park and Catherine Field (Part) Precincts within the South West Growth Area. Lot PL: 11 DP: 1213614 is located in the Oran Park Precinct and LOT: 1353 DP: 1218056 is located in the Catherine Field (Part) Precinct.



### *Site Zoning*

The site is zoned C2 Environmental Conservation pursuant to Appendix 1 and 9, Clause 2.2 of the Growth SEPP.

### *Land Use/Development Definitions*

The development is characterised as being ancillary to an approved 'recreation area' as defined by the Growth SEPP.

### *Permissibility*

The development is permitted with consent in the C2 Environmental Conservation zone pursuant to the land use table in Appendix 1 and 9 of the Growth SEPP.

### *Planning Controls*

An assessment table in which the development is considered against the Growth SEPP's planning controls is provided as an attachment to this report.

### State Environmental Planning Policy (Biodiversity and Conservation) 2021

The Biodiversity and Conservation SEPP aims to protect the environment of the Hawkesbury-Nepean River system by ensuring impacts of future land uses are considered in a regional context. Council staff are satisfied there will be no detrimental impact on the Hawkesbury-Nepean River system as a result of the development.

These considerations are demonstrated through this report and its accompanying attachments, including recommended conditions to manage erosion, sediment and water pollution control.

**(a)(ii) *the provisions of any proposed instrument that is or has been the subject of public consultation under this Act and that has been notified to the consent authority (unless the Secretary has notified the consent authority that the making of the proposed instrument has been deferred indefinitely or has not been approved)***

Nil

**(a)(iii) *the provisions of any development control plan***

The subject site straddles the boundary between the Oran Park and Catherine Field (Part) Precincts within the South West Growth Area.

Oran Park Precinct Development Control Plan 2007 applies to Lot PLt: 11 DP: 1213614 located in the Oran Park Precinct and Camden Growth Centres Development Control Plan applies to LOT: 1353 DP: 1218056 located in the Catherine Field (Part) Precinct.

### Oran Park Development Control Plan 2007 (Oran Park DCP)

An assessment table in which the development is considered against the Oran Park DCP is provided as an attachment to this report.

Camden Growth Centre Precincts Development Control Plan (Growth DCP)

An assessment table in which the development is considered against the Camden Growth DCP is provided as an attachment to this report.

**(a)(iia) the provisions of any planning agreement that has been entered into under section 7.4, or any draft planning agreement that a developer has offered to enter into under section 7.4**

No relevant planning agreement or draft planning agreement exists or has been proposed as part of this DA.

**(a)(iv) the regulations (to the extent that they prescribe matters for the purposes of this paragraph)**

The *Environmental Planning and Assessment Regulation 2021* prescribes several matters that are addressed in the conditions attached to this report.

**(b) the likely impacts of the development, including environmental impacts on both the natural and built environments, and social and economic impacts in the locality**

As demonstrated by the assessment, the development is unlikely to have any unreasonable adverse impacts on either the natural or built environments, or the social and economic conditions in the locality.

**Heritage conservation**

A number of key Aboriginal archaeological sites and areas were identified within the Oran Park Precinct as part of the specialist background studies undertaken during the Precinct Planning process. These background studies were undertaken in consultation with the Aboriginal community and the former NSW Department of Environment and Conservation (now Office of Environment and Heritage). An AHIP has been issued for a large portion of the Oran Park precinct, including the land contained within this application, and indicates that there is an Aboriginal conservation area (OP-1) located within the South Creek corridor adjacent the open space and water cycle management basin.

Works proposed within the conservation area include general riparian restoration works in accordance with the VMP.

A suitably worded condition has been applied to the consent ensuring all works within OP-1 are conducted in accordance with the AHMP (AECOM 2009) and the AHIP.

**(c) the suitability of the site for the development**

As demonstrated by the above assessment, the site is considered to be suitable for the development.

**(d) any submissions made in accordance with this Act or the regulations**

The DA was publicly exhibited for a period of 28 days in accordance with Camden Community Participation Plan 2021. The exhibition period was from 3 August 2021 to 30 August 2021 and no submissions were received.

**(e) the public interest**

The public interest is served through the detailed assessment of this DA under the *Environmental Planning and Assessment Act 1979*, the *Environmental Planning and Assessment Regulation 2021*, environmental planning instruments, development control plans and policies. Based on the above assessment, the development is consistent with the public interest.

**EXTERNAL REFERRALS**

The external referrals undertaken for this DA are summarised in the following table:

External Referral	Response
Natural Resources Access Regulator	No issue was raised. General terms of approval have been provided and form a recommended condition of consent.

**FINANCIAL IMPLICATIONS**

This matter has no direct financial implications for Council.

**CONCLUSION**

The DA has been assessed in accordance with Section 4.15(1) of the *Environmental Planning and Assessment Act 1979* and all relevant instruments, plans and policies. The DA is recommended for approval subject to the conditions attached to this report.

**RECOMMENDED**

**That the Panel approve DA/2021/1246/1 for construction of a pedestrian bridge over Kolombo Creek with associated shared pathways, landscaping, tree removal and civil works at 297 South Circuit, Oran Park subject to the conditions attached to this report.**

**REASONS FOR DETERMINATION**

1. The development is consistent with the objectives of the applicable environmental planning instruments, being State Environmental Planning Policy (Sydney Region Growth Centres) 2006 and State Environmental Planning Policy (Biodiversity and Conservation) 2021.
2. The development is consistent with the objectives of the Camden Growth Centres Development Control Plan and Oran Park Precinct Development Control Plan 2007.
3. The development is considered to be of an appropriate scale and form for the site and the character of the locality.
4. The development is unlikely to have any unreasonable adverse impacts on the

natural or built environment.

5. In consideration of the aforementioned reasons, the development is a suitable and planned use of the site and its approval is in the public interest.

#### ATTACHMENTS

1. Recommended Conditions
2. Growth SEPP Assessment Table
3. Oran Park Precinct DCP Assessment Table
4. Camden Growth Centres DCP Assessment Table
5. Architectural Plans

## RECOMMENDED CONDITIONS

### 1.0 - General Conditions of Consent

The following conditions of consent are general conditions applying to the development.

- (1) **General Terms of Approval/Requirements of State Authorities** - The general terms of approval/requirements from state authorities shall be complied with prior to, during, and at the completion of the development.

The general terms of approval/requirements are:

1. NSW Natural Resources Access Regulator letter dated 25 November 2021

- (2) **Approved Plans and Documents** – The development shall be carried out in accordance with the following plans and documents, and all recommendations made therein, except where amended by the conditions of this development consent:

Plan Reference/ Drawing No.	Name of Plan	Prepared by	Date
Dwg. 000, Revision 2	Cover Sheet	Calibre Group	2/6/2022
Dwg. 001, Revision 2	General Arrangement Plan	Calibre Group	2/6/2022
Dwg. 002, Revision 2	General Notes and Legends	Calibre Group	2/6/2022
Dwg. 101, Revision 2	Erosion & Site Regrading Plan	Calibre Group	2/6/2022
Dwg. 102, Revision 2	Erosion Control Notes & Legends	Calibre Group	2/6/2022
Dwg. 201, Revision 2	Site Sections	Calibre Group	2/6/2022
Dwg. 301, Revision 2	Engineering Plan	Calibre Group	2/6/2022
Dwg. LT01, Issue B	Title Sheet	JMD design	17/2/2022
Dwg. LH01, Issue B	Path Hierarchy Plan	JMD design	17/2/2022
Dwg. LF01, Issue	Landscape Plan B	JMD design	17/2/2022
Dwg. LF02, Issue B	Bridge Detail Plan	JMD design	17/2/2022
Dwg. LD01 Issue B	Bridge Details 01	JMD design	17/2/2022
Dwg. LD02, Issue B	Bridge Details 02	JMD design	17/2/2022
Dwg. LD03, Issue B	Bridge Details 03	JMD design	17/2/2022
Dwg. LD04, Issue B	Landscape Details	JMD design	17/2/2022

Document Title	Prepared by	Date
Kolomo Creek, Oran Park Pedestrian Bridge Flora and Fauna Assessment Report	EcoLogical Australia	15 May 2019
Project 92287.26, Report on Geotechnical and Salinity Investigation	Douglas Partners	March 2019

- (3) **Graffiti Resistant Materials and Finishes** – Graffiti resistant materials and finishes must be used where possible.
- (4) **National Construction Code – Building Code of Australia (BCA)** - All building work shall be carried out in accordance with the BCA. In this condition, a reference to the

BCA is a reference to that Code as in force on the date the application for the relevant Construction Certificate is made.

- (5) **Engineering Specifications** - The development shall be designed and constructed in accordance with Council's Engineering Specifications.
- (6) **Voluntary Planning Agreement** - The proposed development shall be carried out in accordance with the Oran Park South Planning Agreement executed 7 August 2020.
- (7) **Protection of adjoining Bushland and/or Waterfront Areas** - To limit the potential for damage to the adjoining Bushland areas and/or Waterfront areas, the boundaries to these areas must be fenced prior to the commencement of any earthworks, demolition, excavation or construction works. As well as the fencing prior to any earthworks commencing, other protection measures must be completed in accordance with the standards as specified in AS 4970-2009 Protection of Trees on Development sites.

The fencing must be kept in place until the completion of development and maintenance works and be marked by appropriate signage notifying all site visitors that the subject trees and vegetation areas are protected. The fencing should be a minimum of a 1.8 metres high chain link or welded mesh fencing.

- (8) **Protect Existing Vegetation and Natural Landscape Features** - Approval must be sought from Council prior to the removal, pruning, impact upon or any disturbance of the existing vegetation and natural landscape features, other than any existing vegetation and/or natural landscape feature authorised for removal, pruning, impact upon or disturbance by this development consent.

The following procedures shall be strictly observed:

- a) no additional works or access/parking routes, transecting the protected vegetation shall be undertaken without Council approval; and
- b) pedestrian and vehicular access within and through the protected vegetation shall be restricted to Council approved access routes.

The protection of existing trees and other landscape features, other than any existing trees and natural landscape features authorised for removal, pruning, impact upon or disturbance by this Consent, must be carried out as specified in the Australian Standard AS 4970-2009 Protection of Trees on Development Sites.

All initial procedures for the protection of existing trees and landscape features, as detailed in AS 4970-2009, must be installed prior to the commencement of any earthworks, demolition, excavation or construction works on the Development site.

The works and procedures involved with the protection of existing trees and other landscape features, are to be carried out by suitable qualified and experienced persons or organisations. This work should only be carried out by a fully insured and qualified Arborist.

Suitable qualifications for an Arborist are to be a minimum standard of Australian Qualification Framework (AQF) Level 3 in Arboriculture for the actual carrying out of tree works and AQF Level 5 in Arboriculture for Hazard, Tree Health and Risk Assessments and Reports.

- (9) **Noxious Weeds Management** - Weed dispersion must be minimised and weed infestations must be managed during all stages of the development. Any noxious or environmentally invasive weed infestations that occur during or after works must be fully and continuously suppressed and destroyed by appropriate means. New infestations must be reported to Council.

Pursuant to the *Biosecurity Act 2015* and the *Biosecurity Regulation 2017*, the applicant must at all times ensure that any machinery, vehicles or other equipment entering or leaving the site are clean and free from any noxious weed material to prevent the spread of all weeds to or from the property.

Earth moved containing noxious weed material must be disposed of at an approved waste management facility and be transported in compliance with the *Biosecurity Act 2015* and the *Biosecurity Regulation 2017*.

- (10) **Infrastructure in Road and Footpath Areas** – Infrastructure must not be removed and/or reconstructed without prior written approval from Council. Any costs incurred due to the relocation, restoration or reconstruction of pram ramps, footpath, light poles, kerb inlet pits, service provider pits, street trees or other infrastructure in the street footpath area for the proposed development shall be borne by the applicant, and not Council.

**Note.** The issue of this development consent does not imply concurrence or approval of any required public infrastructure work associated with the development.

- (11) **Timber Components – No Ground Contact** – The design of all open space embellishments must ensure that no timber components have direct contact with the ground.
- (12) **Tree Removal** - The trees as shown in the approved plan (Erosion & Site Regrading Plan Drawing No. 101 8/01/2020) identifying trees to be removed are approved for removal.

The following replacement trees shall be planted in accordance with the landscape plan Finishes Plan and Bridge Sections Drawing No. LF01 February 2019 and the amendment made prior to the issue of a Construction Certificate which has been prepared in accordance with Council's Engineering Specifications:

- Twenty (20) *Eucalyptus tereticornis*

This work should only be carried out by a fully insured and qualified Arborist. Suitable qualifications for an Arborist are to be a minimum standard of Australian Qualification Framework (AQF) Level 3 in Arboriculture for the actual carrying out of tree works and AQF Level 5 in Arboriculture for Hazard, Tree Health and Risk Assessments and Reports.

Where possible all green waste generated from the approved tree work is to be recycled into mulch or composted at a designated facility. All reasonable measures must be taken to protect the remaining vegetation on the site from damage during the approved tree works.

The issuing of this Consent is conditioned upon the planting and maintenance of suitable replacement trees as a means to achieve a "No Nett Loss" approach to vegetation management. The plantings are to be installed on the subject property within six (6) months of the removal/s authorised by this consent.

- (13) **Aboriginal Heritage Management** - All works are to be undertaken in accordance with:
- Aboriginal Heritage Impact Permit No 1100632 issued by the Department of Environment and Climate Change on 26 May 2009 and varied by the Office of Environment and Heritage on 22 June 2016.
  - Aboriginal Heritage Impact Permit C0001014 was issued by the Office of Environment and Heritage on 16 April 2015.

## 2.0 - Prior to Issue of a Construction Certificate

The following conditions of consent shall be complied with prior to the issue of a Construction Certificate.

- (1) **Performance Bond** - The applicant is to lodge a bond with Council to provide security for works undertaken within the existing public domain in accordance with Council's Development Infrastructure Bonds Policy.

**Note.** Fees are payable for the lodgement and refund of the bond.

- (2) **Structural Engineer's Details** - The piers/slabs/footings/structural elements shall be designed and certified by a suitably qualified structural engineer and shall take into consideration the recommendations of any geotechnical report applicable to the site. A statement to that effect shall be provided to the accredited certifier.

- (3) **Civil Engineering Plans** - Civil engineering plans indicating drainage, roads, accessways, earthworks, pavement design, details of line-marking, traffic management, water quality and quantity facilities including stormwater detention and disposal, shall be prepared in accordance with the approved plans and Council's Engineering Design and Construction Specifications. Details demonstrating compliance shall be provided to the certifier with the Construction Certificate application.

A stormwater plan is to be submitted to the certifier prior to the augmentation of the existing drainage system to accommodate drainage from the approved development and to protect other property to the satisfaction of the certifier.

**Note.** Under the *Roads Act 1993*, only the Roads Authority can approve commencement of works within an existing road reserve.

- (4) **Soil, Erosion, Sediment and Water Management** - An erosion and sediment control plan shall be prepared in accordance with 'Managing Urban Stormwater – Soils and Construction ('the blue book'). Details demonstrating compliance shall be provided to the certifier with the Construction Certificate application.
- (5) **Works in Road Reserves** - Where any works are proposed in a public road reservation, a Road Opening Permit shall be obtained from Council in accordance with Section 138 of the *Roads Act 1993*.
- (6) **Detailed Landscape Plan** - A detailed landscape plan must be prepared in accordance with Appendix B of Camden Development Control Plan 2019. Details demonstrating compliance must be provided to the certifier.

The detailed landscape plan must also include:



- Eleven (11) additional *Eucalyptus tereticornis* as marked in red on the stamped plans.
- (7) **Damages Bond** - The applicant is to lodge a bond with Council to ensure any damage to existing public infrastructure is rectified in accordance with Council's Development Infrastructure Bonds Policy.
- Note.** A fee is payable for the lodgement of the bond.
- (8) **Long Service Levy** - In accordance with Section 34 of the *Building and Construction Industry Long Service Payments Act 1986*, the applicant shall pay a long service levy at the prescribed rate to either the Long Service Payments Corporation or Council for any building work that cost \$25,000 or more.
- (9) **Structural Engineer's Certificate** - A certificate must be prepared by a practising structural engineer certifying that the building design is capable of withstanding the effects of water and water pressure due to flooding. Details demonstrating compliance shall be provided to the accredited certifier with the Construction Certificate application.
- (10) **Engineering Certification** - An Engineers Report/Structural Certification is to be provided proving that all items/ structures proposed within the creek line can withstand the force of flood water, debris and buoyancy, up to and including the PMF flood event as in the Engineering Design Guidelines.
- (11) **Special Infrastructure Contribution** - A special infrastructure contribution (SIC) is to be made in accordance with the Environmental Planning and Assessment (Special Infrastructure Contribution - Western Sydney Growth Areas) Determination 2011 (as in force when this consent becomes operative).

Evidence of payment of the SIC shall be provided to Council and the certifier.

Alternatively, the applicant must obtain written confirmation from the Department of Planning and Environment that the SIC is not required to be paid for the approved development.

#### **More information**

A request for assessment by the Department of Planning and Environment of the amount of the special infrastructure contribution that is required under this condition can be made through the NSW Planning Portal (<https://www.planningportal.nsw.gov.au/special-infrastructurecontributions-online-service>). Please refer enquiries to [SIContributions@planning.nsw.gov.au](mailto:SIContributions@planning.nsw.gov.au).

### **3.0 - Prior to Commencement of Works**

The following conditions of consent shall be complied with prior to any works commencing on the development site.

- (1) **Protection of Adjoining Bushland and/or Waterfront Areas** – To limit the potential for damage to the adjoining bushland areas and/or waterfront areas, the boundaries to these areas must be fenced prior to the commencement of any earthworks, demolition, excavation or construction works. As well as the fencing prior to any earthworks commencing, other protection measures must be completed in accordance with the standards as specified in AS 4970.

The fencing must be kept in place until the completion of development and maintenance works and be marked by appropriate signage notifying all site visitors that the subject trees and vegetation areas are protected. The fencing should be a minimum of a 1.8 metres high chain link or welded mesh fencing.

- (2) **Public Liability Insurance** - The owner or contractor shall take out a Public Liability Insurance Policy with a minimum cover of \$20 million in relation to the occupation of, and works within, public property (i.e. kerbs, gutters, footpaths, walkways, reserves, etc) for the full duration of the proposed works. Evidence of this Policy shall be provided to Council and the certifier.
- (3) **Notice of Principal Certifier** - Notice shall be given to Council at least two (2) days prior to subdivision and/or building works commencing in accordance with the *Environmental Planning and Assessment (Development Certification and Fire Safety) Regulation 2021*. The notice shall include:
- a) a description of the work to be carried out;
  - b) the address of the land on which the work is to be carried out;
  - c) the registered number and date of issue of the relevant development consent;
  - d) the name and address of the principal certifier, and of the person by whom the principal certifier was appointed;
  - e) the certifier's registration number, and a statement signed by the certifier consenting to being appointed as principal certifier; and
  - f) a telephone number on which the principal certifier may be contacted for business purposes.
- (4) **Notice of Commencement of Work** - Notice shall be given to Council at least two (2) days prior to subdivision and/or building works commencing in accordance with the *Environmental Planning and Assessment (Development Certification and Fire Safety) Regulation 2021*. The notice shall include:
- a) the name and address of the person by whom the notice is being given;
  - b) a description of the work to be carried out;
  - c) the address of the land on which the work is to be carried out;
  - d) the registered number and date of issue of the relevant development consent and construction certificate;
  - e) a statement signed by or on behalf of the principal certifier (only where no principal certifier is required) to the effect that all conditions of the consent that are required to be satisfied prior to the work commencing have been satisfied; and
  - f) the date on which the work is intended to commence.
- (5) **Construction Certificate Required** - In accordance with the requirements of the *EP&A Act 1979*, works approved by this consent shall not commence until the following has been satisfied:
- a) a Construction Certificate has been issued by a certifier;
  - b) a principal certifier has been appointed by the person having benefit of the development consent;
  - c) if Council is not the principal certifier, Council is notified of the appointed principal certifier at least two (2) days before building work commences;

- d) the person having benefit of the development consent notifies Council of the intention to commence building work at least two (2) days before building work commences; and
  - e) the principal certifier is notified in writing of the name and contractor licence number of the owner/builder intending to carry out the approved works.
- (6) **Sign of Principal Certifier and Contact Details** - A sign shall be erected in a prominent position on the site stating the following:
- a) that unauthorised entry to the work site is prohibited,
  - b) the name of the principal contractor, if any, for the building work and a telephone number on which the principal contractor may be contacted outside working hours, and
  - c) the name, address and telephone number of the principal certifier for the work.

The sign must be maintained while the work is being carried out and removed when the work has been completed.

- (7) **Site is to be Secured** - The site shall be secured and fenced.
- (8) **Sydney Water Approval** – The approved construction certificate plans must also be approved by Sydney Water to determine if sewer, water or stormwater mains or easements will be affected by any part of the development. Go to [www.sydneywater.com/tapin](http://www.sydneywater.com/tapin) to apply.

A copy of the approval receipt from Sydney Water must be submitted to the principal certifier.

- (9) **Soil Erosion and Sediment Control** - Soil erosion and sediment controls must be implemented prior to works commencing on the site in accordance with 'Managing Urban Stormwater – Soils and Construction ('the blue book') and any Sediment and Erosion plans approved with this development consent.
- (10) **Dilapidation Report – Council Property** - A dilapidation report prepared by a suitably qualified person, including a photographic survey of existing public roads, kerbs, footpaths, drainage structures, street trees and any other existing public infrastructure within the immediate area of the site shall be prepared. The report must be submitted to the principal certifier and Council at least 2 days prior to the commencement of works.

Should any public property or the environment sustain damage during the course of and as a result of construction, or if the construction works put Council's assets or the environment at risk, Council may carry out any works necessary to repair the damage or remove the risk. The costs incurred will be deducted from the applicant's damages bond.

- (11) **Traffic Management Plan** - A traffic management plan shall be prepared in accordance with Council's Engineering Specifications and AS 1742.3. The plan must be submitted to the principal certifier.
- (12) **Construction Management Plan** - A construction management plan that includes dust, soil and sediment and traffic management, prepared in accordance with Council's Engineering Design Specification, shall be provided to the principal certifier.

- (13) **Protection of Trees to be Retained** - Protection of trees to be retained shall be in accordance with Council's Engineering Specifications. The area beneath the canopies of the tree(s) to be retained shall be fenced. Tree protection signage is required to be attached to each tree protection zone and displayed in a prominent position.

#### 4.0 - During Works

The following conditions of consent shall be complied with during the construction phase of the development.

- (1) **Work Hours** - All work (including delivery of materials) shall be:
- restricted to between the hours of 7am to 5pm Monday to Saturday (inclusive), and
  - not carried out on Sundays or public holidays, unless approved in writing by Council.
- (2) **Aboriginal Objects Discovered During Works** – If any Aboriginal object (including evidence of habitation or remains) is discovered during the work:
- all excavation or disturbance of the area must stop immediately in that area,
  - Heritage NSW must be advised of the discovery in writing in accordance with Section 89A of the *National Parks and Wildlife Act 1974*, and
  - any requirements of Heritage NSW must be implemented.
- (3) **Site Management** - The following practices are to be implemented during construction:
- a) stockpiles of topsoil, sand, aggregate, spoil or other material shall be kept clear of any drainage path, easement, natural watercourse, kerb or road surface and shall have measures in place to prevent the movement of such material off site;
  - b) builder's operations such as brick cutting, washing tools, concreting and bricklaying shall be confined to the building allotment. All pollutants from these activities shall be contained on site and disposed of in an appropriate manner;
  - c) waste shall not be burnt or buried on site or any other properties, nor shall wind-blown rubbish be allowed to leave the site. All waste shall be disposed of at a licenced waste disposal facility;
  - d) a waste storage area shall be located on the site;
  - e) all building materials, plant, equipment and waste control containers shall be placed on the building site. Building materials, plant and equipment (including water closets), shall not to be placed on public property (footpaths, roadways, public reserves, etc);
  - f) toilet facilities shall be provided at, or in the vicinity of, the work site at the rate of 1 toilet for every 20 persons or part thereof employed at the site. Each toilet shall:
    - i) be a standard flushing toilet connected to a public sewer; or
    - ii) have an on-site effluent disposal system approved under the *Local Government Act 1993*; or
    - iii) be a temporary chemical closet approved under the *Local Government Act 1993*.
- (4) **Traffic Management Plan Implementation** - All traffic management procedures and systems identified in the approved traffic management plan shall be introduced and maintained during construction of the development to ensure safety and to minimise the effect on adjoining pedestrian and traffic systems.

- (5) **Site Signage** - A sign shall be erected at all entrances to the site and be maintained until the development has been completed. The sign shall be constructed of durable materials, be a minimum of 1200mm x 900mm, and read as follows:  
*"WARNING UP TO \$8,000 FINE. It is illegal to allow soil, cement slurry or other building materials to enter, drain or be pumped into the stormwater system. Camden Council (02 4654 7777) – Solution to Pollution."*

The wording shall be a minimum of 120mm high and the remainder a minimum of 60mm high. The warning and fine details shall be in red bold capitals and the remaining words in dark coloured lower case letters on a white background, surrounded by a red border.

- (6) **Vehicles Leaving the Site** - The construction supervisor must ensure that:
- all vehicles transporting material from the site cover such material so as to minimise sediment transfer;
  - the wheels of vehicles leaving the site:
    - do not track soil and other waste material onto any public road adjoining the site; and
    - fully traverse the site's stabilised access point.
- (7) **Fill Compaction** - All fill must be compacted in accordance with Camden Council's current Engineering Design Specifications.
- (8) **Removal of Waste Materials** - Where there is a need to remove any identified materials from the site that contain fill/rubbish/asbestos, the waste material shall be assessed and classified in accordance with the NSW EPA Waste Classification Guidelines 2014 (refer to: [www.epa.nsw.gov.au/wasteregulation/classify-guidelines.htm](http://www.epa.nsw.gov.au/wasteregulation/classify-guidelines.htm))

Once assessed, the materials shall be disposed of to a licensed waste facility suitable for that particular classification of waste. Copies of tipping dockets shall be retained and supplied to Council upon request.

- (9) **Soil, Erosion, Sediment and Water Management – Implementation** - All requirements of the erosion and sediment control plan and/or soil and water management plan shall be maintained at all times during the works and any measures required by the plan shall not be removed until the site has been stabilised.
- (10) **Noise During Work** - Noise levels emitted during works must comply with:
- a) Construction period of 4 weeks and under:  
 The LAeq level measured over a period of not less than 15 minutes when the construction site is in operation must not exceed the background level by more than 20 dB(A).
  - b) Construction period greater than 4 weeks and not exceeding 26 weeks:  
 The LAeq level measured over a period of not less than 15 minutes when the construction site is in operation must not exceed the background level by more than 10 dB(A).
  - c) Construction period greater than 26 weeks:  
 The LAeq level measured over a period of not less than 15 minutes when the construction site is in operation must not exceed the background level by more than 5 dB(A).

Alternatively, noise levels emitted during works shall be restricted to comply with the NSW Environment Protection Authority Interim Construction Noise Guidelines.

- (11) **Location of Stockpiles** - Stockpiles of soil shall not be located on / near any drainage lines or easements, natural watercourses or water bodies, footpath or roadway without first providing suitable protective measures adequate to protect these water bodies. All stockpiles of contaminated materials shall be suitably covered to prevent dust and odour nuisance.
- (12) **Disposal of Stormwater** - Water seeping into any site excavations is not to be pumped into the stormwater system unless it complies with relevant EPA and ANZECC standards for water quality discharge.
- (13) **Delivery Register** - The applicant must maintain a register of deliveries which includes date, time, truck registration number, quantity of fill, origin of fill and type of fill delivered. This register must be made available to Council officers on request and be provided to the Council at the completion of the development.
- (14) **Fill Material (VENM)** - Prior to the importation and/or placement of any fill material on the subject site, a validation report and sampling location plan for such material must be provided to and approved by the principal certifier.

The validation report and associated sampling location plan must:

- a) be prepared by a person with experience in the geotechnical aspects of earthworks; and
- b) be endorsed by a practising engineer with Specific Area of Practice in Subdivisional Geotechnics; and
- c) be prepared in accordance with;
  - Virgin Excavated Natural Material (VENM):
    - i) the Department of Land and Water Conservation publication "Site investigation for Urban Salinity;" and
    - ii) the Department of Environment and Conservation - Contaminated Sites Guidelines "Guidelines for the NSW Site Auditor Scheme (Second Edition) - Soil Investigation Levels for Urban Development Sites in NSW."
- d) confirm that the fill material;
  - i) provides no unacceptable risk to human health and the environment;
  - ii) is free of contaminants;
  - iii) has had salinity characteristics identified in the report, specifically the aggressiveness of salts to concrete and steel (refer Department of Land and Water Conservation publication "Site investigation for Urban Salinity");
  - iv) is suitable for its intended purpose and land use; and
  - v) has been lawfully obtained.

Sampling of VENM for salinity of fill volumes:

- e) less than 6000m<sup>3</sup> - 3 sampling locations; and
- f) greater than 6000m<sup>3</sup> - 3 sampling locations with 1 extra location for each additional 2000m<sup>3</sup> or part thereof.

For e) and f) a minimum of 1 sample from each sampling location must be provided for assessment.

Sampling of VENM for contamination and salinity must be undertaken in accordance with the following table:

Classification of Fill Material	No of Samples Per Volume	Volume of Fill (m <sup>3</sup> )
Virgin Excavated Natural Material	1 (see Note)	1000 or part thereof

**Note** – Where the volume of each fill classification is less than that required above, a minimum of 2 separate samples from different locations must be taken.

- (15) **Offensive Noise, Dust, Odour and Vibration** - All work shall not give rise to offensive noise, dust, odour or vibration as defined in the *Protection of the Environment Operations Act 1997* when measured at the property boundary.

- (16) **Erosion and Sedimentation Control** - Soil erosion and sedimentation controls are required to be maintained for the duration of the works. The controls must be undertaken in accordance with version 4 of the Soils and Construction – Managing Urban Stormwater manual (Blue Book).

Soil erosion and sediment control measures shall only be removed upon completion of the works when all landscaping and disturbed surfaces have been stabilised (for example, with site turfing, paving or re-vegetation).

- (17) **Protection for Existing Trees** – The protection of existing trees (on-site and street trees) must be carried out as specified by AS 4970 Protection of Trees on Development Sites.

- (18) **Unexpected Finds Contingency (General)** - Should any suspect materials (identified by unusual staining, odour, discolouration or inclusions such as building rubble, asbestos, ash material, etc.) be encountered during any stage of works (including earthworks, site preparation or construction works, etc.), such works shall cease immediately until a certified contaminated land consultant has been contacted and conducted a thorough assessment.

In the event that contamination is identified as a result of this assessment and if remediation is required, all works shall cease in the vicinity of the contamination and Council shall be notified immediately.

Where remediation work is required, the applicant will be required to obtain consent for the remediation works.

- (19) **Salinity Management Plan** - All approved development that includes earthworks, imported fill, landscaping, buildings and associated infrastructure must be carried out or constructed in accordance with the management strategies as contained within the approved salinity management plan titled 'Project 92287.26, Geotechnical and Salinity Investigation' prepared by Douglas Partners and dated March 2019'.

- (20) **Relics Discovery During Works** – If any relic surviving from the past is uncovered during the work that could have historical significance (but is not an aboriginal object):
- all work must stop immediately in that area;

- Heritage NSW must be advised of the discovery in writing in accordance with Section 146 of the *Heritage Act 1977*, and
- any requirements of Heritage NSW must be implemented.

### 5.0 - Prior to the issue of an Occupation Certificate

An Occupation Certificate shall be obtained prior to any use or occupation of the development. The following conditions of consent shall be complied with prior to the issue of an Occupation Certificate.

- (1) **Waste Management Plan** - The principal certifier shall ensure that all works have been completed in accordance with the approved waste management plan referred to in this development consent.
- (2) **Completion of Landscape Works** - All landscape works, including the removal of noxious weed species, are to be undertaken in accordance with the approved landscape plan and conditions of this development consent.
- (3) **Locks to Access Gates** - All gates and removable bollards that provide restricted access to Council reserves and other public property shall be fitted with a padlock, which is required to be master keyed to Council's requirements. The supply of the padlocks is at the applicant's cost.
- (4) **Flood level Delineation (Works as Executed Plans)** – The Works as Executed plans must clearly delineate the extent of the flood levels in addition to the flood mapping requirements outlined in Council's Engineering Design Specifications. The plans must clearly delineate the extent/location of the 5% annual exceedance probability (AEP), the 1% AEP, the probable maximum flood (PMF) and the flood planning level (FPL) lines and clearly label them as such. The FPL is defined in Council's Flood Risk Management Policy.

An updated flood map that delineates the extent of the FPL and that is based upon the final completed development layout must be provided to Council for integration into Council's database. The updated flood map must be provided as a separate layer in .dxf or .dwg format and show the 5% AEP, the 1% AEP, the PMF and the FPL levels. This must also include the submission of the digital flood models with result files for the 5% AEP, the 1% AEP, the PMF and the FPL levels for integration into Council's mapping system.

- (5) **Completion of Landscape Works** – All landscape works, including the removal of noxious weed species, are to be undertaken in accordance with the approved landscape plan and conditions of this development consent.

Certification for compliance with AS2303-2018 – Tree Stock for Landscape Use from the grower/supplier of the trees must be provided to the principal certifier.

- (6) **Fill Plan** - A fill plan shall be provided to the principal certifier prior to the issue of any Subdivision certificate. The plan must show (where applicable):
  - a) lot boundaries;
  - b) road/drainage/public reserves;
  - c) street names;
  - d) final fill contours and boundaries; and
  - e) depth in filling in maximum 0.5m Increments



The plan is to be provided electronically in portable document format (.PDF).

- (7) **Value of Works** - Itemised data and value of civil works shall be provided to Council for inclusion in Council's Asset Management System in accordance with Council's Engineering Specifications.
- (8) **Works As Executed Plan** - Works As Executed Plans shall be prepared and provided in accordance with Council's Engineering Specifications.

Digital data must be in AutoCAD .dwg or .dxf format, and the data projection coordinate must be in (GDA94.MGA zone 56).

## 6.0 - Ongoing Use

The following conditions of consent are operational conditions applying to the development.

- (1) **Landscaping Maintenance Establishment Period** - Commencing from the date of practical completion, the applicant will have the responsibility to establish and maintain all hard and soft landscaping elements associated with this consent.

The 12 month maintenance and establishment period includes the applicant's responsibility for the establishment, care and repair of all landscaping elements including all street tree installations, plantings, lawn and hardscape elements including paths, walls, bins, seats, BBQs, shelters, playground equipment and soft fall treatments.

The date of practical completion is taken to mean completion of all civil works, soil preparation and treatment and initial weed control, and completion of all planting, turf installation, street tree installation and mulching.

At the completion of the 12 month landscaping maintenance and establishment period, all hard and soft landscaping elements (including any nature strip and road verge areas, street trees, street tree protective guards and bollards, etc) shall be in an undamaged, safe and functional condition and all plantings have signs of healthy and vigorous growth.

At the completion of the maintenance and establishment period, the landscaping works shall comply with the approved landscape plans and all improvements be in full working order.

- (2) **Maintenance of Landscaping** - Landscaping shall be maintained in accordance with the approved landscape plan

**State Environmental Planning Policy (Sydney Region Growth Centres) 2006**

Clause / Requirement	Provided	Yes /No
<p><b>Part 5, 19 Development on Flood Prone and Major Creeks Land - Additional Heads of Consideration</b></p> <p>Consent authority to consider a number of matters relating to flood behaviour, impacts on the floodplain (including erosion) and whether earthworks will alter drainage patterns or impact the future use of the land.</p>	<p>A satisfactory flooding model has been submitted detailing the impact of the pedestrian bridge on the creek. This report has been reviewed by Council's Flood Engineer and is considered to be in accordance with Council's flooding controls.</p>	Yes
<p><b>Part 6, 23 Consent for clearing native vegetation</b></p> <p>To preserve the amenity of the area through the preservation of trees and other vegetation.</p> <p>A person must not ringbark, cut down, top, lop, remove, injure or wilfully destroy any tree or other vegetation without development consent, or a permit granted by the Council.</p>	<p>Four trees are proposed to be removed. The application was reviewed by Council's Landscape Officer, who supports the proposal subject to additional replacement planting and standard conditions of consent (as recommended).</p> <p>The application was also reviewed by Council's Natural Resources Officer, who raised no issue subject to compliance with the Flora and Fauna Assessment Report and standard conditions of consent.</p>	Yes
<b>Appendix 1 Oran Park and Turner Road Precinct Plan</b>		
<p><b>2.3 Zone objectives and land use table</b></p> <p>The consent authority must have regard to the objectives for development in a zone when determining a development application in respect of land within a zone.</p>	<p>The proposal is for environmental protection works in the C2 Environmental Conservation zone.</p> <p>The proposed works are consistent with the objectives of the zone as they protect, manage and restore areas of high ecological, scientific, cultural or aesthetic values.</p>	Yes
<p><b>5.9 Preservation of trees or vegetation</b></p> <p>To preserve the amenity of the area through the preservation of trees and other vegetation.</p>	<p>Four trees are proposed to be removed.</p> <p>The application was reviewed by Council's Landscape Officer, who supports the proposal subject to additional replacement planting and standard conditions of consent (as recommended).</p> <p>The application was also reviewed by Council's Natural Resources Officer, who raised no issue subject to compliance with the Flora and Fauna Assessment Report and standard conditions of consent (as recommended).</p>	Yes
<p><b>5.10 Heritage conservation</b></p> <p>To conserve archaeological sites, and places of Aboriginal heritage significance.</p>	<p>A number of key Aboriginal archaeological sites and areas were identified within the Oran Park Precinct as part of the specialist background studies undertaken during the Precinct Planning process. These background</p>	Yes

CLPP02

Attachment 2

	<p>studies were undertaken in consultation with the Aboriginal community and the former NSW Department of Environment and Conservation (now Office of Environment and Heritage). An AHIP has been issued for a large portion of the Oran Park precinct, including the land contained within this application, and indicates that there is an Aboriginal conservation area (OP-1) located within the South Creek corridor adjacent the open space and water cycle management basin.</p> <p>Works proposed within the conservation area include general riparian restoration works in accordance with the VMP.</p> <p>A suitably worded condition has been recommended to ensure all works within OP-1 are conducted in accordance with the AHMP (AECOM 2009) and the AHIP.</p>	
<b>Appendix 9 Camden Growth Centres Precinct Plan</b>		
<p><b>2.3 Zone objectives and land use table</b></p> <p>The consent authority must have regard to the objectives for development in a zone when determining a development application in respect of land within a zone.</p>	<p>The proposal is for environmental protection works in the C2 Environmental Conservation zone. The proposed works are consistent with the objectives of the zone as they protect, manage and restore areas of high ecological, scientific, cultural or aesthetic values.</p>	Yes
<p><b>5.9 Preservation of trees or vegetation</b></p> <p>To preserve the amenity of the area through the preservation of trees and other vegetation.</p>	<p>Four trees are proposed to be removed.</p> <p>The application was reviewed by Council's Landscape Officer, who supports the proposal subject to additional replacement planting and standard conditions of consent.</p> <p>The application was also reviewed by Council's Natural Resources Officer, who raised no issue subject to compliance with the Flora and Fauna Assessment Report and standard conditions of consent (as recommended).</p>	Yes
<p><b>5.10 Heritage conservation</b></p> <p>To conserve archaeological sites, and places of Aboriginal heritage significance.</p>	<p>A number of key Aboriginal archaeological sites and areas were identified within the Oran Park Precinct as part of the specialist background studies undertaken during the Precinct Planning process. These background studies were undertaken in consultation with the Aboriginal community and the former NSW Department of Environment and Conservation (now Office of Environment and Heritage). An</p>	Yes

CLPP02

Attachment 2

	<p>AHIP has been issued for a large portion of the Oran Park precinct, including the land contained within this application, and indicates that there is an Aboriginal conservation area (OP-1) located within the South Creek corridor adjacent the open space and water cycle management basin.</p> <p>Works proposed within the conservation area include general riparian restoration works in accordance with the VMP.</p> <p>A suitably worded condition has been applied to the consent ensuring all works within OP-1 are conducted in accordance with the AHMP (AECOM 2009) and the AHIP.</p>	
<p><b>6.2 Native Vegetation Retention Areas</b></p> <p>Development consent required for clearing native vegetation within South Creek riparian corridor (land zoned E2 Environmental Conservation) and Council to consider impacts.</p>	<p>Four trees are proposed to be removed. The application was reviewed by Council's Landscape Officer who supports the proposal subject to additional replacement planting and standard conditions of consent (as recommended).</p> <p>The application was also reviewed by Council's Natural Resources Officer, who raised no issue subject to compliance with the Flora and Fauna Assessment Report and standard conditions of consent (as recommended).</p>	Yes

**Oran Park Precinct Development Control Plan 2007**

<b>Control / Requirement</b>	<b>Provided</b>	<b>Yes/No</b>
<p><b>2.1 Indicative Layout Plan</b></p> <p>Development must be in accordance with the broad level development layout specified by the ILP.</p>	The proposal is consistent with the ILP.	Yes.
<p><b>3.2 Pedestrian and Cycle Network</b></p> <p>Key pedestrian and cycleway routes are to be provided generally in accordance with Figure 18.</p> <p>Pedestrian and cycle routes and facilities in public spaces are to be safe, well lit, clearly defined, functional and accessible to all.</p>	<p>Pedestrian/cycleways within open space system is identified in Figure 18.</p> <p>A 3m wide bridge and connecting 2.5m wide sharepath are proposed to connect the two existing sharepaths on either side of Kolombo Creek.</p>	Yes
<p><b>6.1 Riparian Corridors</b></p> <p>Development in and adjoining riparian corridors shall be consistent with Part B2 of this DCP.</p> <p>In the event of any inconsistency between this DCP and the Waterfront Land Strategy, the Waterfront Land Strategy prevails.</p>	The proposed works are within 40m of the creek, the application was referred to the Department of Primary Industries (Office of Water) who have raised no issue, providing general terms of approval.	Yes
<p><b>6.2 Flooding and Watercycle Management</b></p> <p>Minimise the potential impact of flooding on development.</p> <p>Incorporate best practice stormwater management principles and strategies in development proposals.</p>	<p>The application has been reviewed by both of Council's Development Engineer and Flood Engineer and no issue is raised. Appropriate conditions have been included in the recommendation.</p> <p>The proposed works are within 40m of the creek, the application was referred to the Department of Primary Industries (Office of Water) who have raised no issue, providing general terms of approval.</p>	Yes
<p><b>6.4 Aboriginal and European Heritage</b></p> <p>DAs to identify area of Aboriginal significance and impacts require consent from the NSW Office of Environment and Heritage (OEH).</p>	The only protected Aboriginal heritage object in the vicinity of the subject site is OPR17, which is located between the pre-existing sharepath and South Circuit. OPR17 will not be impacted by the construction of the pedestrian bridge, therefore the proposal will have no impact on the cultural heritage of the area.	Yes
<p><b>6.6 Tree Retention and Biodiversity</b></p> <p>The moderate significance vegetation identified at Figure 27 is to be retained where possible.</p>	Four trees are proposed to be removed. Council's Landscape Officer has assessed the landscaping plans submitted. The landscaping proposed is deemed satisfactory subject to additional plantings within the vicinity of the works.	Yes
<p><b>8.2 Stormwater and Construction Management</b></p> <p>Compliance with Council's policies</p>	The stormwater management has been assessed by Council's Engineers who have raised no objection to the proposal subject to conditions for inclusion in any consent issued.	Yes
<p><b>8.3 Waste Management</b></p>	A condition is recommended to ensure compliance with this part.	Yes

CLPP02

Attachment 3

CLPP02

Attachment 3

A satisfactory Waste Management Plan is required that details waste management for construction and ongoing use.		
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**Camden Growth Centres Development Control Plan.**

<b>Control / Requirement</b>	<b>Provided</b>	<b>Yes/No</b>
<b>2.2 The Indicative Layout Plan (ILP)</b>  Development to be generally in accordance with the ILP	The proposal is consistent with the ILP.	Yes
<b>2.3.1 Flooding</b>  Subdivision layout to ensure 1% AEP flood extent is not exceeded and filling permitted where flood study demonstrates it is required.	The application has been reviewed by both Council's Development Engineer and Flood Engineer and no issue is raised in terms of existing drainage patterns.	Yes
<b>2.3.2 Water cycle management</b>  Demonstrate how stormwater will be managed in accordance with Council's Engineering Specifications.	The application has been reviewed by Council's Development Engineer and Infrastructure Planning Engineer and no issue is raised. Appropriate conditions are recommended.	Yes
<b>2.3.4 Aboriginal and European heritage</b>  DAs to identify area of Aboriginal significance and impacts require consent from the NSW Office of Environment and Heritage (OEH).	The only protected Aboriginal heritage object in the vicinity of the subject site is OPR17, which is located between the pre-existing sharepath and South Circuit. OPR17 will not be impacted by the construction of the pedestrian bridge, therefore the proposal will have no impact on the cultural heritage of the area.	Yes
<b>2.3.5 Native Vegetation and Ecology</b>  To conserve and rehabilitate the remaining native vegetation and trees within the relevant Precinct	Four trees proposed to be removed. The application was reviewed by Council's Landscape Officer, who supports the proposal subject to additional replacement planting and standard conditions of consent (as recommended).  The application was also reviewed by Council's Natural Resources Officer, who raised no issue subject to compliance with the Flora and Fauna Assessment Report and standard conditions of consent (as recommended).	Yes
<b>2.3.6 Bushfire Hazard Management</b>  Development to be consistent with <i>Planning for Bush Fire Protection 2006</i>	The location of the proposed works is within a bushfire-prone area.	Yes
<b>2.6 Earthworks</b>  To ensure that earthworks do not adversely impact local drainage patterns or increase flooding impacts.	The application has been reviewed by both Council's Development Engineer and Flood Engineer and no issue is raised in terms of existing drainage patterns.	Yes
<b>3.3.4 Pedestrian and Cycle Network</b> Key pedestrian and cycleway routes are to be provided generally in accordance with the pedestrian and cycleway network figure.	An off-road shared path is identified within the Environmental Conservation area. A 3m wide bridge and connecting 2.5m wide sharepath are proposed to connect the two existing sharepaths on either side of Kolombo Creek.	Yes
<b>3.4 Construction Environmental Management</b>  To ensure that the construction of structures and works is done in an environmentally responsible manner.	A condition is recommended requiring the preparation of a CEMP prior to the issue of a Construction Certificate.	Yes

CLPP02

Attachment 4

CLPP02

Attachment 4

Erosion and sediment controls must be implemented	Appropriate erosion and sediment control measures have been proposed and are recommended as conditions.	
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# KOLOMBO CREEK, PEDESTRIAN CROSSING PEDESTRIAN CROSSING

## FOR DEVELOPMENT APPLICATION



LOCALITY PLAN  
N.T.S.

LGA CAMDEN COUNCIL  
LOT No.1353 DP 1218056  
LOT No.11 DP 1213614

CLIENT:



DRAWING LIST	
NO. SHEET/TITLE	
000	GENERAL
000	COVER SHEET
001	GENERAL ARRANGEMENT PLAN
002	GENERAL NOTES & LEGENDS
SEDIMENT & EROSION CONTROL	
101	EROSION & SITE REGRADING PLAN
102	EROSION CONTROL NOTES & LEGENDS
SITE REGRADING	
201	SITE SECTIONS
ENGINEERING PLAN	
301	ENGINEERING PLAN

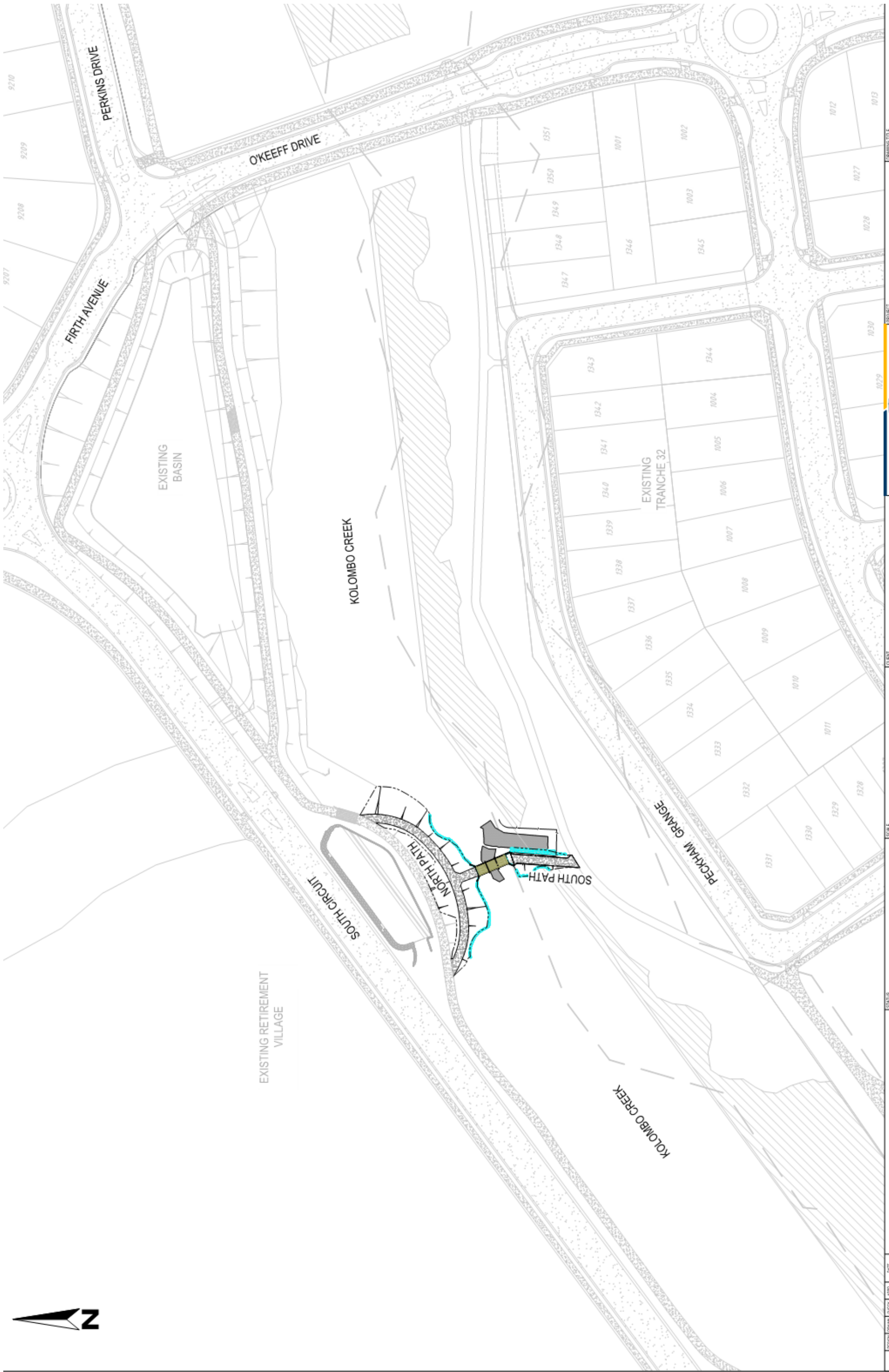
KOLOMBO CREEK, PEDESTRIAN CROSSING  
PEDESTRIAN CROSSING

PROJECT No.	STAGE	REVISION DATE	DRAWING No.	REVISION

Attachment 5

CLPP02

Attachment 5  
CLPP02



REV	NO	DATE	BY	CHECK	DATE
1	SJ	10/01/2022	EP	EP	
2	SJ	10/03/2022	EP	EP	
3	SJ	10/06/2022	EP	EP	

AMENDMENT DETAILS

NO	DATE	DESCRIPTION
1	10/01/2022	INITIAL DESIGN
2	10/03/2022	REVISED DESIGN
3	10/06/2022	FINAL DESIGN

COUNCIL COMMENTS

STATUS: FOR DEVELOPMENT APPLICATION  
 AUTHORIZED FOR ISSUE BY: ENRIQUE FRANCO  
 BE (Res) / ME (Asst) / PE (Eng) / MER  
 SIGN:

SCALE: 1:1000 (A1)  
 SCALE: 1:1000 (A2)

CLIENT: GREENFIELDS Development Company

PROJECT: KOLOMBO CREEK PEDESTRIAN CROSSING ROAD & DRAINAGE DESIGN

DRAWING TITLE: GENERAL ARRANGEMENT PLAN

PROJECT NO: 1014  
 DRAWING NO: 1014  
 SHEET NO: 2

**GENERAL NOTES**

- GENERAL**
- G1. ALL WORK TO BE CARRIED OUT IN ACCORDANCE WITH CAMDEN CITY COUNCIL ENGINEERING DESIGN AND ENGINEERING CONSTRUCTION SPECIFICATIONS AND TO THE REQUIREMENTS OF THE CERTIFYING AUTHORITY.
- G2. INSPECTIONS BY CERTIFYING AUTHORITY ARE REQUIRED AT THE FOLLOWING STAGES AND THE WORKS APPROVED PRIOR TO CONTINUANCE OF ANY FUTURE WORK:
- (A) FOLLOWING INSTALLATION OF EROSION AND SEDIMENT CONTROL STRUCTURES/MEASURES.
  - (B) PRIOR TO BACKFILLING PIPELINES, SUBSOIL DRAINS AND DAMS.
  - (C) PRIOR TO CASTING OF PITS AND OTHER CONCRETE STRUCTURES, INCLUDING KERB AND GUTTER BUT FOLLOWING PLACEMENT OF FOOTINGS, FORMWORK, AND REINFORCEMENT.
  - (D) PRIOR TO PLACEMENT OF SUB BASE AND ALL SUBSEQUENT PAVEMENT LAYERS, A PROOF ROLLER TEST OF EACH PAVEMENT LAYER IS REQUIRED.
  - (E) FORMWORKS PRIOR TO POURING CONCRETE IN PARKING AREA FOR FOOTPATH CROSSING AND OTHER ASSOCIATED WORK.
  - (F) PRIOR TO BACKFILLING PUBLIC UTILITY CROSSINGS IN ROAD RESERVES.
  - (G) FINAL INSPECTIONS AFTER ALL WORKS ARE COMPLETED AND WORKS AS EXECUTED PLANS HAVE BEEN SUBMITTED TO COUNCIL.
- G3. NO TREES ARE TO BE REMOVED UNLESS APPROVAL IS GRANTED BY COUNCIL'S LANDSCAPE COMPLIANCE OFFICER OR AS AUTHORISED BY DEVELOPMENT CONSENT.
- G4. MAKE SMOOTH JUNCTIONS WITH EXISTING WORKS.
- G5. NO WORK IS TO BE CARRIED OUT ON COUNCIL PROPERTY OR ADJOINING PROPERTIES WITHOUT THE WRITTEN PERMISSION FROM THE OWNERS.
- G6. VEHICULAR ACCESS AND ALL UTILITIES/SERVICES ARE TO BE MAINTAINED AT ALL TIMES TO ADJOINING PROPERTIES AFFECTED BY CONSTRUCTION.
- G7. ALL RUBBISH, BUILDINGS, SHEDS AND FENCES TO BE REMOVED TO SATISFACTION OF COUNCIL'S ENGINEER.
- G8. COUNCIL ENGINEERS HAVE DISCRETION TO VARY, AS CONSIDERED NECESSARY, THE ENGINEERING REQUIREMENTS IN RESPECT OF A PARTICULAR SUBDIVISION OR DEVELOPMENT HAVING REGARD TO THE SITE CONTEXT.
- EARTHWORKS**
- E1. EARTHWORKS ARE TO BE CARRIED OUT TO THE SATISFACTION OF THE COUNCIL. UNSUITABLE MATERIALS ARE TO BE REMOVED FROM ROADS AND LOTS PRIOR TO FILLING. THE CONTRACTOR IS TO ARRANGE AND MAKE AVAILABLE COMPACTION TESTING RESULTS FOR ALL AREAS THAT CONTAIN FILL IN EXCESS OF 200mm.
- E2. COMPACTION OF EARTHWORKS SHALL CONTINUE UNTIL A DRY DENSITY RATIO OF 95% FOR SITE FILLING AND 100% FOR ROAD PAVEMENT SUBGRADES HAS BEEN ACHIEVED IN ACCORDANCE WITH TEST METHOD AS1289.5.1 OR AS 1289.5.1.1. THE CONTROL TESTING OF EARTHWORKS SHALL BE IN ACCORDANCE WITH THE GUIDELINES IN AS3708 'GUIDELINES ON EARTHWORKS FOR COMMERCIAL AND RESIDENTIAL DEVELOPMENTS' WHERE IT IS PROPOSED TO USE TEST METHOD AS1289.5.1 TO DETERMINE THE FIELD DENSITY. A SAND REPLACEMENT METHOD SHALL BE USED TO CONFIRM THE RESULTS.
- E3. THE SUITABLE QUALIFIED GEOTECHNICAL ENGINEER SHALL HAVE A LEVEL 1 RESPONSIBILITY FOR ALL FILLING AS DEFINED IN APPENDIX B AS3708 'GUIDELINES ON EARTHWORKS FOR COMMERCIAL AND RESIDENTIAL DEVELOPMENTS', AND AT THE END OF THE WORKS SHALL CONFIRM THE EARTHWORKS COMPLY WITH THE REQUIREMENTS OF THE SPECIFICATION AND DRAWINGS BY WRITTEN NOTIFICATION.
- E4. IN AREAS TO BE FILLED WHERE THE SLOPE OF THE NATURAL SURFACE EXCEEDS 1(V):4(H), BENCHES ARE TO BE CUT TO PREVENT SLIPPING OF THE PLACED FILL MATERIAL AS REQUIRED BY THE COUNCIL.
- E5. ALL BATTERS ARE TO BE SCARIFIED TO A DEPTH OF 50mm TO ASSIST WITH ADHESION OF TOP SOIL TO BATTER FACE.
- E6. PROVIDE MINIMUM 150mm AND MAXIMUM 300mm TOPSOIL ON FOOTPATHS, FILLED AREAS AND ALL OTHER AREAS DISTURBED DURING CONSTRUCTION. TOPSOILED AREAS TO BE STABILISED WITH APPROVED VEGETATION A MAXIMUM OF 14 DAYS AFTER TOPSOILING AND ARE TO BE WATERED TO ENSURE GERMINATION.
- E7. THE CONTRACTOR SHALL CONTROL, SEDIMENTATION, EROSION AND POLLUTION DURING CONSTRUCTION IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT EDITION OF 'MANAGING URBAN STORMWATER: SOILS AND CONSTRUCTION' PRODUCED BY LANDCOM.
- E8. A MINIMUM 1m WIDE CONTINUOUS STRIP OF COUGH GRASS SHALL BE PLACED BEHIND THE BACK OF ALL KERBS & OTHER CONCRETE STRUCTURES IMMEDIATELY AFTER THE COMPLETION OF THE FOOTPATH GRADING OR OTHER ELEMENTS AS APPLICABLE, AND SHALL BE MAINTAINED AND REPLACED AS REQUIRED DURING THE CONSTRUCTION MAINTENANCE PERIOD.
- CALIBRE CONSULTING GENERAL**
- C61. SURVEY SOURCED FROM:  
-SURVEY SOURCED FROM JOHN M. DALY & ASSOCIATES DATED 17 NOVEMBER 2016
- C62. CONTRACTOR IS TO ENSURE THAT ALL WORKS ASSOCIATED WITH PROPERTY BOUNDARIES ARE TO BE SET OUT OR VERIFIED BY A REGISTERED SURVEYOR
- C63. PIPES UP TO 7500 SHALL BE CONSTRUCTED WITH SPIGOT AND SOCKET RUBBER RING JOINTS AND BE OF FIBRE REINFORCED CONCRETE WHICH SHALL CONFORM RESPECTIVELY TO THE REQUIREMENTS OF AS 4139 AND AS 4658. WHERE FIBRE REINFORCED CONCRETE PIPES ARE TO BE USED, A PROPRIETARY COLLAR IS TO BE APPLIED OVER THE PIPE JOINTS.
- C64. PIPES GREATER THAN 7500 ARE TO BE CONSTRUCTED FROM SULPHATE RESISTANT CEMENT.

DESCRIPTION	PROPOSED	EXISTING	FUTURE
STORMWATER PIPELINE			
STORMWATER DRAINAGE PITS			
DRAINAGE LINE No. 3			
DRAINAGE PIT No. 10			
CONCRETE HEADWALL			
CULVERT CROSSING			
SUBSOIL DRAIN			
150mm KERB AND GUTTER			
ROLL KERB AND GUTTER			
KERB ONLY			
EDGE STRIP			
MOUNTABLE KERB			
DISH CROSSING			
VEHICULAR CROSSING			
PEDESTRIAN RAMP			
EDGE OF BITUMEN			
ROAD PAVEMENT			
BATTERS			
CONCRETE PATHWAY			
CONTOURS			
SITE REGRADING AREA			
SERVICE LINES SEWER, GAS, WATER, ELECTRICITY, RECYCLED WATER COMMUNICATION LINES TELSTRA, FIBRE OPTIC, NBN			
OVER HEAD LINES AND POLES			
SERVICE PITS TELECOM PIT, ACCESS CHAMBER, HYDRANT, STOP VALVE, AIR VALVE			
LIMIT OF ROAD CONSTRUCTION			
STAGE BOUNDARY			
FENCE POST AND RAIL FENCE SECURITY FENCE			
LOT NUMBERS			
TREES TO REMAIN WITHIN SITE TREES TO BE REMOVED WITHIN SITE			
RETAINING WALL			
ROCK WALL			
ROOF WATER OUTLET TO KERB			
ROOF WATER OUTLET TO BACK OF PIT			



PROJECT: KOLOMBO CREEK  
PEDESTRIAN CROSSING  
ROAD & DRAINAGE DESIGN

DESIGNER: ALL DIMENSIONS TO BE CHECKED ON SITE BY CONTRACTOR  
DRAWING NO: 10.000041  
NOT FOR CONSTRUCTION UNLESS STAMPED BY

FOR DEVELOPMENT APPLICATION

APPROVED FOR USE BY: ENRIQUE FRANCO  
BE (Hons) MEng, CPEng, MNR

REV	DATE	BY	CHKD	APPD	DATE
1	10/01/2020	EF	EF		
2	10/03/2022	EF	EF		
3	10/09/2022	EF	EF		

AMENDMENT DETAILS

SCALE: CLIENT

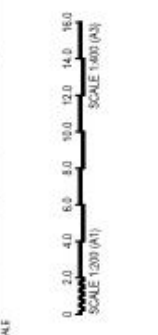
GENERAL NOTES & LEGENDS



DRAWING TITLE  
**EROSION & SITE REGRADING PLAN**

PROJECT  
**KOLOMBO CREEK PEDESTRAIN CROSSING ROAD & DRAINAGE DESIGN**

DESIGNED BY: [Name]  
DRAWING NO: 1/14  
DATE: [Date]



STATUS  
**FOR DEVELOPMENT APPLICATION**

BY: ENRIQUE FRANCO  
BE (Hons) MEng, CP Eng, MSc

NO.	REV.	DATE	BY	CHECKED	APPROVED	REVISIONS
1	1	10/01/2022	EF	EF		
2	1	10/03/2022	EF	EF		
3	1	10/03/2022	EF	EF		

### SEDIMENT & EROSION CONTROL NOTES

- THE CONTRACTOR SHALL IMPLEMENT ALL SOIL EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO THE COMMENCEMENT OF ANY WORKS BEING CARRIED OUT. ALL SOIL AND EROSION MEASURES SHALL BE MAINTAINED AND KEPT IN PLACE FOR THE FULL DURATION OF THE WORKS AND SHALL ONLY BE REMOVED AT FINAL STABILISATION OF THE WORKS. WHERE IT IS NECESSARY TO UNDERTAKE STRIPPING IN ORDER TO CONSTRUCT A SEDIMENT CONTROL DEVICE ONLY SUFFICIENT GROUND SHALL BE STRIPPED TO ALLOW CONSTRUCTION.
- ALL SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE CONSTRUCTED & MAINTAINED AS INDICATED ON THESE DRAWINGS LOCATION AND EXTENT OF SOIL & WATER MANAGEMENT DEVICES IS DIAGRAMMATIC ONLY AND THE ACTUAL REQUIREMENTS SHALL BE CONFIRMED ON SITE PRIOR TO COMMENCEMENT.
- CONFORMITY WITH THIS PLAN SHALL IN NO WAY REDUCE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AGAINST WATER DAMAGE DURING THE COURSE OF THE CONTRACT. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT ANY NECESSARY CONTROL IS IN PLACE EVEN THOUGH SUCH CONTROL MAY NOT BE SHOWN ON THE PLAN.
- THE CONTRACTOR SHALL INFORM ALL SUBCONTRACTORS & ALL EMPLOYEES OF THEIR RESPONSIBILITIES IN MINIMISING THE POTENTIAL FOR SOIL EROSION & POLLUTION TO DOWNSTREAM AREAS
- APART FROM SEDIMENT BASINS, THE CONTRACTOR SHALL REGULARLY MAINTAIN SEDIMENT AND EROSION CONTROL STRUCTURES & DESILT SUCH STRUCTURES PRIOR TO THE REDUCTION IN CAPACITY OF 30% DUE TO ACCUMULATED SEDIMENT. THE SEDIMENT SHALL BE DISPOSED OF ON SITE IN A MANNER APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL TEMPORARILY REHABILITATE WITHIN TEN (10) DAYS ANY DISTURBED AREAS PROVIDING A MINIMUM 60% COVER. FINAL REHABILITATION IS TO BE PROVIDED WITHIN A FURTHER 60 DAYS WITH A MINIMUM 70% COVER.
- THE CONTRACTOR SHALL PROVIDE WATERING OF THE VEGETATED BATTERS FOR MAINTENANCE PERIOD. PLANT, MACHINERY AND VEHICLES SHALL NOT BE DRIVEN OVER GRASSSED AREAS UNLESS AN APPROVED HAULAGE ROUTE.
- ALL DRAINAGE WORKS SHALL BE CONSTRUCTED AND STABILISED AS QUICKLY AS POSSIBLE TO MINIMISE RISK OF EROSION.
- SITE ACCESS SHALL BE RESTRICTED TO THE NOMINATED POINTS. THE CONTRACTOR SHALL PROVIDE STABILISED SITE ACCESS.
- DUST AND SITE DISTURBANCE MUST BE KEPT TO A MINIMUM. DURING WINDY WEATHER, LARGE, UNPROTECTED AREAS MUST BE KEPT MOIST (NOT WET) BY SPRINKLING WITH WATER TO REDUCE WIND EROSION. ERECT BARRIER FENCING TO MINIMISE LAND DISTURBANCE BY PREVENTING VEHICULAR AND PEDESTRIAN ACCESS TO AREAS BEING REHABILITATED AND LANDS THAT DO NOT NEED TO BE DISTURBED BY THIS PROJECT.
- STOCKPILE TOPSOILS, SUBSOILS AND OTHER MATERIALS SEPARATELY.
- TOPSOIL SHALL BE STORED IN LOW MOUNDS NO MORE THAN 2 METRES HIGH AND RE-CUSED WITHIN TWO MONTHS TO MAINTAIN ACTIVE POPULATIONS OF BENEFICIAL SOIL MICROBES & SEED.
- PLACE ALL STOCKPILES AT LEAST FIVE METRES FROM AREAS OF LIKELY CONCENTRATED OR HIGH VELOCITY FLOWS, ESPECIALLY EARTH BANKS AND ROADS. IF NECESSARY, EARTH BANKS OR DRAINS WILL BE CONSTRUCTED TO DIVERT LOCALISED RUN-OFF.
- TURN TOPSOIL STOCKPILES OVER TO AERATE THEM AT MONTHLY INTERVALS. ENSURE VEGETATION IS NOT INCORPORATED INTO THE SOIL.

### FOR DEVELOPMENT APPLICATION

SCALE

AUTHORISED FOR ISSUE:

BY: ENRIQUE FRANCO SIGN: *[Signature]*  
BE (Hons) MEng, CPEng, MNER

### SEDIMENT & EROSION CONTROL NOTES & LEGENDS

PROJECT: KOLOMBO CREEK PEDESTRAIN CROSSING ROAD & DRAINAGE DESIGN

DRAWING NO: 4/17

PROJECT NO: 101/000041

DATE: N/A

REVISION: 2

# Attachment 5

## CLPP02

**SD 4-1**

**STOCKPILES**

**CONSTRUCTION NOTES:**

- PLACE STOCKPILES MORE THAN 2 PREFERABLY 50 METRES FROM EXISTING VEGETATION, CONCENTRATED WATER FLOW, ROADS AND HAZARDOUS AREAS.
- WHERE THERE IS SUFFICIENT AREA, TOPSOIL STOCKPILES SHALL BE LESS THAN 2 METRES IN HEIGHT.
- WHERE THEY ARE TO BE IN PLACE FOR MORE THAN 10 DAYS, STABILISE FOLLOWING THE APPROVED ESKP OR SWMP TO REDUCE THE C-FACTOR TO LESS THAN 8.18.
- CONSTRUCT EARTH BANKS STANDARD DRAWING 5-31 ON THE UPSLOPE SIDE TO DIVERT WATER AROUND STOCKPILES AND SEDIMENT FENCES STANDARD DRAWING 4-81 1 TO 2 METRES DOWN SLOPE.

**SD 6-8**

**SEDIMENT FENCE**

**CONSTRUCTION NOTES:**

- CONSTRUCT SEDIMENT FENCES AS CLOSE AS POSSIBLE TO BEING PARALLEL TO THE CONTOURS OF THE SITE, BUT WITH SMALL RETAINS AS SHOWN IN THE DRAWING TO LIMIT THE CATCHMENT AREA OF ANY ONE SECTION. THE CATCHMENT AREA SHOULD BE SMALL ENOUGH TO LIMIT WATER FLOW TO 10 LITRES PER SECOND IN THE DESIGN STORM EVENT, USUALLY THE 10-YEAR EVENT.
- CUT A 150mm DEEP TRENCH ALONG THE UPSLOPE LINE OF THE FENCE FOR THE BOTTOM OF THE TRENCH TO BE ENTICED TO BE INTO THE TRENCH.
- FIX SELF-SUPPORTING GEOTEXTILE TO THE UPSLOPE SIDE OF THE FENCE ENSURING IT GOES TO THE BASE OF THE TRENCH. FIX THE GEOTEXTILE WITH WIRE TIES OR AS RECOMMENDED BY THE MANUFACTURER. ONLY USE GEOTEXTILE SPECIFICALLY PRODUCED FOR SEDIMENT FENCING. THE USE OF SHADE CLOTH FOR THIS PURPOSE IS NOT SATISFACTORY.
- JOIN SECTIONS OF FABRIC AT A SUPPORT POST WITH A 150mm OVERLAP.
- BACKFILL THE TRENCH OVER THE BASE OF THE FABRIC AND COMPACT IT THOROUGHLY OVER THE GEOTEXTILE.

**SD 6-14**

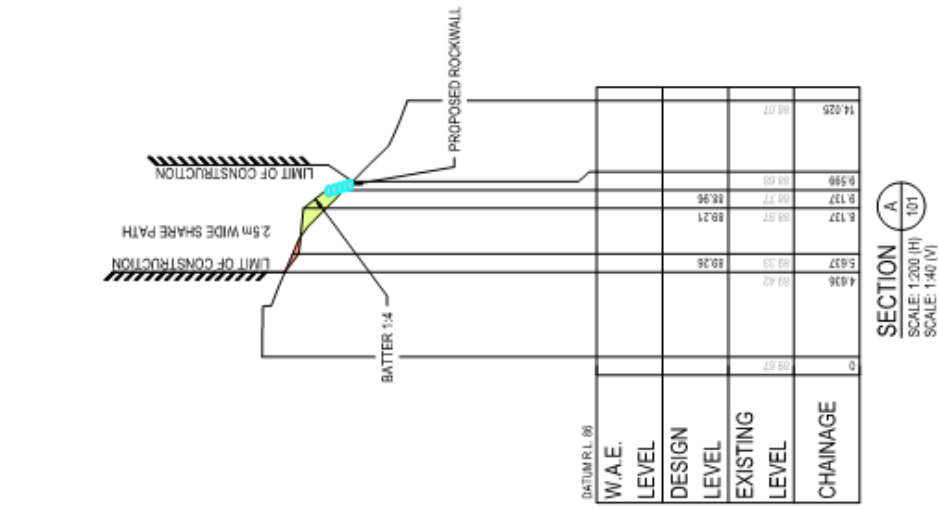
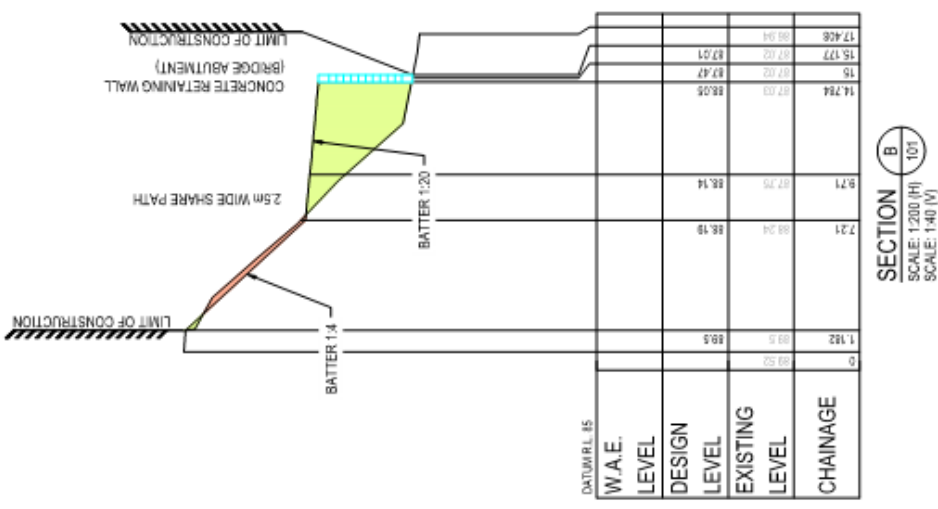
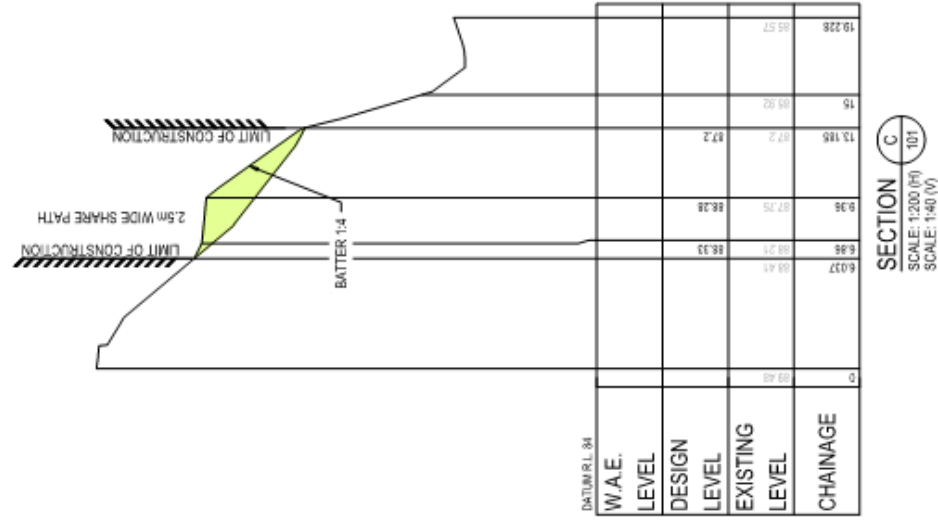
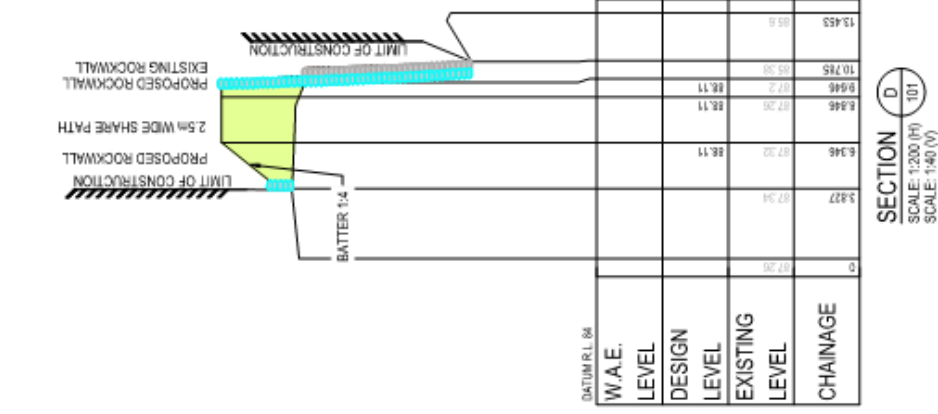
**STABILISED SITE ACCESS**

**CONSTRUCTION NOTES:**

- STRIP THE TOPSOIL LEVEL, THE SITE AND COMPACT THE SUBGRADE.
- COVER THE AREA WITH NEEDLE-PUNCHED GEOTEXTILE.
- CONSTRUCT A 150mm THICK PAD OVER THE GEOTEXTILE USING ROAD BASE OR 30mm AGGREGATE.
- WHERE THE STRUCTURE IS AT LEAST 15 METRES LONG ON TO BUILDING ALIGNMENT AND AT LEAST 20 METRES LONG ON TO ROADWAY ALIGNMENT, CONSTRUCT A HUMP IN THE STABILISED ACCESS TO DIVERT WATER TO THE SEDIMENT FENCE.

**CONSTRUCTION NOTES:**

- AVOID REVERSING THE SOIL PROFILE MATERIALS DURING FILL OPERATIONS - REPLACE DISTURBED SOILS IN THEIR ORIGINAL ORDER.
- ON COMPLETION OF MAJOR EARTHWORKS AND BEFORE ADDING TOPSOIL, LEAVE DISTURBED LANDS WITH A LOOSE SURFACE, ALTERNATELY, DISTURBED AREAS PREVIOUSLY COMPACTED BY CONSTRUCTION WORKS WILL BE RIPPED TO MORE THAN 200mm ALONG THE CONTOUR BEFORE APPLYING TOPSOIL.
- PROVIDING MATERIALS ARE AVAILABLE, SPREAD TOPSOIL TO A MINIMUM DEPTH OF 75mm IN REVEGETATION AREAS ON SLOPES OF 4H:1(V) OR LESS AND TO A DEPTH OF 40 TO 60mm IN REVEGETATION AREAS STEEPER THAN 4:1.
- LEAVE TOPSOIL IN A SCARIFIED OR ROUGH CONDITION ONCE REPLACED TO HELP MOISTURE INFILTRATION AND REDUCE SOIL EROSION.
- ENSURE SOIL IS THOROUGHLY SOAKED TO A DEPTH OF 75mm (RAIN OR IRRIGATION) IMMEDIATELY BEFORE PLANTING.
- HANDLE TOPSOIL ONLY WHEN IT IS MOIST (NOT WET OR DRY) TO AVOID DECLINE OF SOIL STRUCTURE.
- THE CONTRACTOR SHALL MAINTAIN A LOG BOOK DETAILING:
  - RECORDS OF ALL RAINFALL
  - CONDITION OF SOIL AND WATER MANAGEMENT STRUCTURES
  - ANY APPLICATION OF FLOCCULATING AGENTS TO SEDIMENT BASIN
  - VOLUMES OF ALL WATER DISCHARGED FROM SEDIMENT BASINS
  - ANY ADDITIONAL REMEDIAL WORKS REQUIRED.
- THE LOG BOOK SHALL BE MAINTAINED ON A WEEKLY BASIS AND BE MADE AVAILABLE TO ANY AUTHORISED PERSON UPON REQUEST. THE ORIGINAL LOG BOOK SHALL BE ISSUED TO THE PROJECT MANAGER AT THE COMPLETION OF WORKS
- ALL ROAD EMBANKMENTS TO BE STABILISED AS PER LANDSCAPE ARCHITECTS DETAILS.
- A SELF-AUDITING PROGRAM SHOULD BE ESTABLISHED BASED ON A CHECK SHEET DEVELOPED FOR THE SITE. A SITE INSPECTION USING THE CHECK SHEET SHOULD BE MADE BY THE SITE MANAGER AT LEAST WEEKLY IMMEDIATELY BEFORE SITE CLOSURE AND IMMEDIATELY FOLLOWING RAINFALL EVENTS THAT CAUSE RUNOFF.
- UNDER TAKE THE SELF-AUDIT BY:
  - WALKING AROUND THE SITE SYSTEMATICALLY (E.G. CLOCKWISE)
  - RECORDING THE CONDITION OF EVERY BMP EMPLOYED
  - RECORDING MAINTENANCE REQUIREMENTS (IF ANY) FOR EACH BMP
  - RECORDING THE SITE WHERE SEDIMENT IS DISPOSED
  - FORWARDING A SIGNED DUPLICATE OF THE COMPLETED CHECK SHEET TO THE PROJECT MANAGER/DEVELOPER/SITE OPERATOR FOR THEIR INFORMATION
- IN PARTICULAR, INSPECT:
  - LOCATIONS WHERE VEHICLES ENTER AND LEAVE THE SITE
  - ALL INSTALLED EROSION AND SEDIMENT CONTROL MEASURES, ENSURING THEY ARE OPERATING CORRECTLY
  - AREAS THAT MIGHT SHOW WHETHER SEDIMENT OR OTHER POLLUTANTS ARE LEAVING THE SITE OR HAVE POTENTIAL TO DO SO
  - ALL DISCHARGE POINTS, TO ASSESS WHETHER THE EROSION AND SEDIMENT CONTROL MEASURES ARE EFFECTIVE IN PREVENTING IMPACTS TO THE RECEIVING WATERS
- A SITE INSPECTION USING THE CHECK SHEET WILL BE MADE BY THE SITE MANAGER AT LEAST WEEKLY IMMEDIATELY BEFORE SITE CLOSURE AND IMMEDIATELY FOLLOWING RAINFALL EVENTS GREATER THAN 5mm IN 24 HOURS.



REV	DATE	BY	CHKD	APPD
1	08/01/2022	EP	EP	
2	08/02/2022	EP	EP	

**FOR DEVELOPMENT APPLICATION**  
 AUTHORIZED FOR ISSUE:  
 BY: ENRIQUE FRANCO SIKR  
 BE(Hon)/ME(Hon), CP(Eng) MER

**GREENFIELDS**  
 Development Company

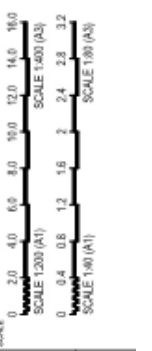
**calbre**

**KOLOMBO CREEK  
 PEDESTRAIN CROSSING  
 ROAD & DRAINAGE DESIGN**

PROJECT: KOLOMBO CREEK PEDESTRAIN CROSSING ROAD & DRAINAGE DESIGN

PROJ. NAME: KOLOMBO CREEK PEDESTRAIN CROSSING ROAD & DRAINAGE DESIGN

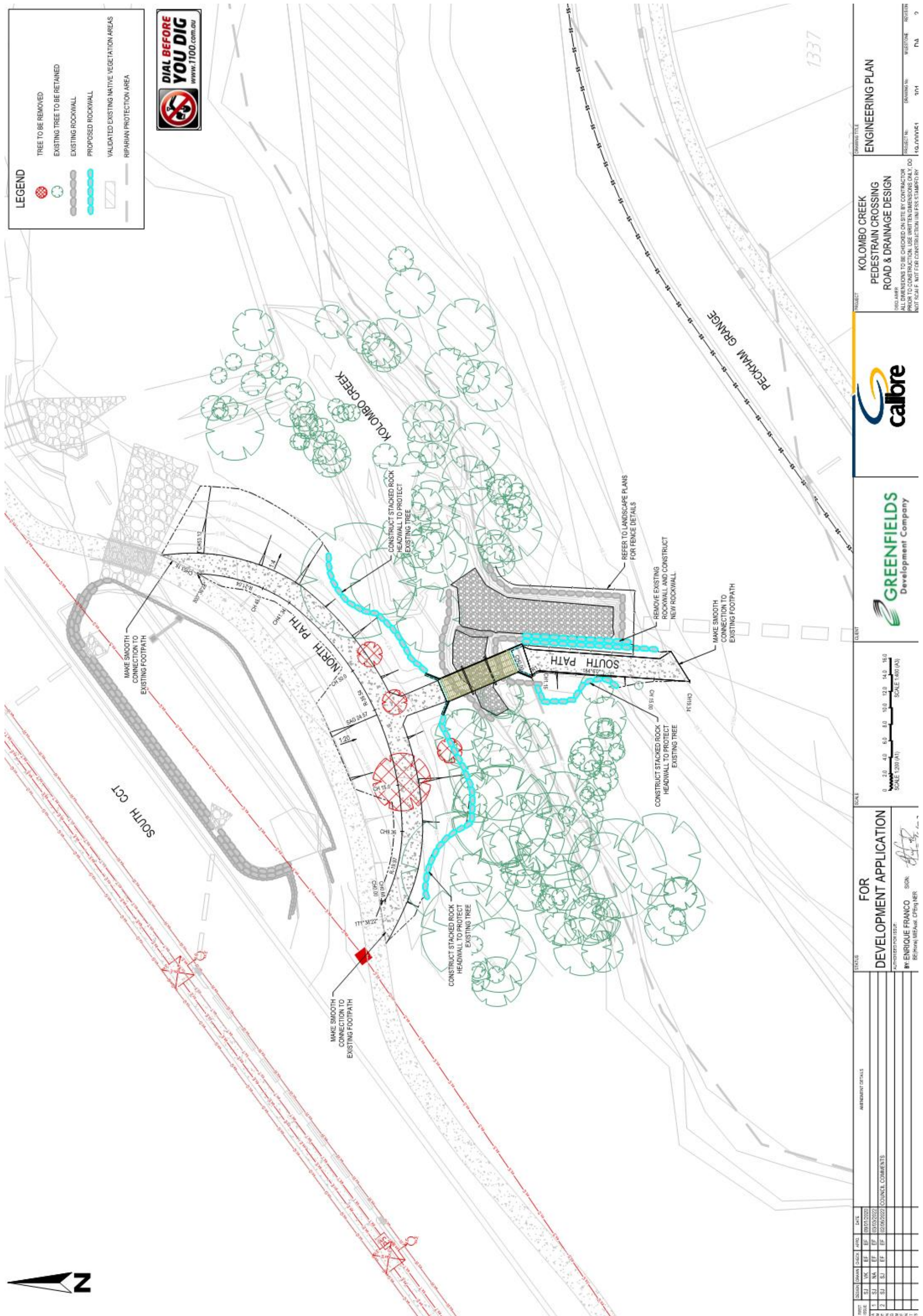
ALL DIMENSIONS TO BE CHECKED ON SITE BY CONTRACTOR PRIOR TO CONSTRUCTION. USE WRITTEN DIMENSIONS ONLY. DO NOT SCALE. NOT FOR CONSTRUCTION. THIS DRAWING IS NOT TO BE USED FOR CONSTRUCTION WITHOUT THE SIGNATURE OF THE DESIGNER.



STATUS: **FOR DEVELOPMENT APPLICATION**

AMENDMENT DETAILS

NO.	REV.	DATE	BY	CHKD	APPD
1	08/01/2022	EP	EP		
2	08/02/2022	EP	EP		



**LEGEND**

- TREE TO BE REMOVED
- EXISTING TREE TO BE RETAINED
- EXISTING ROCKWALL
- PROPOSED ROCKWALL
- VALUATED EXISTING NATIVE VEGETATION AREAS
- RIPARIAN PROTECTION AREA

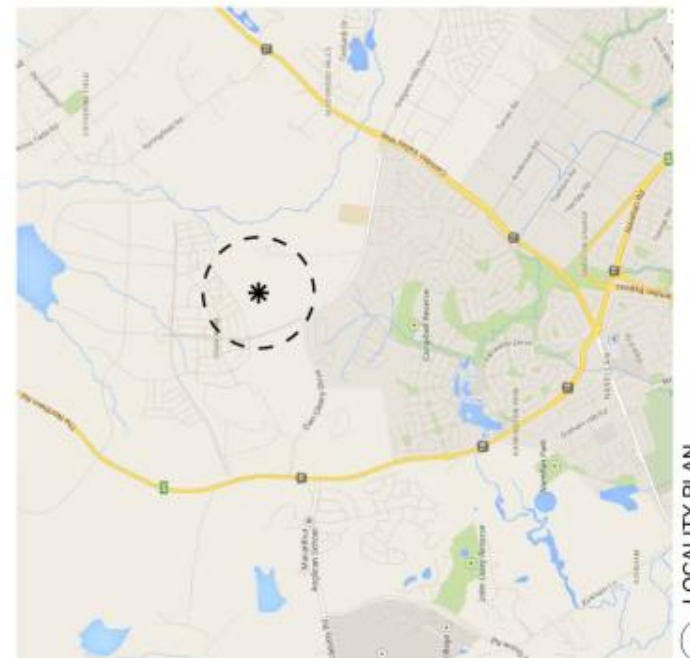
<b>PROJECT</b>	KOLOMBO CREEK PEDESTRIAN CROSSING ROAD & DRAINAGE DESIGN	<b>CLIENT</b>	
<b>DATE</b>	10/07/2020	<b>SCALE</b>	SCALE 1:300 (A1) SCALE 1:400 (A2)
<b>ISSUE</b>	1. S.I. NA EF 03/03/2022 2. S.I. S.I. EF 10/06/2022	<b>FOR DEVELOPMENT APPLICATION</b>	<b>BY: ENRIQUE FRANCO</b> SIGN:
<b>REVISIONS</b>		<b>FOR AMENDMENT DETAILS</b>	<b>BY: (Name) ME/Anal. CPEng/NER</b>
<b>PROJECT NO.</b>	14.000004.1	<b>PROJECT TITLE</b>	ENGINEERING PLAN
<b>DRAWING NO.</b>	204	<b>PROJECT NO.</b>	14.000004.1
<b>DATE</b>	10/07/2020	<b>PROJECT NO.</b>	14.000004.1
<b>SCALE</b>	1:300 (A1)	<b>PROJECT NO.</b>	14.000004.1
<b>SCALE</b>	1:400 (A2)	<b>PROJECT NO.</b>	14.000004.1

# ORAN PARK TOWN SOUTH TRANCHE 31 STAGE 3 KOLOMBO CREEK PEDESTRIAN BRIDGE CROSSING FOR DEVELOPMENT APPLICATION / CONSTRUCTION CERTIFICATE

## DRAWING REGISTER LANDSCAPE PLANS

No.	DRAWING TITLE	SCALE@A1	ISSUE
LT01	TITLE SHEET	AS SHOWN	B
LH01	PATH HIERARCHY PLAN	1:1500	B
LF01	LANDSCAPE PLAN	1:200	B
LD01	BRIDGE DETAIL PLAN	1:50	B
LD02	BRIDGE DETAILS 01	AS SHOWN	B
LD03	BRIDGE DETAILS 02	AS SHOWN	B
LD04	BRIDGE DETAILS 03	AS SHOWN	B
LD04	LANDSCAPE DETAILS	AS SHOWN	B

**NOTES:**  
 - REFER TO CALIBRE CONSULTING FOR ENGINEERING DOCUMENTATION, INCLUDING SEDIMENT AND EROSION CONTROL, CONSTRUCTION AND OPERATIONAL WASTE MANAGEMENT PLAN  
 - ALL WORK TO BE CARRIED OUT IN ACCORDANCE WITH CAMDEN COUNCIL'S ENGINEERING DESIGN AND ENGINEERING CONSTRUCTION SPECIFICATION AND TO THE REQUIREMENTS OF THE CERTIFYING AUTHORITY



1400 Lakeside Street  
 Brighton NSW 2204  
 T: 02 9333 3444 F: 02 9333 3445  
 www.calibreconsulting.com.au

Engineer

Consultant

ESTATE POWER DESIGN PTY LIMITED  
 AN AFFILIATE OF  
 ELECTRICAL INSTALLATION CONSULTANTS  
 142 SPENCER STREET, BLACKTOWN NSW  
 2105  
 TEL: 02 9640 1111  
 EMAIL: info@epdesign.com.au

Client

Key plan

Issue	Date	Description
B	17/03/22	FOR DA/CC
A	14/10/19	FOR DA/CC

Project  
**ORAN PARK SOUTH  
 TRANCHE 31 STAGE 3  
 KOLOMBO CREEK  
 PEDESTRIAN BRIDGE CROSSING**

Title  
**TITLE SHEET**

Scale  
 1:200 @ A1

Date  
 FEBRUARY 2019

Drawing status  
 FOR DA / CC

Drawing number  
 LT01

Designed  
 JZ

Drawn  
 TL

Checked  
 B

Approved  
 By: James Deane  
 Author: JZ  
 Review: JZ  
 Date: 03/03/22

Job N  
 14100

FOR CONSTRUCTION



**ESTATE POWER DESIGN PTY LIMITED**  
 1/100 WINDYBUSH ROAD, WINDYBUSH NSW 2150  
 TEL: (02) 9631 1234 FAX: (02) 9631 1234  
 www.estatepowerdesign.com.au

**GREENFIELDS**  
 Development Company

**Project:**  
 ORAN PARK SOUTH  
 TRANCHE 31 STAGE 3  
 KOLOMBO CREEK  
 PEDESTRIAN BRIDGE CROSSING

**Title:**  
 PATH HIERARCHY PLAN

**Scale:**  
 1:200 @ A1

**Date:**  
 FEBRUARY 2019

**Job N:**  
 14100

**Drawing status:**  
 FOR DA / CC

**Drawing number:**  
 LH01

**Revision:**  
 B

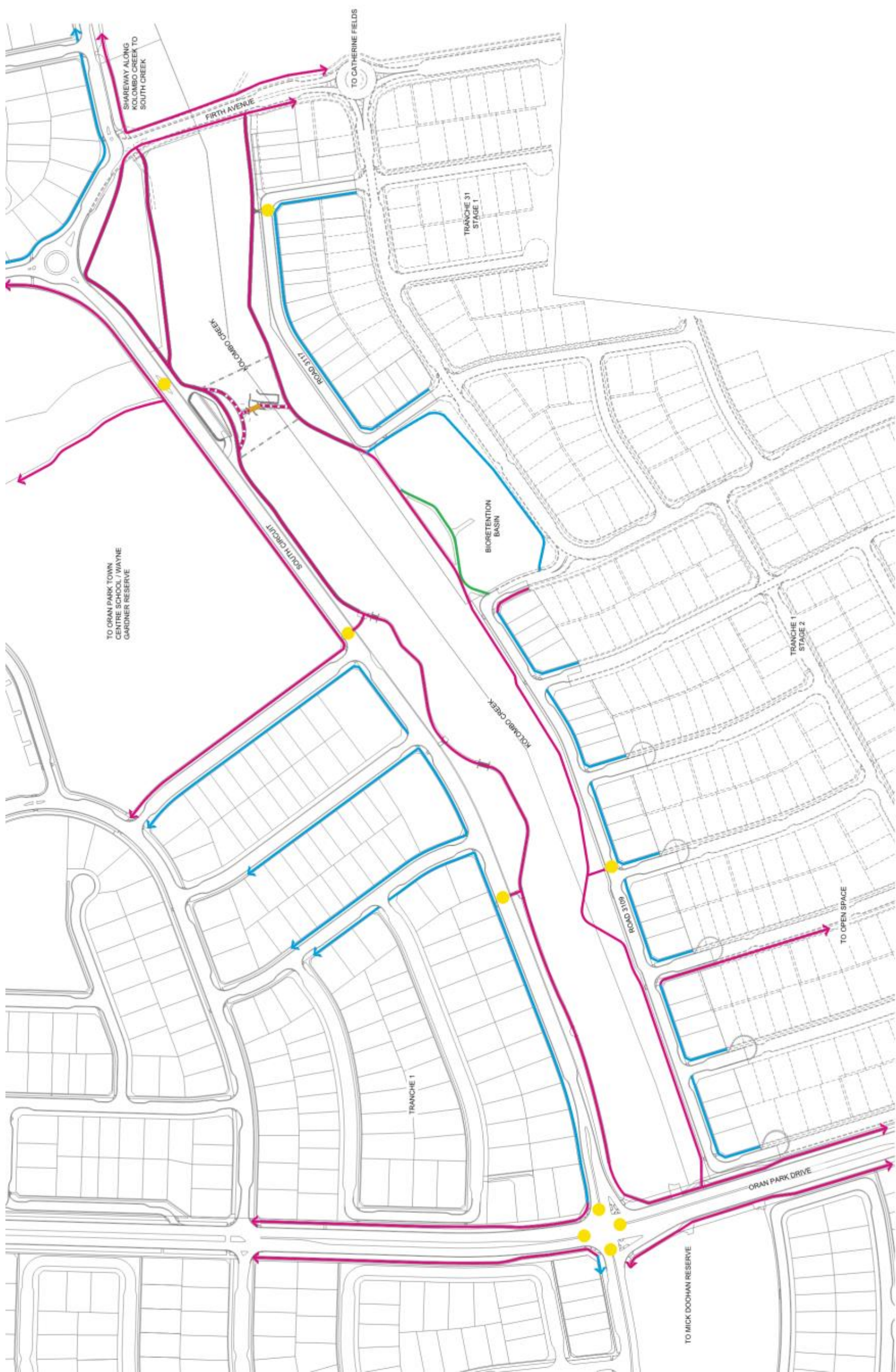
**FOR CONSTRUCTION**

Designed: JLD  
 Drawn: TL  
 Checked: JLD

Approved: James Driscoll  
 M.A.S. Registered Landscape Architect

Signer: *James Driscoll*  
 Date: 03/03/22

Do not scale or alter. Use A3 sized dimensions only. Check & verify all dimensions in the final construction documents.



**LEGEND**

	EXTENT OF SUBMISSION
	2.5m SHAREWAY
	1.5m PATHWAY
	PRAM RAMP ROAD CROSSING
	SERVICE VEHICLE ACCESS
	2.5m SHAREWAY
	3m WIDE BRIDGE CROSSING

**ESTATE POWER DESIGN PTY LIMITED**  
ELECTRICAL RETICULATION CONSULTANTS  
15/150 WILSON ROAD, WILSON PROMENADE  
TEL: (08) 9271 4332 FAX: (08) 9271 2286  
EMAIL: info@estatepowerdesign.com.au

**GREENFIELDS**  
Development Company

Project: **ORAN PARK SOUTH TRANCHE 31 STAGE 3 KOLOMBO CREEK PEDESTRIAN BRIDGE CROSSING**

Title: **FINISHES PLAN AND BRIDGE SECTIONS**

Scale: 1:200 @ A1

Date: FEBRUARY 2019

Drawing status: FOR DA / CC

Drawing number: LFO1

Issue: B

Approved: [Signature] Date: 03.03.22

By: James Deane, FALA - Registered Landscape Architect (L1)

Check: [Signature] Date: 03.03.22

Do not scale drawings. Use legend dimensions only. Check & verify levels and other drawings for full fabrication of components.

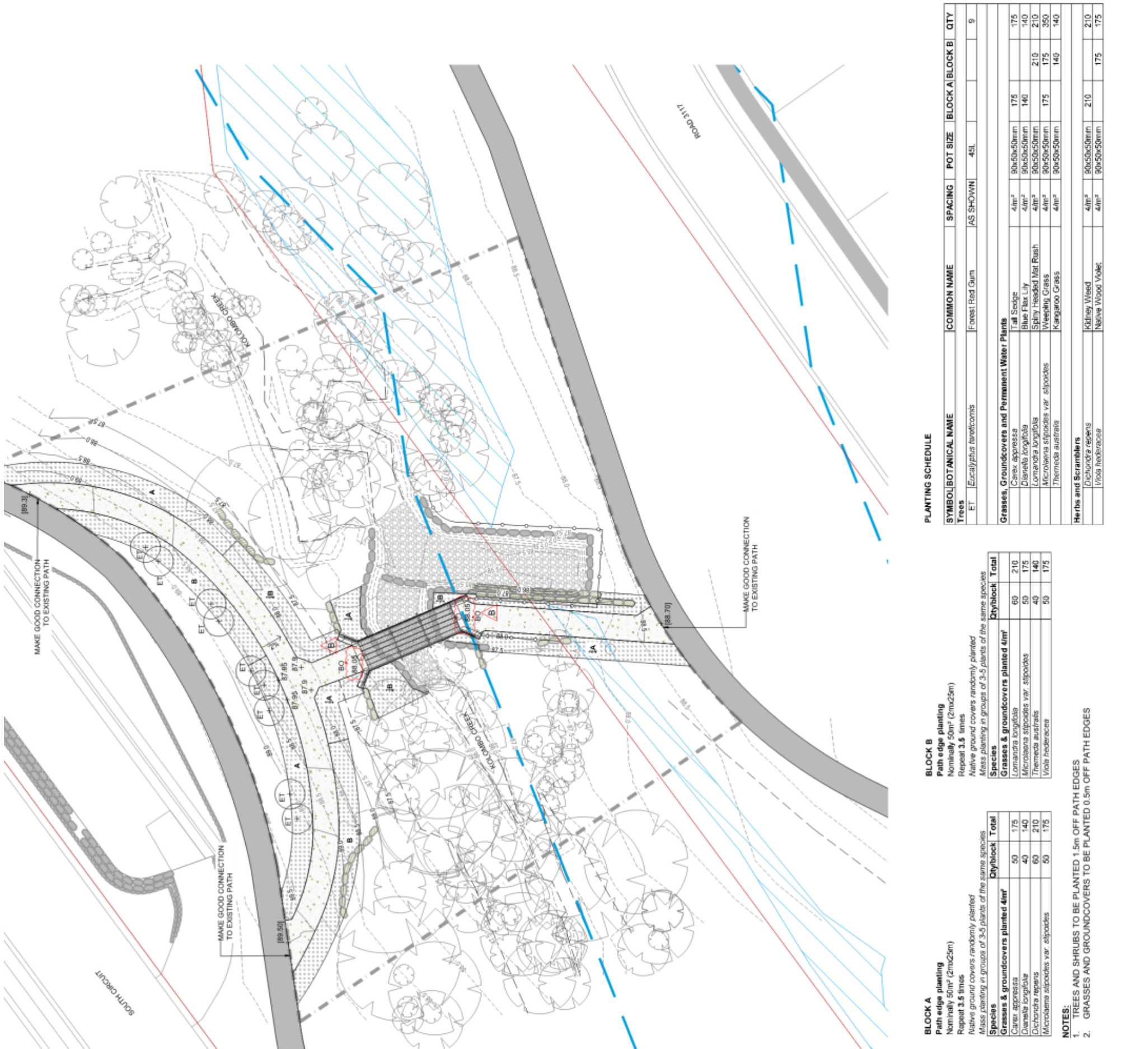
**LEGEND**

**EXISTING**

- EXTENT OF WORKS - TBC
- PROPERTY BOUNDARY
- CONTOURS
- SPOT LEVELS
- TOP SURFACE OF ROCKWALL
- EXTENT OF ROCK PLACEMENT
- 1.5m OR 2.5m CONCRETE PATHWAY
- MONOWALLS FENCE
- TREES TO BE RETAINED
- RIPARIAN PROTECTION AREA
- VALIDATED EXISTING NATIVE VEGETATION AREAS

**PROPOSED**

- CONTOURS
- SPOT LEVELS
- TREES REFER PLANTING SCHEDULE AND DETAILS
- PLANTING BLOCKS TO PLANTING SCHEDULES
- MULCH TO MASS PLANTING AREAS AS SPECIFIED
- 2.5m CONCRETE SHAREWAY AS DETAILED AND SPECIFIED
- 3m WIDE PEDESTRIAN BRIDGE CROSSING WITH BALUSTRADE ABOVE 1.4m 100 YEAR FLOOD LEVEL AS DETAILED AND SPECIFIED
- FIXED BOLLARD AS DETAILED AND SPECIFIED
- STACKED ROCKWALL BY OTHERS REFER ENGINEERS DOCUMENTATION




**PLANTING SCHEDULE**


SYMBOL/BOTANICAL NAME	COMMON NAME	SPACING	POT SIZE	BLOCK A	BLOCK B	QTY
<b>Trees</b>						
ET	Eucalyptus terebinthos	Forest Red Gum	45L			9
<b>Grasses, Groundcovers and Permanent Water Plants</b>						
Carex appressa	Tall Sedge	4m <sup>2</sup>	30x50x50mm	175		175
Clanetia longituba	Blue Flax Lily	4m <sup>2</sup>	30x50x50mm	140		140
Lomandra longituba	Splay Headed Mat Rush	4m <sup>2</sup>	30x50x50mm	210		210
Microseris stipoides var. stipoides	Weeping Grass	4m <sup>2</sup>	30x50x50mm	175		175
Themeda australis	Kangaroo Grass	4m <sup>2</sup>	30x50x50mm	140		140
<b>Herbs and Scramblers</b>						
Dichondra repens	Kidney Weed	4m <sup>2</sup>	30x50x50mm	210		210
Viola hebridica	Native Wood Violet	4m <sup>2</sup>	30x50x50mm	175		175

**NOTES:**

- TREES AND SHRUBS TO BE PLANTED 1.5m OFF PATH EDGES
- GRASSES AND GROUNDCOVERS TO BE PLANTED 0.5m OFF PATH EDGES




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**GREENFIELDS**  
Development Company



Key plan showing site location within a larger development area.



Issue	Date	Description
B	17.02.22	FOR DA/CC / B
A	01.10.19	FOR DA/CC
AD1	19.03.19	FOR DA/CC

**Project:** ORAN PARK SOUTH TRANCHE 31 STAGE 3 KOLOMBO CREEK PEDESTRIAN BRIDGE CROSSING

**Title:** BRIDGE DETAIL PLAN

**Scale:** 1:50 @ A1

**Date:** FEBRUARY 2019

**Job No.:** 1410089

**Drawing status:** FOR DA / CC

**Drawing number:** LF02

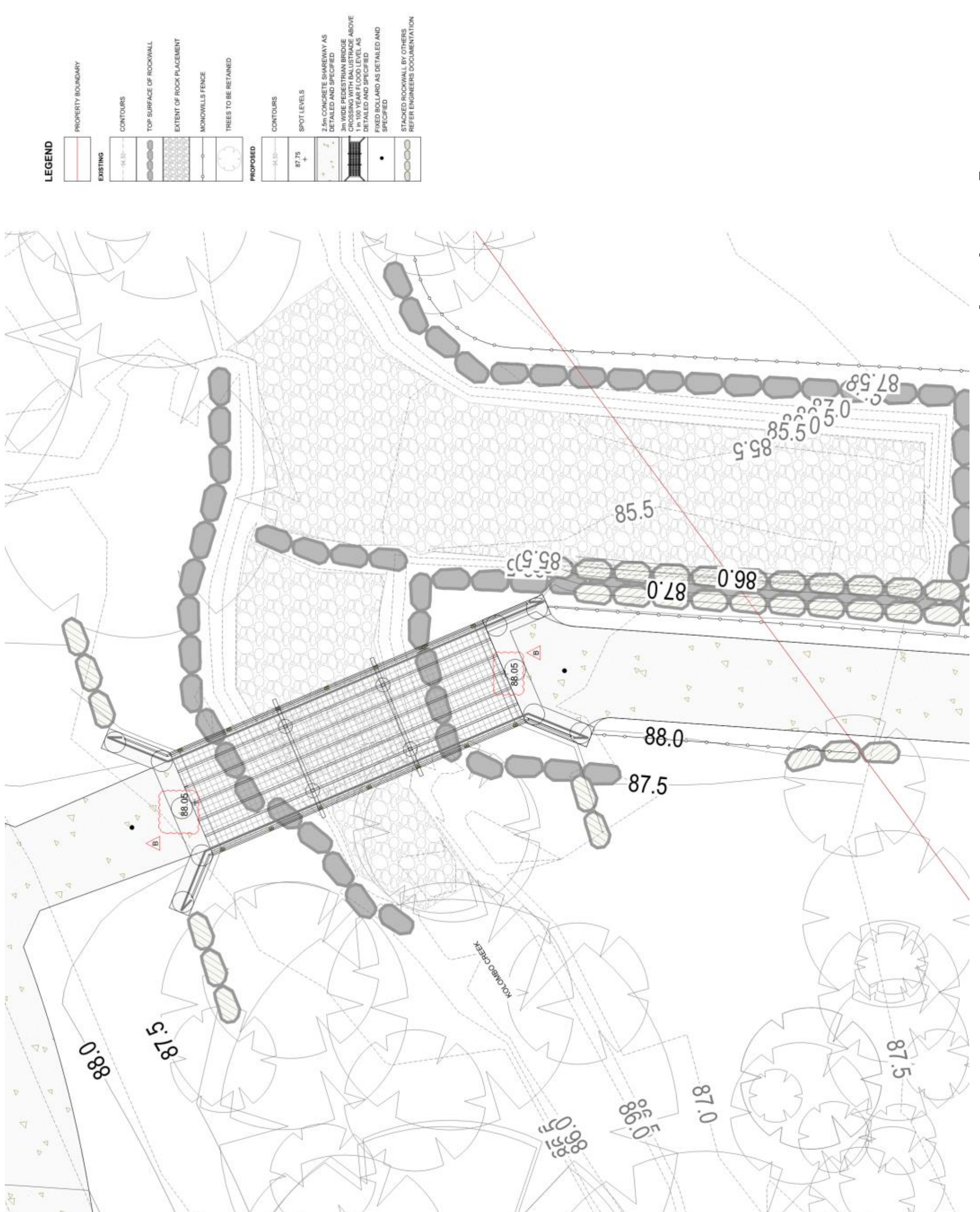
**Issue:** B





**Designed:** JZ  
**Drawn:** TL  
**Check:** JZB

**Approved:** James Delaney  
**By:** James Delaney  
**Checked:** James Delaney

**Signature:** James Delaney  
**Date:** 03.02.22

This is a preliminary design. It is not intended for construction. Check with the relevant authorities for any requirements or conditions of use prior to the commencement of any work. The preparation of this drawing is the responsibility of the consultant.

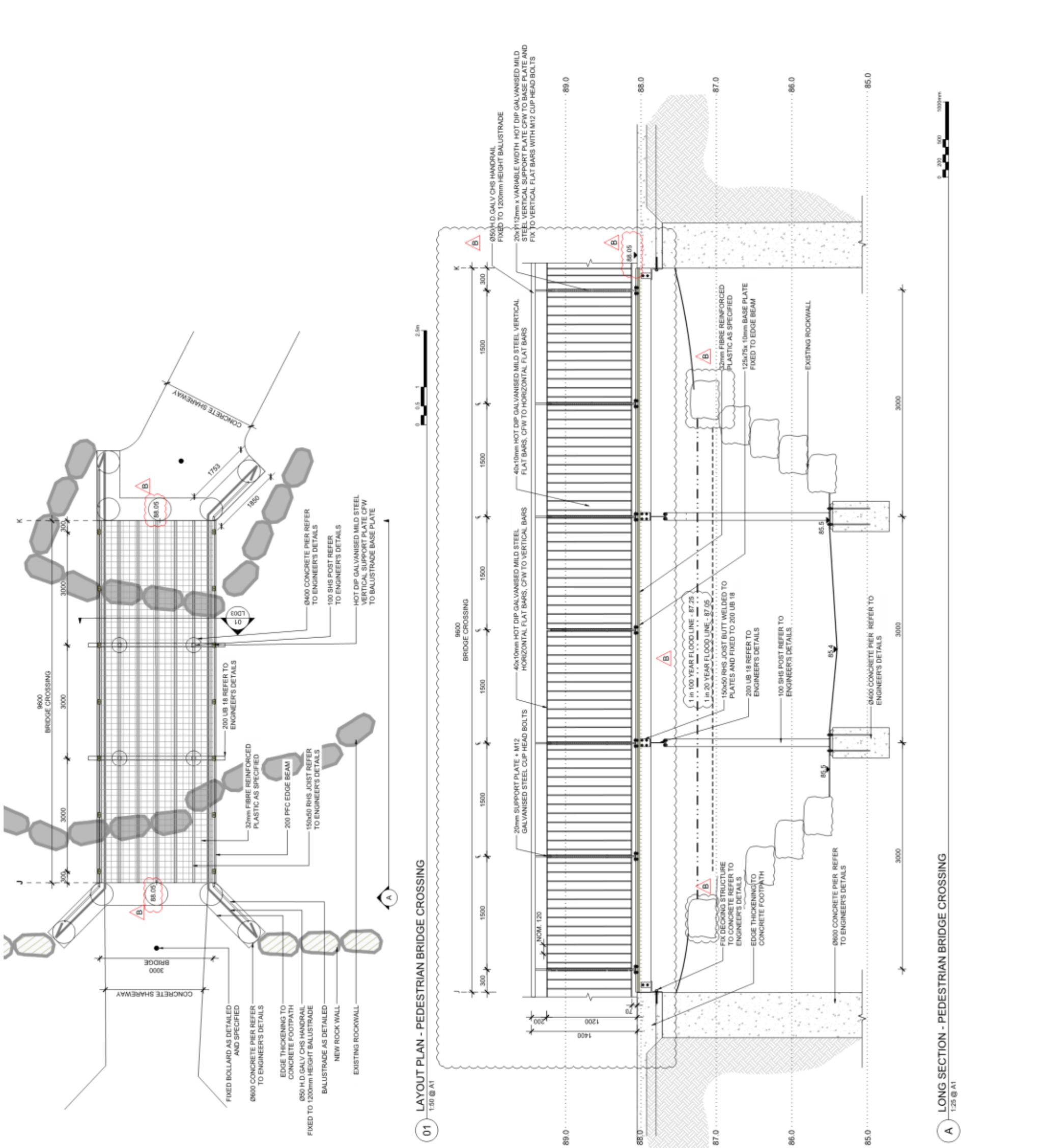


							
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Engineer:		Consultant:		Client:		Key plan:	
Project:		Title:		Scale:		Date:	
B 17.02.22 FOR DA/CC		B 17.02.22 FOR DA/CC		AS SHOWN @ A1		FEBRUARY 2019	
A 18.10.19 FOR DA/CC		A 18.10.19 FOR DA/CC		FOR DA / CC		14100	
Project:		Title:		Scale:		Date:	
ORAN PARK SOUTH TRANCHE 31 STAGE 3 KOLOMBO CREEK PEDESTRIAN BRIDGE CROSSIN		BRIDGE DETAILS 01		AS SHOWN @ A1		FEBRUARY 2019	
Sheet Date:		Drawing status:		FOR DA / CC		14100	
Description:		Drawing number:		LD01		Job N:	
Design:		Drawn:		Checked:		Job N:	
Approved:		Drawn:		Checked:		Job N:	
Approved:		Drawn:		Checked:		Job N:	
Approved:		Drawn:		Checked:		Job N:	
Approved:		Drawn:		Checked:		Job N:	

FOR CONSTRUCTION

Approval: James Delaney  
 Signature: James Delaney  
 Date: 03.02.22

Do not scale drawings. Use stated dimensions only. Check for any discrepancies on site prior to the commencement of any work. Be prepared to stop drawings if the fabrication of components.



**calbre CONSULTING**

**GREENFIELDS Development Company**

**ESTATE POWER DESIGN PTY LIMITED**

Issue No	Date	Description
B	17.02.22	FOR DA/CC
A	14.10.19	FOR DA/CC

Project: **ORAN PARK SOUTH TRANCHE 31 STAGE 3 KOLOMBO CREEK PEDESTRIAN BRIDGE CROSSING**

Title: **BRIDGE DETAILS 02**

Scale: AS SHOWN @ A1

DWH: FEBRUARY 2019 14100

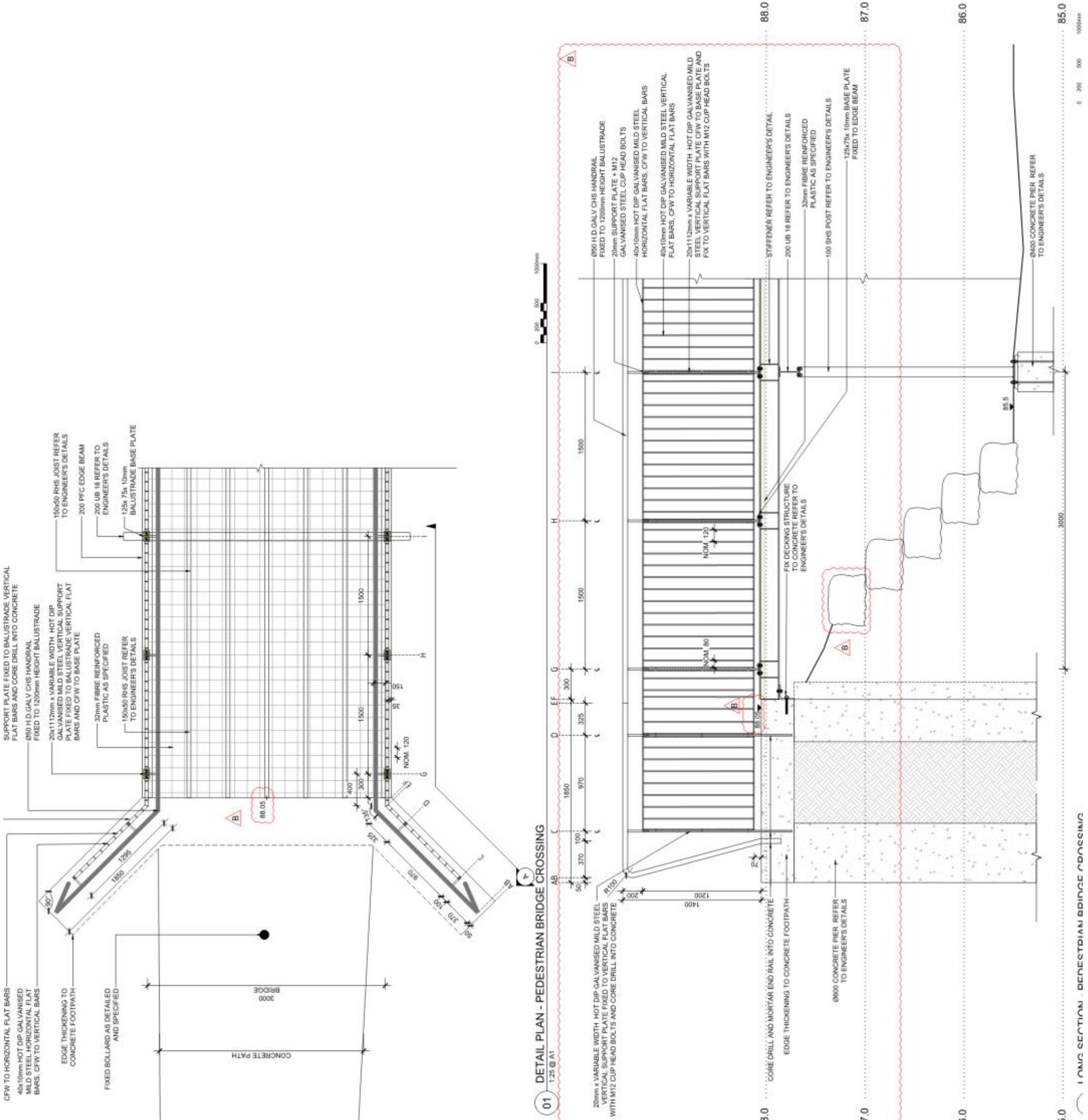
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Drawing number: LD02

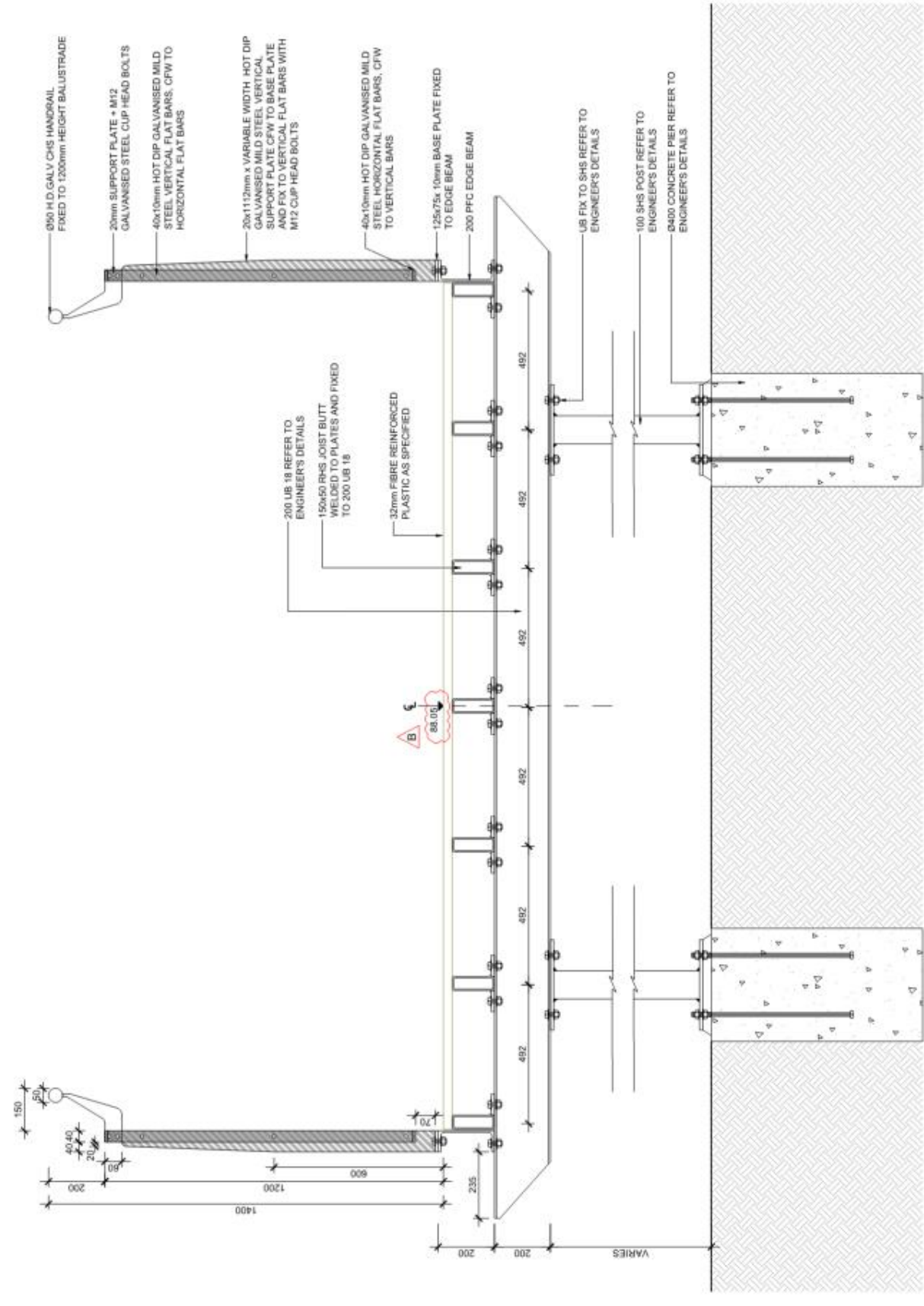
Designed: JLD  
Drawn: TL  
Checked: JL

Approved: James Deaneby  
By: James Deaneby  
Date: 03.03.22

FOR CONSTRUCTION



 <p>1000 Lakeshore Street Camden NJ 08102 7 (609) 291-1000 www.camden.gov</p>	 <p>Engineer</p>	 <p>ESTATE POWER DESIGN PTY LIMITED ELECTRICAL, MECHANICAL, PLUMBING, HEATING, VENTILATION, AIR CONDITIONING, REFRIGERATION, GAS FITTING, AND ALLIED SERVICES 142 TEMPLETON ROAD, BLACKTOWN, NSW Tel: (02) 9615 1234 Email: info@epdesign.com.au</p>	 <p>Client</p>	 <p>Key plan</p>	<table border="1"> <tr> <th>Sheet</th> <th>Date</th> <th>Description</th> </tr> <tr> <td>A</td> <td>14.10.19</td> <td>FOR DA/CC</td> </tr> <tr> <td>B</td> <td>17.03.22</td> <td>FOR DA/CC</td> </tr> </table>	Sheet	Date	Description	A	14.10.19	FOR DA/CC	B	17.03.22	FOR DA/CC	<p>Project</p> <p><b>ORAN PARK SOUTH TRANCHE 31 STAGE 3 KOLOMBO CREEK PEDESTRIAN BRIDGE CROSSING</b></p>	<p>Title</p> <p><b>BRIDGE DETAILS 03</b></p>	<p>Scale</p> <p>AS SHOWN @ A1</p>	<p>Date</p> <p>FEBRUARY 2019</p>	<p>Job N</p> <p>14100</p>	<p>Drawing status</p> <p>FOR DA / CC</p>	<p>Drawing number</p> <p>LD03</p>	<p>Design: JZ</p> <p>Drawn: TL</p> <p>Check: JZ</p>	<p>Approval:</p> <p>By: James Deane</p> <p>Author: (A666) Local Civil Design Checked: (A666) Engineering Consultant Date: 03.02.22</p>	<p>FOR CONSTRUCTION</p>
Sheet	Date	Description																						
A	14.10.19	FOR DA/CC																						
B	17.03.22	FOR DA/CC																						



01 TYPICAL SECTION - PEDESTRIAN BRIDGE CROSSING  
1:10 @ A1

Issue Date	Description
17.03.22	FOR BA/CC
12.10.19	FOR BA/CC
19.05.19	FOR BA/CC

**Project:** ORAN PARK SOUTH TRANCHE 31 STAGE 3 KOLOMBO CREEK PEDESTRIAN BRIDGE CROSSIN

**Title:** LANDSCAPE DETAILS

**Scale:** AS SHOWN @ A1

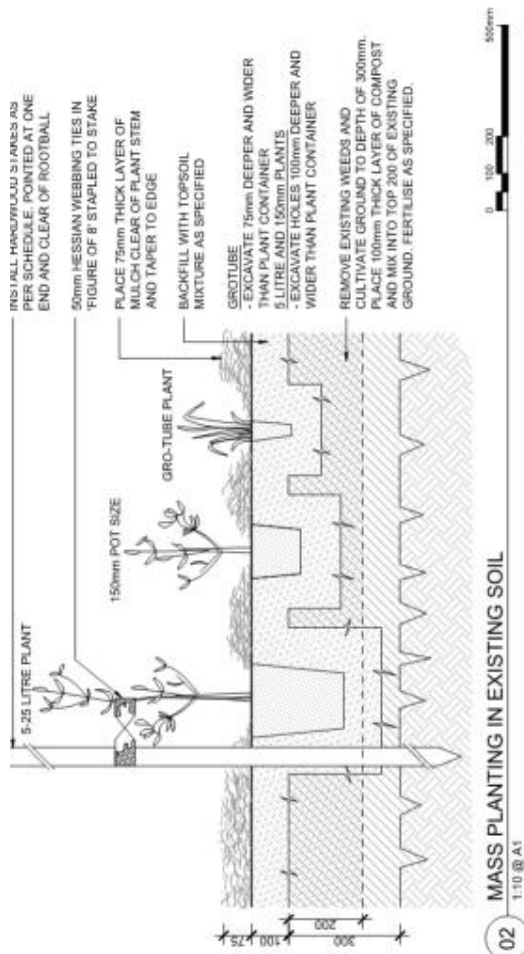
**DWG:** FEBRUARY 2019 14100

**Drawing status:** FOR DA / CC

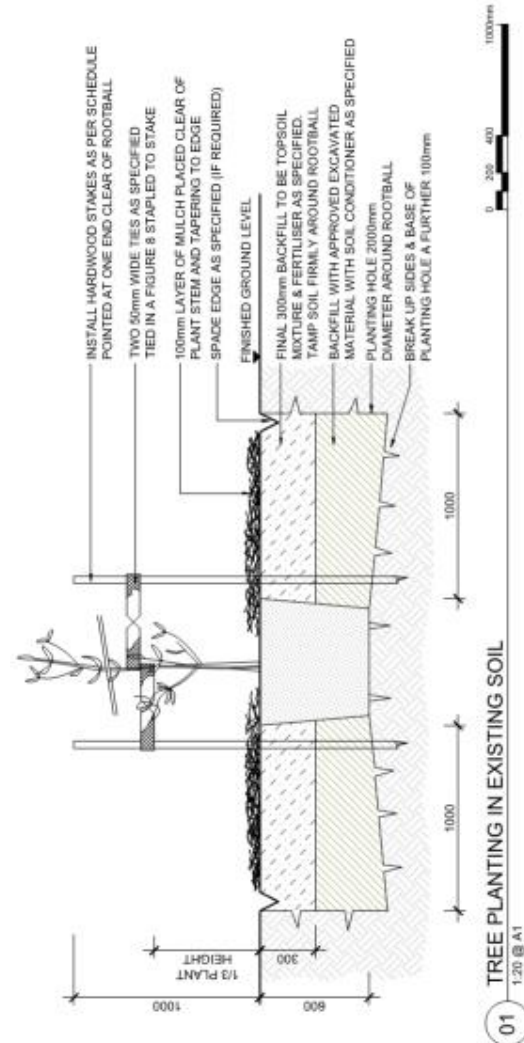
**Drawing number:** LD04

Designed:	Drawn:	TL:	Check:
Approved:	By:	James Drabney	James Drabney
Do not scale or modify. Use original dimensions only. Check & verify level line drawings in the fabrication of components.			

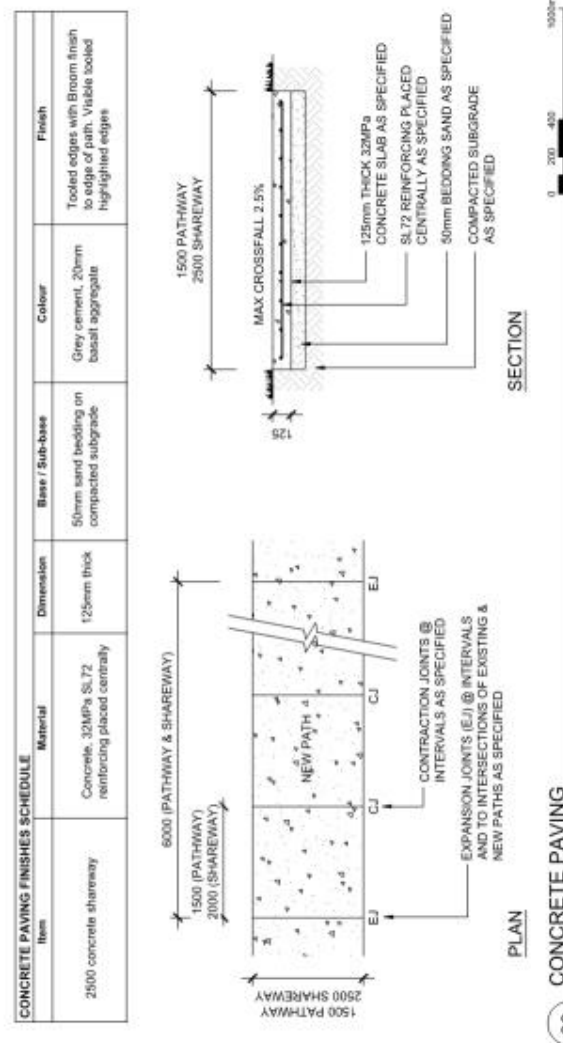
FOR CONSTRUCTION



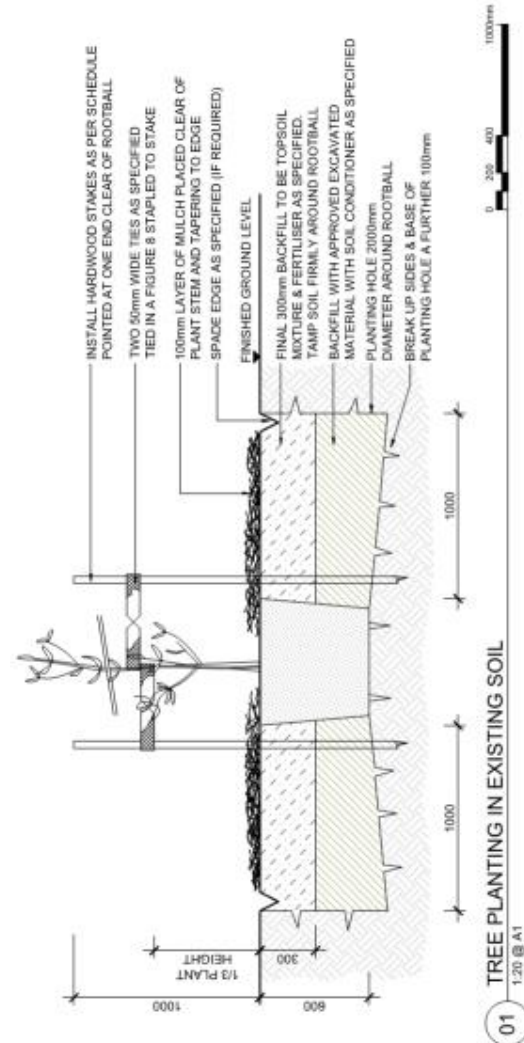
02 MASS PLANTING IN EXISTING SOIL  
1:20 @ A1



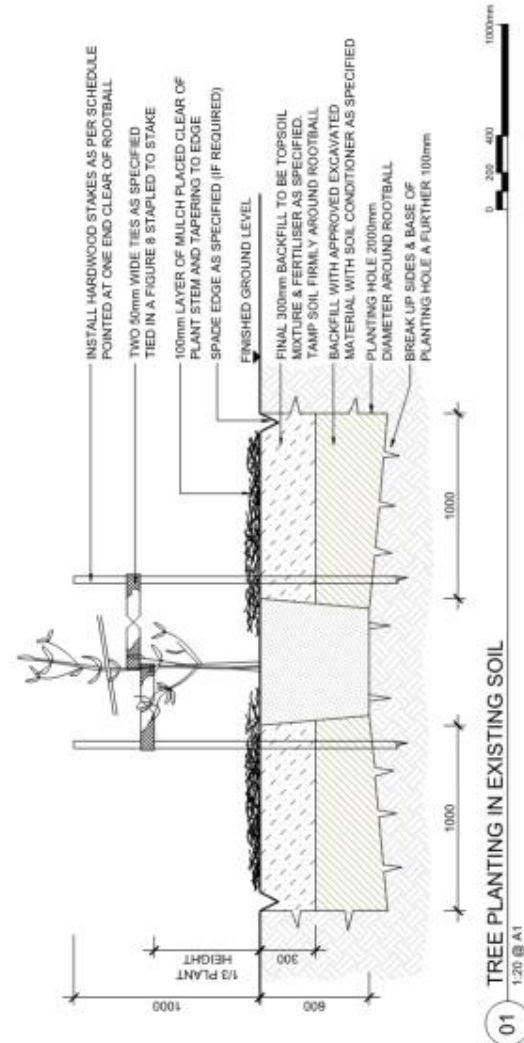
01 TREE PLANTING IN EXISTING SOIL  
1:20 @ A1



03 CONCRETE PAVING  
1:20 @ A1



04 CONCRETE JOINTS  
1:20 @ A1



05 FIXED BOLLARD - BO  
1:10 @ A1

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